



STAFF REPORT

DATE: 12/08/2025

TO: Honorable Chair and Commissioners

FROM: Planning and Development Department

2025-441

APPLICATION REQUEST:

PP2024-0001: Precise Plan to review the site plan, architecture, landscaping, and other development features associated with a proposed 40,000 square foot Northgate Gonzalez Market and remodel of an existing 6,930 square foot commercial building on 4.70 acres, generally located at the northwest corner of W. Sixth Street and N. Main Street. (Applicant: Marco Arzola for Northgate Gonzalez Market, 1201 N. Magnolia Avenue, Anaheim, CA 92801)

RECOMMENDED ACTION:

That the Planning and Housing Commission adopt Resolution No. 2677 GRANTING PP2024-0001 based on the findings contained in the staff report and conditions of approval.

PROJECT SITE SUMMARY:

Area of Project Site	4.70 acres
Existing Zoning	Downtown (D), Single Family (SF) and Gateway Business (GB)
Proposed Zoning	Downtown (D)
Existing General Plan	Mixed Use Downtown (MUD), Low Density Residential (LDR) and General Commercial (GC)
Proposed General Plan	Mixed Use Downtown (MUD)
Existing Land Use	Vacant parcels, strip retail and commercial bank buildings
Proposed Land Use	Northgate grocery market and commercial bank/restaurant building
Surrounding Zoning / Land Use	N: SF and GB / Single Family Residential E: D / Commercial S: D / Medical Office and Public Library W: SF and D / Single Family Residential and Commercial

BACKGROUND

The proposed project is for the development of a 40,000 square foot Northgate Gonzalez grocery market and remodel of an existing 6,930 square foot commercial building into a bank and restaurant use on 4.70 net acres. The development includes new landscaping, parking, signage, and public right-of-way improvements such as sidewalks, curb and gutter, and utility and stormwater improvements.

The project site is comprised of 13 parcels located on the west side of S. Main Street, between W. Sixth Street to W. Fourth Street, and includes two parcels located on the north side of W. Fourth Street, between S. Belle Avenue and S. Washburn Avenue (Exhibit 2). Eleven of the parcels are currently vacant, while two contain commercial buildings (Citizens Bank and a strip retail building) slated for demolition as part of the project development. The project site also includes segments of W. Fifth Street and W. Fourth Street totaling approximately 0.87 acres which presently intersect the project site and will be vacated to facilitate the project's development. The one-acre Helmle parcel, located at the northwest corner of W. Sixth Street and S. Main Street (203 W. Sixth Street), is not included within the project site.

The parcels are located within the Downtown Revitalization Specific Plan (DRSP) and are governed by the development standards adopted for said specific plan. The 11 parcels between W. Sixth Street and W. Fourth Street will maintain their existing zoning of Downtown (D) and General Plan land use designation of Mixed Use Downtown (MUD). The two parcels on the north side of W. Fourth Street are zoned Single Family (SF) and Gateway Business (GB) and have a General Plan land use designation of Low Density Residential (LDR) and General Commercial (GC).

In order to implement the proposed commercial project, the applicant is requesting concurrent approvals of a General Plan Amendment (GPA2024-0003) and Specific Plan Amendment (SPA2024-0003) to change the General Plan land use designation and zoning of the two parcels on the north side of W. Fourth Street to MUD and D respectively, and extend the MUD and D designations to the 0.87 acres of public right-of-way that are to be vacated. The applicant also seeks approval of Parcel Map (PM) 38981 to consolidate the 13 parcels and 0.87 acres of public right-of-way into a single 4.70-acre parcel for the development. Details associated with the aforementioned applications are provided under separate staff reports for the Planning and Housing Commission's consideration.

The project was initially reviewed by the Development Plan Review (DPR) Committee on August 10, 2023. The applicant formally submitted the Precise Plan and the abovementioned applications to the city on August 19, 2024. The applications were reviewed by the Project and Environmental Review Committee (PRC) on September 12, 2024. Missing and revised items were subsequently submitted, and the applications were deemed complete on

October 28, 2025, and scheduled for the Planning and Housing Commission meeting of December 8, 2025.

PROJECT DESCRIPTION

The following sections discuss the details of the project including the site development, operations, architectural design, landscaping, walls and fencing, signage, and public right-of-way improvements.

Site Plan

The market is proposed on the northern part of the site, while the bank and restaurant building is located at the southwestern corner. A shared parking lot is centrally located to serve both facilities. An interior sidewalk is provided within the parking lot to connect the market to the commercial building. The interior sidewalk also discourages cut-through vehicular traffic from S. Main Street through the project site to S. Belle Avenue. The site plan is attached as Exhibit 4.

Customer entrance into the Northgate market is located on the south side of the building, while the north side contains a loading dock and trash compactor. The shopping carts will be located within the front of the store, outside of any drive aisles, for patrons, with several shopping cart return locations within the market parking lot area. Northgate did not submit a floor plan to the city for review but has indicated to city staff that the floor plan would be similar to their other recently opened stores. The floor plan will be reviewed by staff during the plan checking process.

The plotting of the buildings meets the minimum setback requirements prescribed by the DRSP for the Downtown (D) zone. This is summarized in Table 1.

TABLE 1: BUILDING SETBACKS

Setback Location	Setback Standard	Setback Provided	Meets Standard
Front Yard Setback (W. Sixth St.)	0 feet	9 feet (setback after street dedication)	Yes
Street Side Yard Setback (S. Main St.)	0 feet	11 feet	Yes
Street Side Yard Setback (S. Belle Ave.)	0 feet	36	Yes
Rear Yard Setback	10 feet minimum	42 feet	Yes

Operations

Northgate Gonzalez has been in operation for over 45 years with 43 markets in operation. The company is operated by the Gonzalez family and is in its 4th generation with 32 family members spanning three generations working within the business.

The new 40,000 square foot Northgate Gonzalez grocery market specializes in traditional Mexican foods, including fresh vegetables, deli serving hot Mexican dishes, full-service meat and fish market including specialty cheeses, tortilleria (fresh tortillas), panaderia (Mexican sweet bread), daily made fresh salsas, tamales, etc. The project proposes an outdoor patio dining area of approximately 650 square feet featuring a walk-up Taqueria (taco) order/pick-up window operated by Northgate Gonzalez market. This concept is similar to Northgate's other market locations in the cities of Fontana and La Habra, to name a few.

Additional information regarding the company's history and background is provided in the applicant's project description letter dated July 22, 2024, and applicant's business operations letter dated October 10, 2025 (Exhibit 15). This store would be a regular Northgate Gonzalez grocery market and not based on their large Costa Mesa Mercado market.

The market will be open seven (7) days a week, from 6:30 a.m. to 10 p.m. The applicant anticipates the market will employ approximately 180 to 200 employees. Grocery store deliveries will be from 7 a.m. to 7 p.m. daily. There will be a variety of deliveries to the market daily as follows:

- One (1) delivery of produce, meat and deli per day, seven (7) days a week;
- One (1) delivery of groceries per day, five (5) days a week, occurring in the afternoon;
- Two (2) delivery loads of beer/soda per day, from Monday through Saturday; and
- 2 to 4 delivery loads of bread, chips and tortillas per day.

In total, there will be an average of 4 to 5 large truck and trailer deliveries per day, and an average of two bobtail truck deliveries per day.

It is anticipated that the future restaurant tenant and bank will have a small number of employees, and the restaurant would typically have a daily delivery of supplies from a smaller bobtail truck. No other information regarding the restaurant and bank building has been provided to city staff.

Architecture

The DRSP includes architectural design guidelines to ensure quality architecture for commercial developments within the Downtown Core area. The DRSP does not prescribe a specific architectural theme for commercial buildings, and replicating past styles is not mandatory, but acknowledging historic architectural elements is required. In keeping with the design guidelines, the project features a Spanish Colonial architectural theme using clean, white stucco with colored accent trimming, decorative arched features with Saltillo tiles, decorative Spanish accent tile insets, metal awnings and metal trellis for vines, precast

concrete trimming, wooden trellis, Spanish tile roofing, and large clay tile potted plants. The market's maximum height is 40 feet, while the bank/restaurant building is 33'-5" tall. The architecture is compatible with and complements the prevalent Spanish-influence character of Corona's downtown area. The Conceptual Rendering and Colored Elevations are provided as Exhibits 8 and 9, respectively.

Trash Enclosure and Compactor

One trash enclosure is provided near the bank/restaurant building to serve this facility. The trash enclosure has stucco walls with solid metal gates and a decorative metal cover. The space between the enclosure walls and cover are secured by metal mesh material on all four sides.

A trash compactor is proposed on the north side of the Northgate market to serve the market. The compactor is located between the building and a 10-foot high screen/sound wall. According to the applicant, the market does not utilize trash enclosures at any of their market locations as they operate solely using their on-site trash compactor located at the rear of the market. Staff provided a copy of the site plan with proposed trash enclosure and trash compactor locations to Waste Management. Waste Management has approved the project's site plan, including the proposed trash compactor.

Lighting

The project's lighting includes parking lot lighting fixtures as well as decorative wall-mounted lighting on the market and multi-tenant commercial building. All outdoor lighting are required to be directed downward and shielded to minimize off-site spillover onto adjacent residential and sensitive land uses. The project site is located in a developed area where ambient lighting exists from the existing streetlight poles and existing, surrounding developments. Therefore, the project's lighting is not expected to be a nuisance to the area.

Perimeter Walls

The Wall and Fence Plan is provided in Exhibit 12. The project includes a six-foot-high split face masonry wall along the northern property line, which will provide a physical buffer between the project site and the abutting residential uses to the north.

The project features a 12-foot-high screen wall at the truck loading area and a 10-foot-high screen wall at the trash compactor unit. The trash compactor area will be fenced and gated to prevent unauthorized access and cut-through traffic behind the market.

Landscaping

The Conceptual Landscape Plan includes approximately 27,642 square feet of new drought-tolerant, low-water use ornamental landscaping throughout the site. Landscaping would include a variety of trees, such as Jacarandas, Palo Verdes, Fern Pines, Southern Live Oaks,

Ornamental Orange and Lemon, Date Palms and others. The parking lot area and perimeters of the project site include ornamental shade trees, screen shrubs to conceal the parking area and a variety of ground covers. Along the northern property line, the project includes Fern Pines, an evergreen tree, to screen the project from the neighboring residential land uses to the north. The screen trees are in addition to the six-foot high block wall that the applicant will construct along the north property line.

The project also includes the installation of new trees and landscaping within the public right-of-way along S. Main Street and S. Belle Avenue in compliance with the Downtown Revitalization Specific Plan. All new landscape materials proposed for the project are required to be California-friendly, and irrigation is required to be water efficient to meet the City’s landscape design guidelines for commercial projects. Formal landscape and irrigation plans will be subject to final review and approval by the city staff. The Conceptual Landscape Plan is provided in Exhibit 11.

Signage

The project includes a comprehensive sign program for the market and commercial building which will be reviewed and permitted separately during plan check submittal. The conceptual sign program is provided in Exhibit 14. Table 2 describes the proposed project signage.

TABLE 2: PROPOSED PROJECT SIGN PROGRAM

Type of Sign	Total Signs	Location	Proposed Height	Proposed Sign Area	Sign Text	Meets DRSP Standard
Monument Entry Sign (Internally Illuminated)	3	Entrance on Main St., Corner of 6 th St. & Belle Ave., 2 nd entrance on Belle Ave.	7 feet	45 sq.ft.	Northgate Market, Restaurant and Bank	Yes, except for (1) the sign at the Main St. entrance and (2) the internally illuminated sign on S. Belle Ave. (Sign at entrance encroaches into public right-of-way; and no internal or external illumination permitted when sign is located adjacent to residential zone)

Wall/Market (Internally Illuminated)	3	South, east and west building elevations	48-inch letters / 84- inch logo (South and East) & 30-inch letters / 48- inch logo (West)	200 sq.ft. 100 sq.ft.	Northgate Market and Gonzalez Logo	No (south & east) (Max. sign area 150 sq.ft.) Yes (west)
Wall/Market Secondary (Internally Illuminated)	4	South building elevation	14-inch letters	50 sq.ft. per sign	Panaderia Carniceria Tortilleria Cocina	No (Max. sign area 150 sq.ft.)
Sub-Tenant Market Wall (Internally Illuminated)	2	South and east building elevation	48-inch high 18-inch high	50 sq.ft. 25 sq.ft.	Prospera and Taqueria	No (Max. sign area 150 sq.ft.)
Multi-Tenant (Internally Illuminated)	6 (3 signs per tenant)	2 per building elevation (east & west) 1 per north & south elevation	Logo and sign not to exceed 48- inches	1.5 sq.ft.	Restaurant and Bank	No (Max. sign area 1 sq.ft. per linear foot of building elevation)

The applicant is required to submit the final signage plan to the Planning and Development Department for review and permitting before installation. A condition has been added that the project sign program comply with all codes in place at the time of permitting, including the DRSP and any applicable conditions of approval.

Access and Circulation

The project has five vehicular access points. The main entrance into the project site is located at the site’s east perimeter and is a shared driveway with the adjacent Hemle parcel (203 W. Sixth Street). This entrance is proposed directly across from the existing entrance to the Corona Mall commercial center located on the east side of S. Main Street. The entrance will form a four-way intersection at S. Main Street and will be signalized to allow vehicles to make full turn movements.

One entrance is located at the site’s south perimeter and will allow westbound vehicles on Sixth Street to make right-turns into the site. Eastbound vehicles will not be able to turn left into the site due to the existing raised median on W. Sixth Street.

Three additional entrances are located at the site’s west perimeter and will allow vehicles to enter the site from S. Belle Avenue. The northernmost entrance on S. Belle Avenue will primarily be used for truck access to the market’s rear loading area. The entrances on S. Belle Avenue will allow for full access. The entrances on W. Sixth Street and S. Belle Avenue are not required to be signalized.

The project’s internal circulation features standard size 9-foot by 20-foot parking spaces with 25- to 28-foot-wide two-way drive aisles for vehicle access to parking spaces and to the adjacent Hemle parcel. The Fire Department confirmed that the internal circulation meets the 28-foot minimum width fire lane requirements.

Delivery trucks will access the project site from the SR-91 freeway off-ramp at Main Street and head southbound, turning westbound onto W. Sixth Street and northbound onto S. Belle Avenue and into the project site. After unloading, the trucks will exit the site onto S. Belle Avenue, travel southbound towards W. Sixth Street, eastbound on W. Sixth Street, northbound on N. Main Street, and back onto the SR-91 freeway west/east bound on-ramps. The applicant’s Truck Route Map is attached as Exhibit 5. Exhibit 6 is the applicant’s Truck Turn Analysis which demonstrates that Main Street, Sixth Street and Belle Avenue are adequately sized to accommodate the delivery trucks. Both exhibits were reviewed and accepted by the Traffic Division of the Public Works Department.

Parking

Based on the Corona Municipal Code (CMC), the project requires a total of 251 parking stalls for both the market and bank/restaurant building. The project provides a total of 261 parking spaces, of which 236 are on-site and the remaining 25 are on-street parking spaces located adjacent to the project site, as permitted by the DRSP. The on-street parking spaces are located along the east side of Belle Avenue adjacent to the project site and at the south end of S. Washburn Avenue, just east of the proposed market. Per the DRSP, parking is not required to be provided for the restaurant’s outdoor dining patio. The project’s parking requirements are summarized in Table 3.

TABLE 3: PARKING SUMMARY

Use	S.F.	Code Requirement	Parking Required	Parking Provided	Meets Requirement
Market	40,000	1 space per 200 sq.ft.	200	203	Yes
Restaurant	3,297	1 space per 100 sq.ft.	33	40	Yes
Bank	3,633	1 space per 200 sq.ft.	18	18	Yes
TOTAL			251	261¹	Yes

1. The project parking includes off-street and on-street parking spaces, which is allowed per Section 3.6.2 of the Downtown Revitalization Specific Plan.

Water, Sewer, and Drainage Improvements

Water for the proposed market building and commercial building will be provided by connecting to an existing 8-inch diameter waterline in S. Belle Avenue.

The project proposes a 6-inch diameter public sewer line to tie into the existing 10-inch diameter sewer in S. Main Street. For the proposed market building, a 6-inch diameter private sewer lateral is proposed at the northeast corner which will tie into the existing 6-inch diameter sewer at the northern project boundary. Connection details, alignments, and sizes of proposed sewer facilities will be validated during the plan check review process. Water and sewer service to the project site will be provided by the city.

Development of the project will increase impervious surface coverage on the site and increase surface runoff. Surface runoff, however, will be conveyed via an underground storm drain to the proposed MWS (Modular Wetlands System) treatment units to address water quality requirements before reaching an underground chamber system within the market parking area for increased runoff mitigation. A pump will then convey flows from the underground chambers to a v-ditch along the western project boundary before discharging into an existing 24-inch storm drain located in S. Belle Avenue. The conceptual Grading and Drainage Plans are provided in Exhibit 13.

Public Right-of-Way Improvements

As part of the development of the project site, the applicant is required to dedicate and/or construct the necessary and missing public improvements adjacent to the project site as described in the following subsections.

W. Sixth Street.

No public improvements are required to W. Sixth Street, however, the project site's south perimeter along W. Sixth Street requires an additional eight (8) feet of street dedication.

N. Main Street.

A fifteen (15) feet street dedication and corresponding street widening are required along the portion of Main Street adjacent to the project site. This widening is necessary to accommodate public right-of-way improvements on the west half of Main Street, including parkway landscaping, sidewalk, street trees and curb and gutter. The widening also provides sufficient width to restripe the southbound lanes to include:

- Two (2) 12-foot wide through-lanes;
- One (1) 12-foot wide left-turn pocket for vehicles entering the North Corona Mall entrance; and
- One (1) 12-foot wide right-turn pocket for vehicles entering the project site.

In addition, a new traffic signal is required to be installed at the project's primary entrance on Main Street.

S. Belle Avenue.

The City will vacate six (6) feet of right-of-way along the site's west perimeter adjacent to S. Belle Avenue, adding square footage to the project's parking area. The project is required to construct public right-of-way improvements consisting of new street trees along S. Belle Avenue adjacent to the project site, while the existing sidewalk, curb and gutter along Belle Avenue will remain.

Traffic Impact Analysis

A traffic impact analysis (LL&G, dated July 2025) was prepared for the project to evaluate the project's site access, internal circulation, and the project's potential traffic impact to the surrounding roadway circulation.

The analysis determined that the project's overall layout does not create significant vehicle-pedestrian conflict points or significant internal queuing/stacking at the project driveways. The alignment and spacing of the project driveways are also deemed to be adequate, and vehicle turning movements into and out of the site at the driveways are anticipated to operate at acceptable service levels.

The project is expected to generate approximately 3,377 daily trips, including 133 trips during the AM peak hour and 324 trips during the PM peak hour. In comparison, the prior use on the property generated a total of 884 trips per day, with 70 trips in the AM peak hour and 142 trips in the PM peak hour.

As a result, the project is anticipated to generate a net of 2,493 more trips per day with 63 more AM peak hour trips and 182 more PM peak hour trips compared to the prior use. The project's net daily trips is not anticipated to adversely impact the level of service for the 15 street intersections and nine (9) roadway segments that were evaluated in the traffic study.

Distribution of Plans

The project's development plans were circulated to service providers (Fire Department, Corona Police Department, etc.) and utility companies, allowing each entity the opportunity to review the proposal and determine the impacts of the development relative to their services. In addition, the plans were circulated to internal divisions/departments such as Development Services, Traffic, Building and Public Works. Comments from these agencies have been considered and incorporated as modifications to the plans or were included as recommended conditions to this project.

ENVIRONMENTAL ANALYSIS

Per Section 15070(b) of the State Guidelines for Implementing the California Environmental Quality Act (CEQA) and Section 6.02 of the City's Local Guidelines, a Mitigated Negative Declaration was prepared for the project since the Initial Study identified that the project's potentially significant effects to the environment are capable of being mitigated to less than significant. Therefore, based on the project's mitigation measures and mitigation monitoring and reporting program identified in the Mitigated Negative Declaration, there is no substantial evidence, in light of the whole record before the city, that the project may have a significant or potentially significant effect on the environment. The Mitigated Negative Declaration is recommended for adoption with the associated General Plan Amendment application GPA2024-0003 (Exhibit 16).

FISCAL IMPACT

The applicant has paid the applicable application processing fees for the project.

PUBLIC NOTICE AND COMMENTS

A 20-day bilingual (English and Spanish) public notice was mailed to all property owners and occupants within a 500-foot radius of the project site, as well as advertised in the Sentinel Weekly News and posted at the project site. Additionally, the MND was electronically sent to the State Clearinghouse (SCH#2025110646). As of the preparation of this report, staff received two comments related to the project and one related to the MND (Exhibits 17 and 18).

STAFF ANALYSIS

The project is a classic infill development in an urbanized area, replacing outdated buildings and structures, dilapidated chain-link fencing, missing public improvements and landscaping. The current site is incompatible with the Downtown Revitalization Specific Plan's vision for downtown, which aims to promote new developments that are respectful of the traditional town center character and enhance visual appeal and interest along the downtown streetscapes. The project will consolidate multiple, underutilized properties into a single property for a new Northgate Gonzalez market and repurpose an old bank building into a new bank and restaurant. The project will introduce improvements such as landscaped parkways with trees, new sidewalks, Spanish Colonial architectural features, new landscaping, decorative walls, new stamped driveway entries and new signage.

As proposed and conditioned, the project complies with the development standards for parking, landscaping, setbacks and other requirements prescribed by the Specific Plan. All potential environmental impacts associated with the project have been analyzed pursuant to CEQA, and all potential impacts identified in the Mitigated Negative Declaration are capable of being mitigated.

The market and bank/restaurant uses are allowed by right in the Downtown zone. The location is conveniently close to the SR-91 freeway, major and local streets and is within walking distance of residential neighborhoods. This development supports a pedestrian-friendly environment, aligning with the Specific Plan's vision for the Downtown Core area, and serves as a "catalyst" to jump start much needed economic development within the downtown area. The project fulfils the goals and policies of the General Plan that promote the redevelopment of Downtown Corona while retaining the Downtown's historic character.

Therefore, the Planning and Development Department recommends approval of PP2024-0001, subject to the findings below and the recommended conditions of approval in Exhibit 3.

FINDINGS OF APPROVAL FOR PP2024-0001

1. An initial study (environmental assessment) has been conducted by the City of Corona so as to evaluate the potential for adverse environmental impacts. The initial study identifies potentially significant effects on the environment, but:
 - a. *The project applicant has agreed to revise the project to avoid these significant effects or to mitigate the effects to a point where it is clear that no significant effects would occur, as reflected in the Mitigation and Monitoring Program within the Mitigated Negative Declaration and within the Conditions of Approval attached as Exhibits 16 and 3, respectively.*
 - b. *There is no substantial evidence before the city that the revised project may have a significant effect.*
2. All the conditions necessary to granting a Precise Plan as set forth in Section 17.91.070 of the CMC do exist in reference to PP2024-0001 for the following reasons:
 - a. *The project is consistent with the General Plan land use designation of Mixed Use Downtown (MUD), as this designation accommodates the development of properties exclusively for retail commercial uses or an integrated mix of commercial and residential uses. Additionally, the project proposes a Floor Area Ratio (FAR) of 0.22, which does not exceed the maximum FAR of 3.0 established by the General Plan for the MUD designation. Furthermore, the project is consistent with the following General Plan policies:*
 - i. *Policy LU-11.1, which encourages having a range of retail, service and other commercial uses in the City that provide goods and services to meet the diverse needs of Corona's residents and businesses, in accordance with the land use plan's designations and applicable density standards and design and development policies. The new Northgate Gonzalez market will offer specialty*

foods, grocery products, and services that respond to the needs of the community. Also, the proposed use complies with the site's Downtown zoning and Mixed Use Downtown general plan land use designation, as well as the design and development standards of the Downtown Revitalization Specific Plan, ensuring compatibility with the surrounding area and the City's long-term planning objectives. Therefore, the project is consistent with Policy LU-11.1.

- ii. Policy LU-11.3, which seeks to promote reinvestment in declining shopping centers and districts, which an emphasis on new retail uses that serve adjacent neighborhoods and contribute to the overall vitality of the centers. The project represents a significant reinvestment in the Downtown area by redeveloping an underutilized site with neighborhood-oriented retail and commercial service uses. The project will serve the daily shopping needs of surrounding residential neighborhoods, attract new customers to Downtown, increase pedestrian activity, and generate economic activity. As such, the project supports revitalization efforts and contributes to the long-term economic vitality of the Downtown District, consistent with Policy LU-11.3.
 - iii. Policy LU-17.7, which is to ensure that new Downtown development is attractive and creates an image conducive to economic revitalization consistent with the adopted specific plan. The project has been designed in accordance with the adopted Downtown Revitalization Specific Plan design guidelines and development standards, and incorporates attractive architecture features, including Spanish-style elements and new on-site landscaping as well as landscaping within the adjacent parkways in the public right-of-way. Accordingly, the project is consistent with the intent and requirement of Policy LU-17.7.
 - iv. Policy LU-17.15, which seeks to enhance the historic character of the Downtown by requiring new construction to implement architectural features reminiscent of the era. The project's Spanish Colonial architecture is compatible with and complements the prevalent Spanish-influence character of the downtown area, thereby furthering the intent of this policy.
- b. The project adheres to the Downtown (D) zone regulations outlined in the Downtown Revitalization Specific Plan, ensuring compliance with the development standards as well as the relevant codes and requirements stipulated in the Corona Municipal Code.
- c. The project has been reviewed in compliance with the California Environmental Quality Act, and all applicable requirements and procedures of the act have been followed. As justified in Finding # 1, a Mitigated Negative Declaration is recommended for adoption with associated application GPA2024-0003 as the initial study identifies potentially significant effects on the environment, but the project applicant has

agreed to revise the project to avoid these significant effects or to mitigate the effects to a point where it is clear that no significant effects would occur.

- d. The project site is of a sufficient size and configuration to accommodate the design and scale of proposed development, including buildings and elevations, landscaping, parking and other physical features of the proposal, as demonstrated by the project plans for PP2024-0001.*

- e. The architectural design of the proposed development consists of Spanish Colonial architectural elements that are compatible with the character of the surrounding neighborhood. The project will enhance the visual character of the neighborhood by improving vacant undeveloped property and will provide for harmonious, orderly and attractive development of the site.*

PREPARED BY: ROCIO LOPEZ, SENIOR PLANNER

REVIEWED BY: SANDRA VANIAN, PLANNING MANAGER

SUBMITTED BY: COLBY CATALDI, PLANNING AND DEVELOPMENT DIRECTOR

EXHIBITS:

1. Resolution No. 2677
2. Locational and Zoning Map
3. Conditions of Approval
4. Site Plan
5. Truck Route Map
6. Truck Turn Analysis Plan
7. Photos of Site
8. Colored Rendering
9. Colored Elevations
10. Color Material Palette
11. Conceptual Landscape Plan
12. Wall and Fence Plan
13. Conceptual Grading and Drainage Plans
14. Conceptual Sign Program
15. Applicant's letters, dated July 22, 2024, and October 10, 2025
16. Environmental Documentation
17. Public Comments, dated November 17, 2025 and received December 3, 2025
18. Public Comment on MND, dated November 25, 2025

Case Planner: Rocio Lopez (951) 736-2293



RESOLUTION NO. 2677

APPLICATION NUMBER: PP2024-0001

A RESOLUTION OF THE PLANNING AND HOUSING COMMISSION OF THE CITY OF CORONA, CALIFORNIA, GRANTING A PRECISE PLAN TO REVIEW THE SITE PLAN, ARCHITECTURE, LANDSCAPING, AND OTHER DEVELOPMENT FEATURES ASSOCIATED WITH A PROPOSED 40,000 SQUARE FOOT NORTHGATE GONZALEZ MARKET AND REMODEL OF AN EXISTING 6,930 SQUARE FOOT COMMERCIAL BUILDING ON 4.70 ACRES, GENERALLY LOCATED AT THE NORTHWEST CORNER OF W. SIXTH STREET AND N. MAIN STREET. (APPLICANT: MARCO ARZOLA FOR NORTHGATE GONZALEZ)

WHEREAS, the application to the City of Corona, California, for a Precise Plan under the provisions of Chapter 17.91 in the Corona Municipal Code, has been duly submitted to said City's Planning and Housing Commission for the review of the site plan, architecture, landscaping, and other development features associated with a proposed 40,000 square foot Northgate Gonzalez Market and remodel of an existing 6,930 square foot commercial building on 4.70 acres, generally located at the northwest corner of W. Sixth Street and N. Main Street; and

WHEREAS, the Precise Plan was submitted in conjunction with General Plan Amendment 2024-0003 (GPA204-0003), Specific Plan Amendment 2024-0003 (SPA2024-0003), and Parcel Map 38981 (PM 38981) ("Proposed Project"); and

WHEREAS, the Planning and Housing Commission held a noticed public hearing for GPA2024-0003, SPA2024-0003, PM 38981 on December 8, 2025, as required by law; and

WHEREAS, on December 8, 2025, as the first action on the Proposed Project, the Planning and Housing Commission approved Resolution No. 2676 adopting GPA2024-0003 and recommended to the City Council the adoption of the Mitigated Negative Declaration (MND) prepared for the Proposed Project pursuant to California Environmental Quality Act (CEQA) Guidelines Section 15070(b) and Section 6.02 of the City Local CEQA Guidelines,

EXHIBIT 1

because the information contained in the MND, the initial study and the administrative records for this project, including all written and oral evidence provided during the comment period and presented to the Planning and Housing Commission, the Commission found that potential environmental impacts of this project are either no impact or less-than-significant.

WHEREAS, after close of said hearing, the Commission by formal action, found that all the conditions necessary to granting a Precise Plan as set forth in Corona Municipal Code Section 17.91.070 do exist in reference to PP2024-0001 based on the evidence presented to the Commission during said hearing; and

WHEREAS, the Planning and Housing Commission based its recommendation to approve PP2024-0001 on certain conditions of approval and the findings set forth below.

NOW, THEREFORE, THE PLANNING AND HOUSING COMMISSION OF THE CITY OF CORONA, CALIFORNIA, DOES ORDAIN AS FOLLOWS:

SECTION 1. CEQA Findings. As the decision-making body for this PP2024-0001, the Planning and Housing Commission has reviewed and considered the information contained in the MND, the initial study and the administrative records for this PP2024-0001, including all written and oral evidence provided during the comment period, and the Commission finds that the project's potentially significant effects to the environment are capable of being mitigated to less than significant. Therefore, based on the project's mitigation measures and mitigation monitoring and reporting program identified in the MND, there is no substantial evidence, in light of the whole record before the city, that the project may have a significant or potentially significant effect on the environment.

SECTION 2. Precise Plan Findings. Pursuant to Corona Municipal Code ("CMC") Section 17.91.070 and based on the entire record before the Planning and Housing Commission, including all written and oral evidence presented to the Commission, the Commission hereby makes and adopts the following findings:

1. An initial study (environmental assessment) has been conducted by the City of Corona so as to evaluate the potential for adverse environmental impacts. The initial study identifies potentially significant effects on the environment, but:
 - a. *The project applicant has agreed to revise the project to avoid these significant effects or to mitigate the effects to a point where it is clear that no significant effects would occur, as reflected in the Mitigation and Monitoring Program within the Mitigated Negative Declaration and within the Conditions of Approval attached as Exhibits 16 and 3, respectively, to the staff report for PP2024-0001.*
 - b. *There is no substantial evidence before the city that the revised project may have a significant effect.*
2. All the conditions necessary to granting a Precise Plan as set forth in Section 17.91.070

of the CMC do exist in reference to PP2024-0001 for the following reasons:

- a. *The project is consistent with the General Plan land use designation of Mixed Use Downtown (MUD), as this designation accommodates the development of properties exclusively for retail commercial uses or an integrated mix of commercial and residential uses. Additionally, the project proposes a Floor Area Ratio (FAR) of 0.22, which does not exceed the maximum FAR of 3.0 established by the General Plan for the MUD designation. Furthermore, the project is consistent with the following General Plan policies:*
 - i. *Policy LU-11.1, which encourages having a range of retail, service and other commercial uses in the City that provide goods and services to meet the diverse needs of Corona's residents and businesses, in accordance with the land use plan's designations and applicable density standards and design and development policies. The new Northgate Gonzalez market will offer specialty foods, grocery products, and services that respond to the needs of the community. Also, the proposed use complies with the site's Downtown zoning and Mixed Use Downtown general plan land use designation, as well as the design and development standards of the Downtown Revitalization Specific Plan, ensuring compatibility with the surrounding area and the City's long-term planning objectives. Therefore, the project is consistent with Policy LU-11.1*
 - ii. *Policy LU-11.3, which seeks to promote reinvestment in declining shopping centers and districts, which an emphasis on new retail uses that serve adjacent neighborhoods and contribute to the overall vitality of the centers. The project represents a significant reinvestment in the Downtown area by redeveloping an underutilized site with neighborhood-oriented retail and commercial service uses. The project will serve the daily shopping needs of surrounding residential neighborhoods, attract new customers to Downtown, increase pedestrian activity, and generate economic activity. As such, the project supports revitalization efforts and contributes to the long-term economic vitality of the Downtown District, consistent with Policy LU-11.3.*
 - iii. *Policy LU-17.7, which is to ensure that new Downtown development is attractive and creates an image conducive to economic revitalization consistent with the adopted specific plan. The project has been designed in accordance with the adopted Downtown Revitalization Specific Plan design guidelines and development standards, and incorporates attractive architecture features, including Spanish-style elements and new on-site landscaping as well as landscaping within the adjacent parkways in the public right-of-way. Accordingly, the project is consistent with the intent and requirement of Policy LU-17.7.*

- iv. Policy LU-17.15, which seeks to enhance the historic character of the Downtown by requiring new construction to implement architectural features reminiscent of the era. The project's Spanish Colonial architecture is compatible with and complements the prevalent Spanish-influence character of the downtown area, thereby furthering the intent of this policy.*
- b. The project adheres to the Downtown (D) zone regulations outlined in the Downtown Revitalization Specific Plan, ensuring compliance with the development standards as well as the relevant codes and requirements stipulated in the Corona Municipal Code.*
- c. The project has been reviewed in compliance with the California Environmental Quality Act, and all applicable requirements and procedures of the act have been followed. As justified in Finding # 1, a Mitigated Negative Declaration is recommended for adoption with associated application GPA2024-0003 as the initial study identifies potentially significant effects on the environment, but the project applicant has agreed to revise the project to avoid these significant effects or to mitigate the effects to a point where it is clear that no significant effects would occur.*
- d. The project site is of a sufficient size and configuration to accommodate the design and scale of proposed development, including buildings and elevations, landscaping, parking and other physical features of the proposal, as demonstrated by the project plans for PP2024-0001.*
- e. The architectural design of the proposed development consists of Spanish Colonial architectural elements that are compatible with the character of the surrounding neighborhood. The project will enhance the visual character of the neighborhood by improving vacant undeveloped property and will provide for harmonious, orderly and attractive development of the site.*

BE IT FURTHER RESOLVED that a copy of this Resolution be delivered to the City Clerk of said City and a copy thereof be sent to the applicant therefore at the address of said applicant as set forth in the application for said Precise Plan.

Adopted this 8th day of December, 2025.

Sarah Longwell, Chair
Planning and Housing Commission
City of Corona, California

ATTEST:

Belinda Capilla
Secretary, Planning and Housing Commission
City of Corona, California

I, Belinda Capilla, Secretary to the Planning and Housing Commission of the City of Corona, California, do hereby certify that the foregoing Resolution was regularly introduced and adopted in a regular session of said Planning and Housing Commission duly called and held on the 8th day of December, 2025, and was duly passed and adopted by the following vote, to wit:

AYES:

NOES:

ABSENT:

ABSTAINED:

Belinda Capilla
Secretary, Planning and Housing Commission
City of Corona, California

LOCATIONAL & ZONING MAP



LEGEND

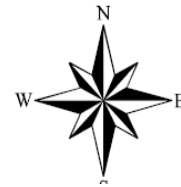
 Project Site

ZONING LEGEND:

- D - Downtown
- SF - Single Family
- GB- Gateway Business



**PP2024-0001
NWC W. 6th & S. Main
Streets**





Project Conditions

City of Corona

Project Number: PP2024-0001

Description: **Northgate Market**

Applied: **8/19/2024**

Approved:

Site Address: **323 S BELLE AVE CORONA, CA 92882**

Closed:

Expired:

Status: **RECEIVED**

Applicant: **NADEL STUDIO ONE, INC.**

Parent Project: **DPR2023-0021**

1990 S. Bundy Drive, Ste. 400 Los Angeles CA, 90025

Details:

LIST OF CONDITIONS

DEPARTMENT	CONTACT
BUILDING	Anthony Hurley
<p>1. The Project shall be designed to show compliance with the latest adopted edition of the following codes as applicable:</p> <ul style="list-style-type: none">A. California Building CodeB. California Residential CodeC. California Electrical CodeD. California Mechanical CodeE. California Plumbing CodeF. California Energy CodeG. California Fire CodeH. California Green Building Standards CodeI. Including local amendments, and the Corona Municipal Code. <p>2. A complete code analysis for the project shall be required. The code analysis shall specify allowable/actual floor areas, allowable/actual height, number of stories, distances to the property line, type of construction, occupancy groups, sprinkler and fire alarm requirements, setbacks, occupant loads, and plumbing fixture requirements.</p> <p>Clearly delineate any frontage used to justify allowable area increases per CBC 506.3. Show any frontage/allowable area increases in code analysis calculations.</p> <p>3. Prior to issuance of building permits, the following items must be completed:</p> <ul style="list-style-type: none">a) Precise grading plans shall be approved,b) Rough grading permit issued and completed,c) Compaction certification,d) Pad elevation certification,e) Rough grade inspection signed off by the Public Work's Inspector. <p>Grading is a separate submittal and is completely independent of the Plan Check for the building(s).</p> <p>4. Project entitlement approvals shall be obtained prior to plan approval and issuance of Building permits. The Tract or Parcel map shall record prior to the issuance of any Building permits. Final Conditions of Approval shall be made part of set of plans [Conditions of approval shall be pasted on plans]</p>	



Project Conditions

City of Corona

BUILDING	Anthony Hurley
<p>5. Project plans shall show a parking summary to include the following:</p> <ul style="list-style-type: none">a) Number of parking spaces required/provided.b) Handicap parking spaces required/provided. (CBC 11A or 11B)c) Number of van accessible parking spaces required/provided.d) Number of EV and EVCS parking spaces required/provided. (CGBSC, sect. 5.106.5.3)e) Number of EVCS van accessible, standard accessible, and ambulatory parking spaces required/provided. (CBC Chapter 11B, section 11B-228.3)f) Bicycle parking facilities required/provided. (CGBSC, sect. 5.106.4)g) Electrical vehicle (EV) charging: medium-duty and heavy-duty (CGBSC, sect. 5.106.5.4) <p>6. Prior to issuance of building permits, applicant shall comply with the Department of Toxic Substances Control (DTSC) document dated November 25, 2025, Re: Mitigated Negative Declaration (MND) for the Northgate Gonzalez Market dated November 17, 2025, State Clearinghouse Number 2025110646.</p> <p>7. Prior to beginning construction, applicant shall obtain a demolition permit for all existing buildings, foundations, and utilities.</p> <p>8. Project shall comply with the current CBC Chapter 11B requirements for accessibility. Applicant shall provide an accessibility site plan showing site circulation.</p> <p>9. Access, sanitary facilities, and parking shall comply with Title 24 Handicap Requirements.</p> <p>10. Construction activity shall not occur between the hours of 8:00 pm to 7:00 am, Monday thru Saturday and 6:00 pm to 10:00 am on Sundays and Federal Holidays.</p> <p>11. Roofing material shall be Class A.</p> <p>12. Exterior walls shall be constructed of the required fire resistance rating in accordance with CBC tables 601 and 705.5.</p> <p>13. Plans for food preparation areas shall be approved by the Riverside County Health Dept. prior to plan check approval from this department. Provide an additional set of plans to the County for review.</p> <p>14. Project plans can be submitted electronically online. If hard copy plans are submitted for plan review, the applicant shall submit five (5) complete sets of plans to include the following - * Plot Plan * Foundation Plan * Floor Plan * Ceiling and roof framing plan * Electrical Plans (electrical service shall be underground per Corona Municipal Code Section 15.06), including size of main switch, number and size of service entrance conductors, circuit schedule and demand load. * Plumbing and sewer plan, isometric, including underground diagram, water piping diagram, sewer or septic tank location, fixture units, gas piping and vents, heating and air conditioning diagram. * Landscape and Irrigation plans; Submit four (4) complete sets detached from building plans. Landscape Maintenance District plans shall be submitted directly to the Planning and Development Department, Development Services Division. Landscape plans shall be approved prior to the issuance of any Building Permits.</p> <p>If hard copy plans are submitted for plan review, the applicant shall submit two (2) sets of structural calculations, energy conservation calculations and soils reports. Architects/Engineers stamp and wet signature is required prior to submittal of plan check.</p> <p>15. Upon tenant improvement plan check submittal there may be additional Building Department requirements.</p> <p>16. Separate permits are required for all fences, walls, signs, and PV systems.</p> <p>17. Comply with the Corona Burglary Ordinance contained in CMC chapter 15.52. Copies are available at the Building Division counter or online at www.CoronaCA.gov/Building.</p> <p>18. All contractors must show proof of State and City licenses, and workmen's compensation insurance to the City prior to the issuance of permits.</p> <p>19. Buildings shall not be occupied prior to posting of Certificate of Occupancy issued by the Building Division.</p> <p>20. An approved Construction Waste Management Plan and Recycling Worksheets shall be kept on site and maintained by the applicant and made available for inspection by City representatives at all times in accordance with the California Green Building Standards Code. Documentation which demonstrates compliance with the minimum recycling of waste materials required by CAL Green shall be provided to the building inspector prior to issuance of Certificate of Occupancy or Final Approval. Please contact Miguel Corral with Waste Management at (818) 266-1117 or mcorral@wm.com for documentation requirements.</p>	



Project Conditions

City of Corona

BUILDING	Anthony Hurley
<p>21. Trash and recycling enclosures shall be accessible per CBC chapter 11B and meet the standards of the City's waste hauler (Waste Management). Provide enlarged plans and construction drawings/details for construction of trash/recycling enclosures.</p> <p>Trash enclosure shall be located on a code-compliant accessible path of travel for all employees.</p> <p>22. All fees, including but not limited to, occupancy fees, property development tax, and school fees must be paid in full prior to issuance of building permits. Any development impact fees that apply to the project must be paid in full prior to NIC or CofO issuance.</p> <p>23. In order to submit building plans for plan review, a permanent or temporary address must be issued for the project by the Development Services Division. Please apply for the address at least 3 weeks prior to submittal to allow for processing.</p> <p>24. 1) Provide Building Addressing and Premises Identification per the Corona Fire Department Addressing Standards.</p> <p>2) Provide permanent rooftop address and illuminated building addressing per the Corona Fire Department addressing standards.</p> <p>25. Provide minimum sized solar PV and battery energy storage system per the California Energy Code. A separate permit is required</p> <p>26. Utilities shall meet the utility authority's service requirements. Applicant shall coordinate the utility locations and design with the respective utility provider.</p> <p>27. The on-site electrical service feeding the building shall be underground per CMC chapter 15.60.</p> <p>28. A pre-construction meeting is required with the building inspector assigned to the project. You may schedule a pre-con meeting by calling 951-736-2250 or emailing Building.Inspection@CoronaCA.gov.</p> <p>29. All deferred features and related permits shall be approved, and the building not be occupied, prior to issuance of CofO. Utilities will not be released until a CofO, TCO, or Temporary Utility Bond has been issued.</p> <p>30. Project site shall be kept clean, secure, safe, and BMPs installed and maintained, and follow all city, state, and county requirements and due standard of care.</p>	<p>21. Trash and recycling enclosures shall be accessible per CBC chapter 11B and meet the standards of the City's waste hauler (Waste Management). Provide enlarged plans and construction drawings/details for construction of trash/recycling enclosures.</p> <p>Trash enclosure shall be located on a code-compliant accessible path of travel for all employees.</p> <p>22. All fees, including but not limited to, occupancy fees, property development tax, and school fees must be paid in full prior to issuance of building permits. Any development impact fees that apply to the project must be paid in full prior to NIC or CofO issuance.</p> <p>23. In order to submit building plans for plan review, a permanent or temporary address must be issued for the project by the Development Services Division. Please apply for the address at least 3 weeks prior to submittal to allow for processing.</p> <p>24. 1) Provide Building Addressing and Premises Identification per the Corona Fire Department Addressing Standards.</p> <p>2) Provide permanent rooftop address and illuminated building addressing per the Corona Fire Department addressing standards.</p> <p>25. Provide minimum sized solar PV and battery energy storage system per the California Energy Code. A separate permit is required</p> <p>26. Utilities shall meet the utility authority's service requirements. Applicant shall coordinate the utility locations and design with the respective utility provider.</p> <p>27. The on-site electrical service feeding the building shall be underground per CMC chapter 15.60.</p> <p>28. A pre-construction meeting is required with the building inspector assigned to the project. You may schedule a pre-con meeting by calling 951-736-2250 or emailing Building.Inspection@CoronaCA.gov.</p> <p>29. All deferred features and related permits shall be approved, and the building not be occupied, prior to issuance of CofO. Utilities will not be released until a CofO, TCO, or Temporary Utility Bond has been issued.</p> <p>30. Project site shall be kept clean, secure, safe, and BMPs installed and maintained, and follow all city, state, and county requirements and due standard of care.</p>
FIRE	Xente Baker
<p>1. Any revised site plan shall be submitted to the Fire Department for screen check approval prior to building plan submittal.</p> <p>2. All projects shall comply with the City of Corona Fire Department Site Construction Standard. A copy of which is available at the coronaca.gov. Projects shall have approved all weather access from two (2) directions and fire hydrants providing the required fire flow tested and accepted prior to combustible construction.</p> <p>3. Dead end access drives shall not exceed one hundred fifty (150) feet in length.</p> <p>4. Provide turn-around for access drive(s) meeting Fire Department standards/approval.</p> <p>5. Applies to exit/entry at 4th St. Can fire apparatus navigate this exit/entry point? Plot turning radii, 25' inside and 50' outside.</p> <p>6. Meet with Corona Fire Department to determine locations of red curbing and signage by fire hydrants, fire department connections, and designated fire lanes on site.</p> <p>7. A Knox Box shall be provided for this business. Applies to the proposed "swing gates" at the rear (North side) of the building. To apply for a Knox product visit https://www.knoxbox.com/</p> <p>8. Provide Class A roofing material on all structures per the Corona Municipal Code.</p> <p>9. Trash enclosures in excess of 1.5 cubic yards shall not be located within five (5) feet of combustible construction or building openings.</p> <p>10. A public safety radio communication study is required for this project. Consult with the fire department for specific requirements for this study or our guideline is available online at coronaca.gov/fire.</p> <p>11. Storage, Use and Dispensing of hazardous materials shall be in accordance with the California Building and Fire Code.</p> <p>12. At no time shall fire hydrants or fire lanes be blocked by building materials, storage, equipment, and/or vehicles.</p> <p>13. Please identify the proposed location of the fire riser lateral.</p>	<p>1. Any revised site plan shall be submitted to the Fire Department for screen check approval prior to building plan submittal.</p> <p>2. All projects shall comply with the City of Corona Fire Department Site Construction Standard. A copy of which is available at the coronaca.gov. Projects shall have approved all weather access from two (2) directions and fire hydrants providing the required fire flow tested and accepted prior to combustible construction.</p> <p>3. Dead end access drives shall not exceed one hundred fifty (150) feet in length.</p> <p>4. Provide turn-around for access drive(s) meeting Fire Department standards/approval.</p> <p>5. Applies to exit/entry at 4th St. Can fire apparatus navigate this exit/entry point? Plot turning radii, 25' inside and 50' outside.</p> <p>6. Meet with Corona Fire Department to determine locations of red curbing and signage by fire hydrants, fire department connections, and designated fire lanes on site.</p> <p>7. A Knox Box shall be provided for this business. Applies to the proposed "swing gates" at the rear (North side) of the building. To apply for a Knox product visit https://www.knoxbox.com/</p> <p>8. Provide Class A roofing material on all structures per the Corona Municipal Code.</p> <p>9. Trash enclosures in excess of 1.5 cubic yards shall not be located within five (5) feet of combustible construction or building openings.</p> <p>10. A public safety radio communication study is required for this project. Consult with the fire department for specific requirements for this study or our guideline is available online at coronaca.gov/fire.</p> <p>11. Storage, Use and Dispensing of hazardous materials shall be in accordance with the California Building and Fire Code.</p> <p>12. At no time shall fire hydrants or fire lanes be blocked by building materials, storage, equipment, and/or vehicles.</p> <p>13. Please identify the proposed location of the fire riser lateral.</p>



Project Conditions

City of Corona

FIRE	Xente Baker
<p>14. Roof top address numerals shall be required. Numerals shall be 3' in height, placed on a contrasting background and located on the "street side" of the building.</p>	
PLANNING	Rocio Lopez
<ol style="list-style-type: none"> 1. The project shall comply with all applicable requirements of the Downtown Revitalization Specific Plan and the Corona Municipal Code (CMC), including the payment of all required fees. 2. The applicant or his successor in interest shall comply with the project's Mitigation Monitoring and Reporting Program established in the Mitigated Negative Declaration prepared for the project. 3. To the fullest extent permitted by law, the applicant shall defend, indemnify and hold the City of Corona and its directors, officials, officers, employees, volunteers and agents free and harmless from any and all claims, demands, causes of action, proceedings, costs, expenses, liabilities, losses, damages or injuries of any kind, in law or equity, in any manner arising out of, pertaining to, or incident to any attack against or attempt to challenge, set aside, void or annul any approval, decision or other action of the City of Corona, whether such approval, decision or other action was by its City Council, Planning and Housing Commission or other board, director, official, officer, employee, volunteer or agent. To the extent that Government Code Section 66474.9 applies, the City will promptly notify the applicant of any claim, action or proceeding made known to the City to which Government Code Section 66474.9 applies and the City will fully cooperate in the defense. The Applicant's obligations hereunder shall include, without limitation, the payment of any and all damages, consultant and expert fees, and attorney's fees and other related costs and expenses. The City shall have the right to retain such legal counsel as the City deems necessary and appropriate. 4. Nothing herein shall be construed to require City to defend any attack against or attempt to challenge, set aside, void or annul any such City approval, decision or other action. If at any time Applicant chooses not to defend (or continue to defend) any attack against or attempt to challenge, set aside, void or annul any such City approval, decision or other action, the City may choose, in its sole discretion, to defend or not defend any such action. In the event that the City decides not to defend or continue the defense, Applicant shall be obligated to reimburse City for any and all costs, fees, penalties or damages associated with dismissing the action or proceeding. If at any time both the Applicant and the City choose not to defend (or continue to defend) any action noted herein, all subject City approvals, decisions or other actions shall be null and void. The Applicant shall be required to enter into any reimbursement agreement deemed necessary by the City to effectuate the terms of this condition. 5. Approval of this precise plan shall be utilized within two (2) years of the approval date, unless some other period is established by the decision-making entity at the time the permit is approved. The permit must be utilized, or if the permit involves a building, construction must be commenced and carried on diligently to completion of at least one usable unit, prior to the expiration of the time limit. If the time limit expires, any privilege, permit or variance granted shall be deemed to have lapsed. 6. All signage shall be architecturally integrated with the overall project, and the sign program shall meet the Downtown Revitalization Specific Plan and Chapter 17.74 of the Corona Municipal Code requirements. The sign program shall be submitted to the Planning and Development Department's Planning Division for separate review and approval. A separate sign permit shall then be obtained from the Planning and Building Divisions prior to the installation of any signs. <p>Additionally:</p> <ol style="list-style-type: none"> a. No internally or externally illuminated building signage shall be installed on the west elevation of the Northgate building, as residential properties are located directly across the street. b. The monument sign located at the project entrance on Belle Avenue shall be limited to external illumination. However, if the sign is relocated to another area along Belle Avenue where it is not directly across from residential properties, internal illumination may be considered subject to the review and approval of the Planning & Development Department. c. No monument signs shall be placed in the public right-of-way. <ol style="list-style-type: none"> 7. All landscape plans shall be prepared by a licensed professional. Plans shall be prepared in accordance with the Downtown Revitalization Specific Plan and the city's Landscape Design Guidelines for Commercial Developments and the requirements of the CMC, Chapter 17.70 (available on the city's website at: https://www.coronaca.gov/government/departments/community-development/planning-division); and the State of California Model Water Efficiency Landscape Ordinance (MWEL0). 	



Project Conditions

City of Corona

PLANNING

Rocio Lopez

8. Landscape plans are required for the project and shall be submitted as a separate submittal for plan check to the Building Division. Landscape plans must also be submitted prior to issuance of a building permit.

At time of plan check submittal, the developer shall also submit a landscape deposit in the amount of \$5,000 to the Planning Division for landscape plan check and inspection services which will be completed by a landscape consultant. The deposit will cover on-site landscaping and landscaping within parkways. This fee is separate from the Building Division's landscape plan check submittal fee. Any unused deposit funds will be reimbursed to the developer upon completion of the project.

9. Prior to the issuance of a building permit, applicant shall submit to the Building Division a separate wall and fence plan. Wall and fence plan shall include anti-graffiti coating or protection for the exterior side of all perimeter walls.

The owner, or his/her successor, shall remove any graffiti on the property as soon as possible. In addition, if the applicant was notified by the City, the applicant shall remove the graffiti within seven (7) days of the City's notice.

10. All landscaping (on-site and off-site) and perimeter walls/fencing shall be installed prior to issuance of a Certificate of Occupancy.
11. PP2024-0001 is contingent upon the approval of GPA2024-0001, SPA2024-0001 and PM 38981.
12. Prior to the issuance of any building permit, Parcel Map 38981 shall be recorded.
13. At plan check, provide photometric plan. Lighting shall ensure the project meets the criteria of CMC Section 17.84.070. Outdoor lighting is required to be directed downward and shielded to minimize off-site spillover onto adjacent residential and sensitive land uses.
14. Prior to the start of construction, the developer shall place a sign on the property that is visible from S. Main Street that provides a point of contact and related contact information during construction. The sign shall also clearly state that construction operations, including building related activities and deliveries, shall be restricted to Monday through Saturday from 7:00 a.m. to 8:00 p.m. and from 10:00 a.m. to 6:00 p.m. on Sundays and federal holidays, in accordance with the CMC Section 17.84.040(D)(2).
15. All mechanical units, including air conditioning units, shall comply with the stationary noise standards in CMC Section 17.84.040 (C)(2).
16. All rooftop mechanical equipment shall be screened from ground view behind a parapet wall or other rooftop screen wall as approved by the Planning & Development Department.
17. This project is subject to Riverside County's Multiple Species Habitat Conservation Plan (MSHCP) fee, which is calculated and collected at the time building permits are issued.
18. Prior to the commencement of on-site grading, the applicant shall notify residents along Belle Avenue and directly to the north of the project site of the anticipated grading and construction schedule. Notification may be provided by mailed letters or notices placed in residents' mailboxes. The notice shall include the estimated grading and construction timeline, the City's permitted construction hours per Section 17.84.040(D)(2) of the Corona Municipal Code, and the names with contact information (phone number and email address) for the project's construction superintendent and developer for inquiries or complaints. Prior to distribution, the applicant shall submit the proposed notice to the Project Planner in the Planning and Development Department for review and approval.
19. Prior to the issuance of a grading permit, the applicant shall retain the services of a licensed vector or pest control operator to implement a rodent control program on-site prior to the commencement of grading activities. The applicant shall provide documentation, such as an executed service agreement between the applicant and the licensed vector or pest control operator, to the Project Planner with the Planning & Development Department as evidence for compliance with this condition.
20. The applicant shall implement all necessary measures to reduce and control construction-related dust from migrating onto surrounding residential properties. These measures shall include, but are not limited to, frequent water spraying of the site, covering material stockpiles, and installing dust-control wind fencing ("dust tamers") along the perimeters of the project site for the duration of construction. In addition, all construction staging areas shall be located as far away from nearby residences as feasible.



Project Conditions

City of Corona

PLANNING	Rocio Lopez
<p>21. The applicant shall install and maintain the appropriate electrical outlets at the loading docks to minimize truck idling noise. Upon arrival, all delivery trucks, if properly equipped, - including those equipped with Transport Refrigeration Units (TRUs) – shall immediately shut down their engines and, when needed, connect to the provided electrical outlets in lieu of idling. The applicant shall install a “No Truck Idling” sign at the truck loading area. The electrical outlets shall be shown on the applicant’s construction plans for the Northgate building. [ADDED BY THE PLANNING AND HOUSING COMMISSION ON DECEMBER 8, 2025]</p> <p>22. Prior to commencement of on-site grading, the applicant shall conduct community outreach with the residents along Belle Avenue (between Sixth Street and Third Street) and along Washburn Avenue (between Fourth Street to Third Street) on the project. Outreach may be completed through written, bilingual (English and Spanish), notification such as mailed letters, flyers, or notices placed in residents’ mailboxes. The notification shall include, at a minimum, the project site plan and contact information for a Northgate representative (name, phone number, and email). Prior to distribution, the applicant shall provide a copy of the bilingual notice and the list of addresses to the Project Planner with the Planning & Development Department as evidence for compliance with this condition. [ADDED BY THE PLANNING AND HOUSING COMMISSION ON DECEMBER 8, 2025]</p>	
PUBLIC WORKS	Maria Miranda
<ol style="list-style-type: none"> 1. The Public Works, Planning and Development, and Utilities Department conditions of approval for the subject application shall be completed at no cost to any government agency. All questions regarding the intent of the conditions shall be referred to the Planning and Development Department, Development Services Division. Should a conflict arise between City of Corona standards and design criteria and any other standards and design criteria, City of Corona standards and design criteria shall prevail. 2. The developer shall comply with the State of California Subdivision Map Act and all applicable City ordinances and resolutions. 3. Prior to issuance of grading permit, the applicant shall demonstrate to the satisfaction of the City Engineer that the proposed subdivision will not unreasonably interfere with the use of any easement holder of the property. 4. All improvement and grading plans shall be drawn on twenty-four (24) inch by thirty-six (36) inch Mylar and signed by a registered civil engineer or other registered/licensed professional as required. 5. The submitted site plan shall correctly show all existing easements, traveled ways, and drainage courses. Any omission or misrepresentation of these documents may require said site plan to be resubmitted for further consideration. 6. In the event that off-site right-of-way or easements are required for the City of Corona master plan facilities to comply with these conditions of approval, the developer is required to secure such right-of-way or easements at no cost to the City. 7. All existing and new utilities adjacent to and on-site shall be placed underground in accordance with City of Corona ordinances. 8. Prior to issuance of a Certificate of Occupancy, the developer shall cause the engineer of record to submit project base line work for all layers in AutoCAD DXF format on Compact Disc (CD) to the Planning and Development Department, Development Services Division. If the required files are unavailable, the developer shall pay a scanning fee to cover the cost of scanning the as-built plans. 9. The developer shall monitor, supervise and control all construction and construction related activities to prevent them from causing a public nuisance including, but not limited to, insuring strict adherence to the following: <ol style="list-style-type: none"> a) Removal of dirt, debris or other construction material deposited on any public street no later than the end of each working day. b) Construction operations, including building related activities and deliveries, shall be restricted to Monday through Saturday from 7:00 a.m. to 8:00 p.m., excluding holidays, and from 10:00 a.m. to 6:00 p.m. on Sundays and holidays, in accordance with City Municipal Code 15.04.060, unless otherwise extended or shortened by the City Engineer or Building Official. c) The construction site shall accommodate the parking of all motor vehicles used by persons working at or providing deliveries to the site. <p>Violation of any condition or restriction or prohibition set forth in these conditions shall subject the owner, applicant, developer or contractor(s) to remedies as noted in the City Municipal Code. In addition, the City Engineer or Building Official may suspend all construction related activities for violation of any condition, restriction or prohibition set forth in these conditions until such time as it has been determined that all operations and activities are in conformance with these conditions.</p>	



Project Conditions

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10. Prior to issuance of a building permit, the developer shall finish the construction or post security guaranteeing the construction of all public improvements. Said improvements shall include, but are not limited to, the following:
 - a) All missing or deficient street facilities along S Main Street, W Sixth Street, S Belle Avenue, S Washburn Avenue and alley ways. Including but not limited to the widening of S Main St along the project frontage; driveway, sidewalk, and parkway improvements along the frontage of W Sixth St; street, sidewalk, and parkway improvements along the frontage of S Belle Av and S Washburn Av; and access improvements to facilitate the flow of traffic from the alley between APNs 117-103-005 and 117-103-008.
 - b) All required demolition and relocation of existing utilities and facilities in the areas of W Fourth St, W Fifth St, and the alleyways to be abandoned.
 - c) All public drainage facilities.
 - d) All required grading, including erosion control.
 - e) All required sewer and water facilities.
 - f) All required landscaping facilities.
 - g) All under grounding of overhead utilities, except for cables greater than 32k volts.
 - h) All improvements recommended by the Traffic Impact Analysis Report.
 - i) All required traffic signal improvements per latest city standards; connectivity to the Traffic Management Center, and timing plans for signal coordination with W Sixth Street at the S Main Avenue intersection.
 - j) Micro transit stop spaces.
11. The applicant shall return to the possession of the city all above ground city-owned facilities removed with the project including street lights, utility/ meter boxes, fire hydrants, air/ vacs, etc., unless otherwise approved by the City Engineer. The applicant shall return the facilities to the city corporation yard or as directed by the city public works inspector.
12. No decorative pavement or private signs shall be installed in the public right-of-way unless otherwise approved by the City Engineer.
13. Prior to approval of improvement plans, the developer shall ensure no private improvements are located within public right of way, unless otherwise approved by the City Engineer.
14. Prior to approval of improvement plans, the developer shall vacate the portion of W Fifth Street that abuts to 203 W Sixth Street (APN 117-144-002) and secure permission to construct and maintain the proposed private driveway, drive aisle, and on-site sidewalk improvements. If the developer is unable to secure permission, then the developer shall enter into an agreement with the City, to the satisfaction of the Planning & Development Director and City Attorney, to maintain said improvements, and will indemnify the City from any incidents resulting from the said improvements. The agreement shall record against Parcel 1.
15. Prior to the vacation of any street or alley right-of-way, the developer shall reserve easements or grant additional easement(s) by separate instrument for existing utilities. The developer shall coordinate all reservations or separate instruments with third party utilities providers.
16. All the grading design criteria shall be per City of Corona standards, Corona Municipal Code Title 15 Chapter 15.36 and City Council Ordinance Number 2568, unless otherwise approved by the City Engineer.
17. Prior to approval of grading plans, the applicant shall submit two (2) copies of a soils and geologic report prepared by a Registered Engineer to the Planning and Development Department, Development Services Division. The report shall address the soil's stability and geological conditions of the site. If applicable, the report shall also address: deep seated and surficial stability of existing natural slopes; modified natural slopes which are subject to fuel zones; manufactured slopes and stability along proposed daylight lines; minimum required setbacks from structures; locations and length of proposed bench drains, sub-drains or french drains; and any other applicable data necessary to adequately analyze the proposed development.
18. Prior to approval of grading plans, erosion control plans and notes shall be submitted and approved by the Planning and Development Department, Development Services Division.
19. Prior to approval of grading plans, the applicant shall obtain a General Construction Activity Storm Water Permit from the State Water Resources Control Board in compliance with National Pollutant Discharge Elimination System (NPDES) requirements. Proof of filing a Notice of Intent (NOI) will be required by the City. The WDID # shall be displayed on the title sheet of the grading plans.



Project Conditions

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PUBLIC WORKS	Maria Miranda
<ol style="list-style-type: none">20. Prior to approval of grading plans, the applicant shall comply with the Federal Clean Water Act and shall prepare a Storm Water Pollution Prevention Plan (SWPPP). The SWPPP shall be available at the project site for review.21. Prior to the issuance of a grading permit the developer shall submit recorded easements, agreements, or written letters of permission from adjacent landowners in all areas where grading is proposed to take place outside of the project boundaries.22. Prior to issuance of building permits, the developer shall cause the civil engineer of record and soils engineer of record for the approved grading plans to submit pad certifications and compaction test reports for the subject lots where building permits are requested.23. Prior to release of grading security, the developer shall cause the civil engineer of record for the approved grading plans to submit a set of as-built grading plans with respect to Water Quality Control facilities.24. Prior to issuance of any grading permit, any environmental Phase I and Phase II findings and recommended actions to remove contamination resulting from previous use of the subject site shall be implemented.25. Prior to grading permit issuance, the applicant shall provide confirmation of how the project complies with the requirements and recommendations of the Department of Toxic Substance Control (DTSC) letter dated November 25, 2025. DTSC recommends all imported soil/fill material be tested to ensure all Constituents of Concern (COCs) meet screening levels as outlined in DTSC's PEA Guidance Manual. Furthermore, DTSC advises referencing the DTSC Information Advisory Clean Imported Fill Material Fact Sheet if importing fill is necessary. To minimize the possibility of introducing contaminated soil/fill material there should be documentation of the origins of the soil/fill material and, if applicable, sampling be conducted to ensure that the imported soil/fill material are suitable for the intended land use. The soil sampling should include analysis based on the source of the soil/fill and knowledge of prior land use. Soils testing requirements and oversight shall be coordinated with the Riverside County Department of Environmental Health unless otherwise approved by the City Engineer.26. All City of Corona NPDES permit requirements for NPDES and Water Quality Management Plans (WQMP) shall be met per Corona Municipal Code Title 13 Chapter 13.27 and City Council Ordinance Numbers 2291 and 2828 unless otherwise approved by the City Engineer.27. Prior to the issuance of a grading permit, a Final WQMP, prepared in substantial conformance with the approved Preliminary WQMP, shall be submitted to the Planning and Development Department, Development Services Division for approval. Prior to approval of the final WQMP, the applicant shall ensure that all Drainage Management Areas (DMAs) including those areas flowing offsite comply with the approved Guidance Document for the Santa Ana Region of Riverside County. All modular wetland systems shall have a vegetative top unless otherwise approved by the City Engineer. Upon its final approval, the applicant shall submit one copy on a CD-ROM in PDF format.28. Prior to the issuance of the first Certificate of Occupancy, the applicant shall record Covenants, Conditions and Restrictions (CC&R's) or enter into an acceptable maintenance agreement with the City to inform future property owners to implement the approved WQMP.29. Prior to issuance of the first Certificate of Occupancy, the applicant shall provide proof of notification to the future occupants of all non-structural BMPs and educational and training requirements for said BMPs as directed in the approved WQMP.30. Prior to issuance of Certificate of Occupancy, the applicant shall ensure all structural post construction BMPs identified in the approved project specific Final WQMP are constructed and operational.31. All the drainage design criteria shall be per City of Corona standards and the Riverside County Flood Control and Water Conservation District standards unless otherwise approved by the City Engineer.32. Prior to approval of any improvement plans, the applicant shall submit a detailed hydrology study. Said study shall include the existing, interim and the ultimate proposed hydrologic conditions including key elevations, drainage patterns and proposed locations and sizes of all existing and proposed drainage devices. The hydrology study shall present a full breakdown of all the runoff generated on- and off-site.	



Project Conditions

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33. Prior to approval of improvement plans, the improvement plans submitted by the applicant shall address the following: The project drainage design shall be designed to accept and properly convey all on- and off-site drainage flowing on or through the site. The project drainage system design shall protect downstream properties from any damage caused by alteration of drainage patterns such as concentration or diversion of flow. The project shall drain toward an approved water quality or drainage facility. Once onsite drainage has been treated it may continue into an approved public drainage facility or diverted through under-sidewalk parkway drains. Where feasible, discharge of the site into the public right-of-way shall be designed to allow for future storm drain connection(s).
34. Street design criteria and cross sections shall be per City of Corona standards, approved Specific Plan design guidelines and the State of California Department of Transportation Highway Design Manual unless otherwise approved by the City Engineer.
35. Prior to approval of improvement plans, the applicant shall offer for dedication all required street rights-of-way including 15-feet along S Main Street, 8-feet along W Sixth Street, and the corner cut-off of W Sixth St and S Belle Av. unless otherwise approved by the City Engineer. Dedications along S Main St and W Sixth St shall be made in fee unless otherwise approved by the City Engineer. Any additional street right-of-way dedications shall be by separate instrument. All dedications shall be free of all encumbrances and approved by the City Engineer.
36. Prior to approval of improvement plans, the applicant shall grant a minimum 20 foot-wide ingress and egress easement for public access from the alley between APNs 117-103-005 and 117-103-008 to a public street.
37. Prior to approval of the precise grading plans, the developer shall coordinate with Waste Management for operation and access via the proposed ingress and egress access easement.
38. Prior to approval of improvement plans, the improvement plans submitted by the applicant shall include the following:
 - a) All driveways shall conform to the applicable City of Corona standards and shall be shown on the street improvement plans.
 - b) All street intersections shall be at ninety (90) degrees or as approved by the City Engineer.
 - c) Under grounding of existing and proposed utility lines.
 - d) Street lights.
 - e) All other public improvements shall conform to City of Corona standards.
39. Prior to approval of improvement plans, the applicant shall ensure that street lights along the project frontage conform to Standard 500 unless otherwise approved by the City Engineer and City Traffic Engineer. This includes ensuring adequate lighting at T-intersections and Cul-de-sacs.
40. Prior to approval of improvement plans, all street name signs shall be approved by the City Engineer.
41. Prior to approval of improvement plans, the improvement plans shall show Belle Avenue to be improved to half width plus ten (10) additional feet; improvements for Washburn Avenue shall show the bulb to be improved fully; improvements for S Main Street shall include half width and extend to the raised median, and shall improve the full intersection of W Fifth Street and S Main Street, unless otherwise approved by the City Engineer. At the discretion of the applicant, the existing pavement maybe cored to confirm adequate section and R values during the design process and any findings shall be incorporated into the project design. Therefore improvements may include full pavement reconstruction, grind and overlay, or slurry seal. All striping shall be replaced in kind.
42. Prior to release of public improvement security, the developer shall cause the civil engineer of record for the approved improvement plans to submit a set of as-built plans for review and approval by the Planning and Development Department, Development Services Division.
43. Prior to acceptance of improvements, the City Engineer may determine that aggregate slurry, as defined in the Standard Specifications for Public Works Construction, may be required one year after acceptance of street(s) by the City if the condition of the street(s) warrant its application. All striping shall be replaced in kind. The applicant is the sole responsible party for the maintenance of all the improvements until said acceptance takes place.



Project Conditions

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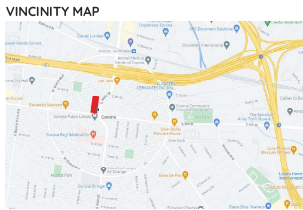
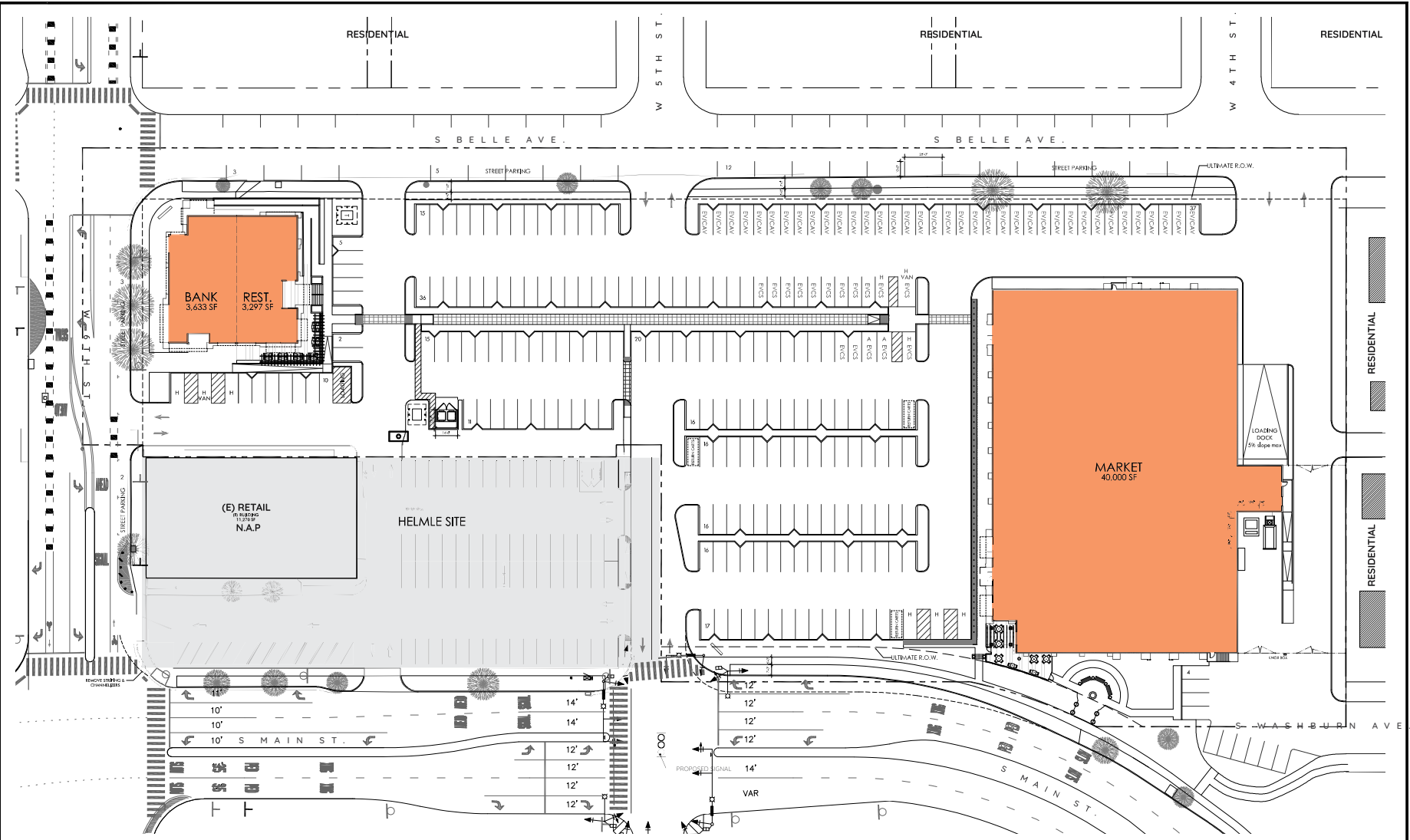
44. Prior to Certificate of Occupancy, the developer shall comply with the approved traffic study recommendations, including but not limited to:
 - a) Install a traffic signal at S Main Street and W Fifth Street per city standards.
 - b) Widen S Main Street and restripe the north leg of the intersection to provide an exclusive southbound right turn lane with a minimum of 100 feet of storage.
 - c) Modify the existing median along S Main Street to provide a southbound left turn lane with a minimum of 100 feet of storage.
45. Prior to approval of the improvement plans, the developer shall provide 2 designated ADA compliant parking spaces for a micro transit stop. The location and requirements for the micro transit stop spaces are subject to the review and approval of the Public Works Department and Transit Program Manager.
46. Prior to issuance of a building permit and/or issuance of a Certificate of Occupancy, the applicant shall pay all development fees, including but not limited to Development Impact Fees (DIF) per City Municipal Code 16.23 and Transportation Uniform Mitigation Fees (TUMF) per City Municipal Code 16.21. Said fees shall be collected at the rate in effect at the time of fee collection as specified by the current City Council fee resolutions and ordinances.
47. Prior to building permit issuance the applicant shall provide all of the necessary documents and fees needed to annex this project into a City of Corona Community Facilities District (CFD) 2016-3 (Maintenance Services). All assessable parcels therein shall be subject to annual CFD charges (special taxes or assessments). The developer shall be responsible for all costs incurred during annexation into the CFDs.
48. Prior to issuance of a Certificate of Occupancy, all proposed parkway, slope maintenance, and/or median landscaping specified in the site plans or in these Conditions of Approval shall be constructed.
49. Prior to the issuance of a Certificate of Occupancy, any damage to existing landscape easement areas due to project construction shall be repaired or replaced by the developer, or developer's successors in interest, at no cost to the City of Corona.
50. All the potable water, reclaimed water, and sewer design criteria shall be per City of Corona Utilities Department standards, design policy and Riverside County Department of Health Services Standards unless otherwise approved by the City Engineer and Utilities Director.
51. Prior to issuance of any building permits, a domestic water and fire flow system shall be approved by the Utilities Department and constructed by the developer, to the satisfaction of the Utilities Director and Fire Chief.
52. Prior to approval of improvement plans, the developer shall construct or guarantee the replacement and relocation of the existing 6-inch sewer main in the Alley Way between W Sixth Street and W Fourth Street with an 8-inch diameter VCP sewer line per City of Corona Utilities Department Standard Plans and Specifications unless otherwise approved by the Utilities Department. Sewer laterals shall not enter manholes unless otherwise approved by the Utilities Department.
53. Prior to building permit issuance, the developer shall construct or guarantee the abandonment of the existing water line in the alley between APNs 117-144-017 and 117-144-002, the abandonment of the water line in W Fifth Street, and the relocation of water services per the Utilities Department Standards, Design Policy, and Specifications or as otherwise approved by the Utilities Department. The Design Policy requires commercial water services be 2-inch minimum.
54. Prior to building permit issuance, the developer shall construct or guarantee the replacement and relocation of the existing 8-inch water main in existing W Fourth Street from S Belle Avenue to S Washburn Avenue with an 8-inch ductile iron pipe per City of Corona Utilities Department Standard Plans, Design Policy, and Specifications, or as required per the recommendations in the approved final water study. The water main and appurtenant facilities shall be located within a water easement free of structures and parking spaces to the satisfaction of the Utilities Department.
55. Prior to issuance of the first Certificate of Occupancy, all weather access road(s) shall be provided to all sewer manholes not located within public right-of-way.
56. Prior to improvement plans approval, the applicant shall ensure that all water meters, fire hydrants or other water appurtenances shall not be located within a drive aisle or path of travel.
57. Prior to issuance of any building permits, the developer shall pay all water and sewer fees, including but not limited to connection fees, wastewater treatment fees, sewer capacity fees and all other appropriate water and sewer fees.



Project Conditions

City of Corona

PUBLIC WORKS	Maria Miranda
<p>58. Prior to approval of improvement plans, the applicant shall submit a final potable water study prepared by a registered civil engineer to the Planning and Development Department, Development Services Division for review and approval. The study shall analyze the existing and proposed water facilities. Results of the system analysis may require special construction for the potable water, such as upsizing downstream water lines, installing pressure regulators, booster pumps, special material for pipeline construction, backwater valves and construction of other appurtenances as necessary to serve the proposed development. Effects of the proposed development, engineering analysis and special construction requirements shall be submitted for review and approval by the Planning and Development Department, Development Services Division, and the Utilities Department. The applicant shall comply with all requirements in the approved Water Study including the construction of offsite facilities needed to serve the project.</p> <p>59. Prior to building permit issuance, the applicant shall construct or guarantee the construction of all required public improvements including but not limited to, the potable water line, sewer line, potable water services, sewer laterals, double detector check assemblies and reduced pressure principle assemblies within the public right of way and-or easements.</p> <p>60. Prior to building permit issuance, the applicant shall dedicate easements for all public water and sewer facilities needed to serve the project in accordance the Utilities Department standards. The minimum easement width shall be 20 feet for one utility and 30 feet for more than one public utility facility. All public water and sewer facilities shall be provided a minimum 20 foot wide paved access road unless otherwise approved by the Utilities Director. Structures and trees shall not be constructed or installed within a public utility easement. The applicant shall vacate all city utility easements determined by the Utilities Department to be excess due to relocation or abandonment of facilities.</p> <p>61. Prior to building permit issuance, the applicant shall construct or guarantee the construction of a private fire system with double detector check assemblies at all public fire services to the satisfaction of the Utilities Department and Fire Chief.</p> <p>62. Fire Hydrants shall be a maximum 250-300 feet apart or as directed by the Fire Department.</p> <p>63. Manhole rim elevations shall be lower than all pad elevations immediately downstream. Otherwise a back flow prevention valve will be required.</p> <p>64. Static pressures exceeding 80 psi require an individual pressure regulator.</p> <p>65. The applicant shall provide a separate irrigation water service for all CFD landscaped lots or easements.</p> <p>66. The landscape plans of all parkway and Landscape Maintenance District (LMD) and Community Facilities District (CFD) lots shall be prepared by a licensed Landscape Architect and shall be submitted to the City for review and approval.</p> <p>67. When conflicts arise between these written conditions of approval and the applicant's plans, the written conditions of approval shall govern unless otherwise approved by the City Engineer, Utilities Director, and/ or Planning and Development Director.</p> <p>68. Reclaimed water shall be used for any construction activity, unless otherwise approved by the Utilities Director or their designee. Prior to obtaining a reclaimed construction meter from the City, a Reclaimed Water Application shall be submitted for the contractor to receive certification to handle reclaimed water.</p>	



SITE SUMMARY

GENERAL PLAN DESIGNATION	MUD (MIXED USE DOWNTOWN)	CONSTRUCTION	REST/SHOPS	VB FULLY SPRINKLERED
ZONE DESIGNATION	D - COMMERCIAL	MARKET	IBB FULLY SPRINKLERED	
SPECIFIC PLAN	SP-98-1			
SITE AREA	212,575 SF 4.88 ac.			
BUILDING	REST + BANK 6,950 SF MARKET 40,000 SF TOTAL 46,950 SF			
FAR	0.22			
LOT COVERAGE	22.1%			

RETAIL + PARKING

MARKET	40,000 SF	TOTAL BUILDING AREA	46,950 SF
Total Required @1/200	200 stalls	Site Parking Provided	256 stalls
Van Accessible provided	2 stalls	Street Parking Provided	25 stalls
Standard Accessible provided	4 stalls	TOTAL PARKING REQUIRED	258 stalls
		TOTAL PARKING PROVIDED	281 stalls
		PARKING RATIO	5.69/1000
REST/BANK	6,950 SF		
Rest + 3,297 SF @1/100	33 stalls		
Bank + 3,653 SF @1/200	18 stalls		
Rest: Patio + 650 SF @1/100	7 stalls		
Total Required	58 stalls		
Van Accessible	1 stalls		
Standard Accessible	2 stalls		

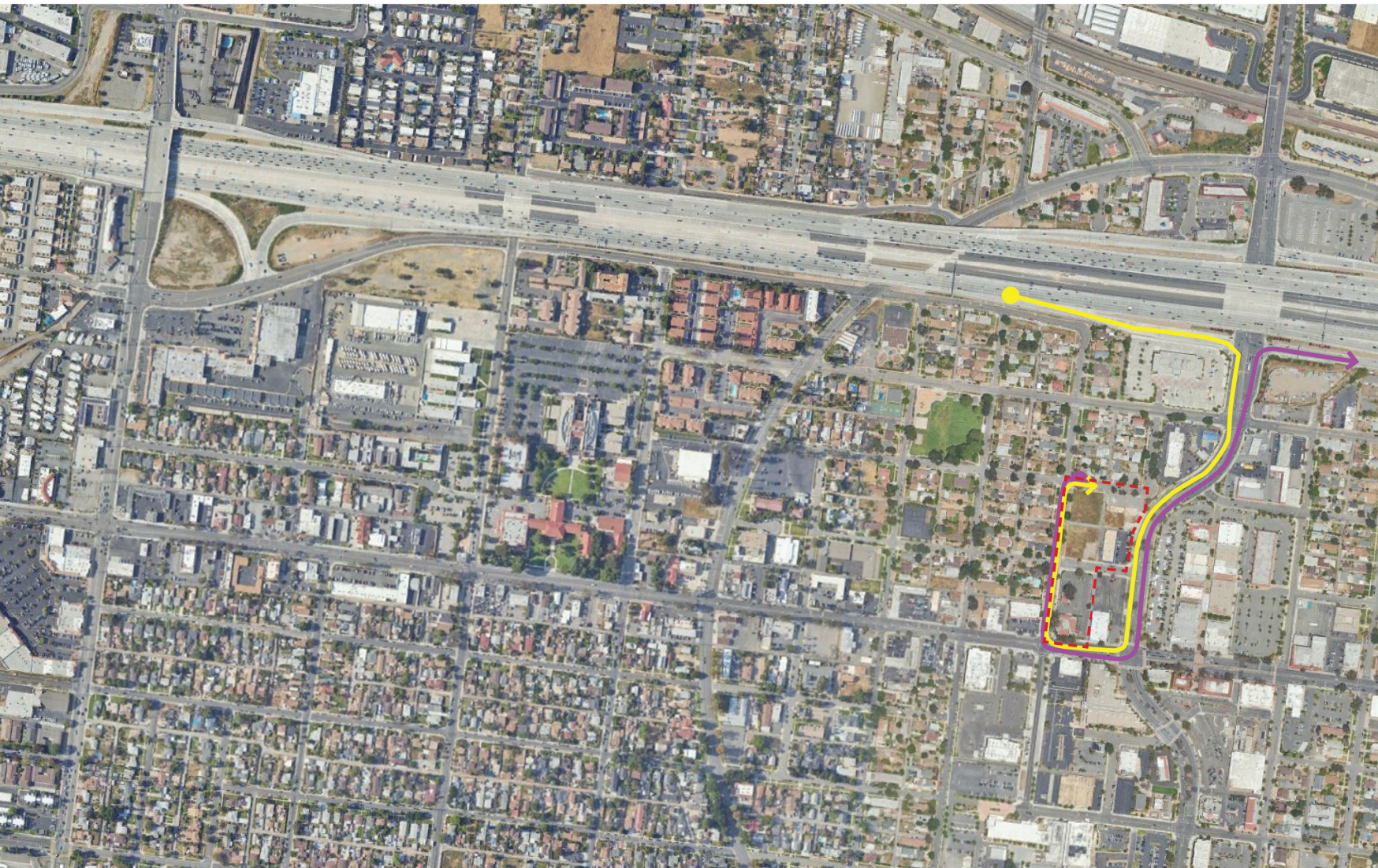
EV PARKING

SITE REQUIREMENTS	
EV Charging	13 stalls
Standard EV	8 stalls
Standard Accessible EV	2 stalls
Ambulatory EV	2 stalls
Van Accessible EV	1 stalls
EV Capable/CAV	39 stalls
Req. 30% of 258 stalls	52 stalls

PROPOSED SITE PLAN

SCALE: 1" = 30'-0"



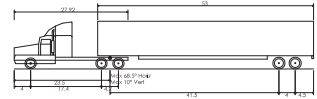
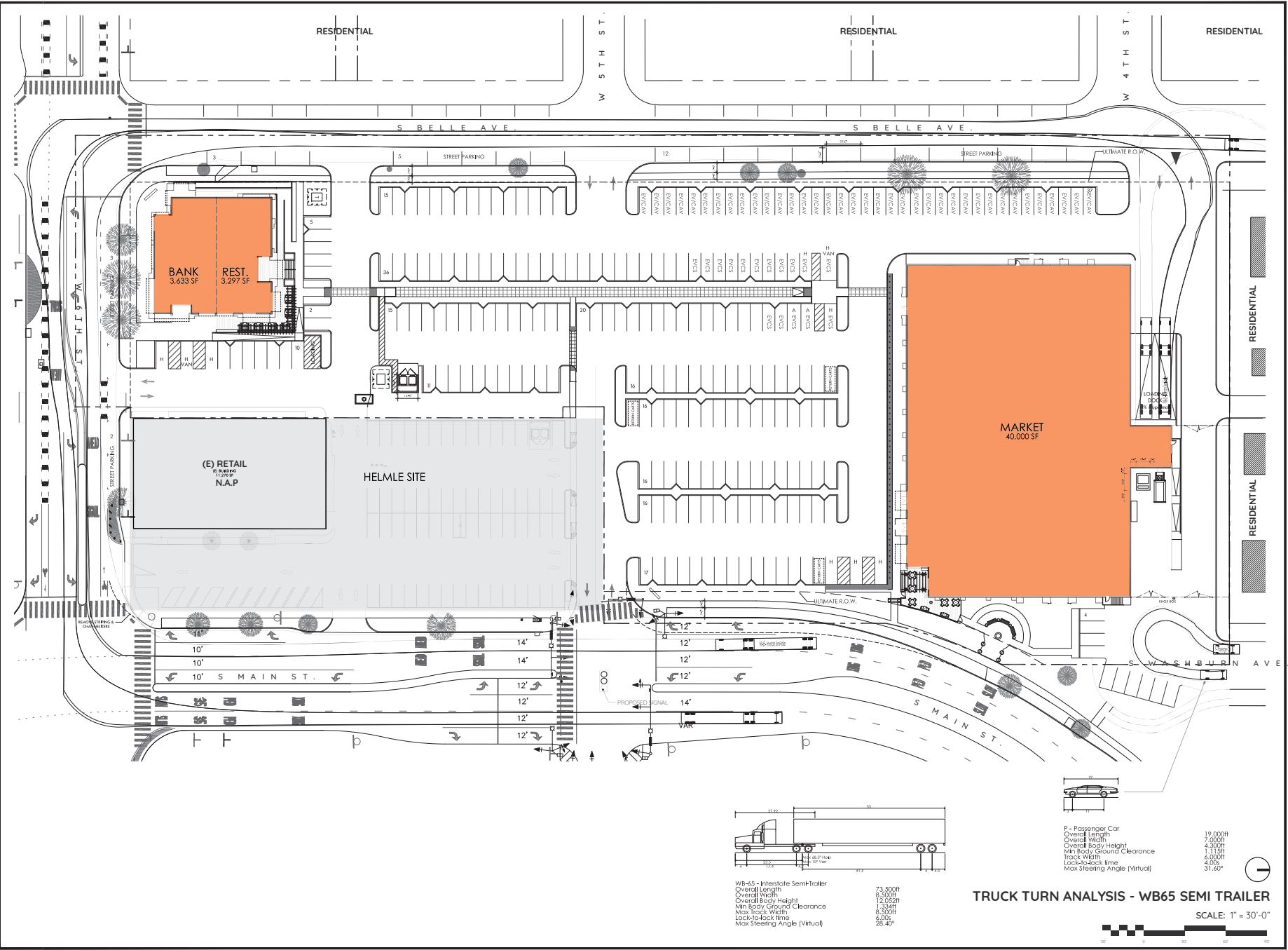


LEGEND

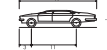
- PROJECT SITE 
- ROUTE TO SITE 
- ROUTE FROM SITE 



EXHIBIT 5



WB-65 - Interstate Semi-Trailer
 Overall Length 73.500ft
 Overall Width 8.500ft
 Overall Body Height 12.000ft
 Min Body Ground Clearance 1.334ft
 Max Track Width 8.900ft
 Lock-to-Lock time 6.00s
 Max Steering Angle (Virtual) 28.40°

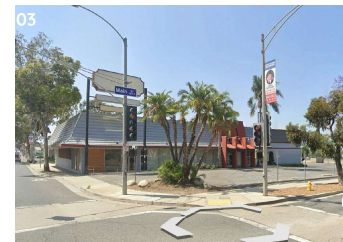
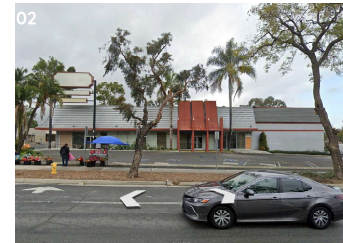
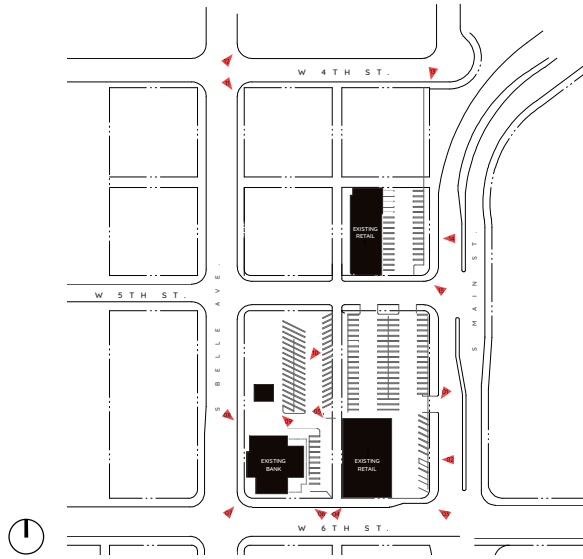


P - Passenger Car
 Overall Length 19.000ft
 Overall Width 7.000ft
 Overall Body Height 4.300ft
 Min Body Ground Clearance 1.115ft
 Track Width 4.900ft
 Lock-to-Lock time 4.00s
 Max Steering Angle (Virtual) 31.80°

TRUCK TURN ANALYSIS - WB65 SEMI TRAILER

SCALE: 1" = 30'-0"





NADEL

NADEL STUDIO ONE INC

1990 S. BUNDY DRIVE
SUITE 400
LOS ANGELES, CA 90025
T. 310.826.2100
F. 310.826.0182
WWW.NADELARC.COM

PROFESSIONAL STAMP(S):

CONSULTANT:

CLIENT:
NORTHGATE MARKETS

1201 N. MAGNOLIA AVENUE
ANAHEIM, CA 92801

PROJECT:
NWC 6TH AND MAIN

TBD - 6TH AND MAIN
CORONA, CALIFORNIA

PUBLIC AGENCY SUBMITTAL:
ISSUED FOR BIDDING: N/A
ISSUED FOR CONSTRUCTION: N/A

REVISIONS:
ISSUE DESCRIPTION: ISSUE DATE:
NO.: DATE:

DPR2023-0021

NADEL PROJECT NO: 19010

PROJECT DATE: 02.15.2024

DRAWING TITLE:

PHOTOS OF SITE
AND SURROUNDING
AREAS

A-4.0

DEVELOPMENT PLAN REVIEW

EXHIBIT 7



VIEW 1 - MARKET VIEW

- LOOKING TOWARDS NORTH WEST FROM PLAZA ENTRANCE ON MAIN STREET

EXHIBIT 8

NADEL

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ANAHEIM, CA 92801

PROJECT:

NWC 6TH AND MAIN

TBD - 6TH AND MAIN
CORONA, CALIFORNIA

PUBLIC AGENCY SUBMITTAL:

ISSUED FOR BIDDING: N/A

ISSUED FOR CONSTRUCTION: N/A

REVISIONS:

ISSUE NO. DESCRIPTION: ISSUE DATE:

DPR2023-0021

NADEL PROJECT NO: 19010

PROJECT DATE: 02.15.2024

DRAWING TITLE:

DEVELOPMENT PLAN REVIEW

RENDERING

A-6.0



VIEW 3 - MARKET VIEW

- LOOKING TOWARDS SOUTH WEST FROM
MAIN STREET

NADEL

NADEL STUDIO ONE INC

1990 S. BUNNY DRIVE
SUITE 400
LOS ANGELES, CA 90025
T. 310.826.2100
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WWW.NADELARC.COM

PROFESSIONAL STAMP(S):

CONSULTANT:

CLIENT:
NORTHGATE MARKETS

1201 N. MAGNOLIA AVENUE
ANAHEIM, CA 92801

PROJECT:
NWC 6TH AND MAIN

TBD - 6TH AND MAIN
CORONA, CALIFORNIA

PUBLIC AGENCY SUBMITTAL: -
ISSUED FOR BIDDING: N/A
ISSUED FOR CONSTRUCTION: N/A

REVISIONS:	ISSUE	
NO.:	DESCRIPTION:	DATE:
DPR2023-0021		

DPR2023-0021

NADEL PROJECT NO:	19910
PROJECT DATE:	02.15.2024
ISSUE:	
DRAWING TITLE:	

RENDERING

A-6.2

DEVELOPMENT PLAN REVIEW



VIEW 6 - SHOP BUILDING VIEW

- LOOKING TOWARDS NORTH EAST FROM
PLAZA ENTRANCE ON BELLE AVENUE

NADEL

NADEL STUDIO ONE INC

1990 S. BUNDY DRIVE
SUITE 400
LOS ANGELES, CA 90025
T. 310.826.2100
F. 310.826.0182
WWW.NADELARC.COM

PROFESSIONAL STAMP(S):

CONSULTANT:

CLIENT:

NORTHGATE MARKETS

1201 N. MAGNOLIA AVENUE
ANAHEIM, CA 92801

PROJECT:

NWC 6TH AND MAIN

TBD - 6TH AND MAIN
CORONA, CALIFORNIA

PUBLIC AGENCY SUBMITTAL: -
ISSUED FOR BIDDING: N/A
ISSUED FOR CONSTRUCTION: N/A

REVISIONS:	ISSUE	
NO.	DESCRIPTION	DATE
DPR2023-0021		

DPR2023-0021

NADEL PROJECT NO:	19010
PROJECT DATE:	02.15.2024
DRAWING TITLE:	

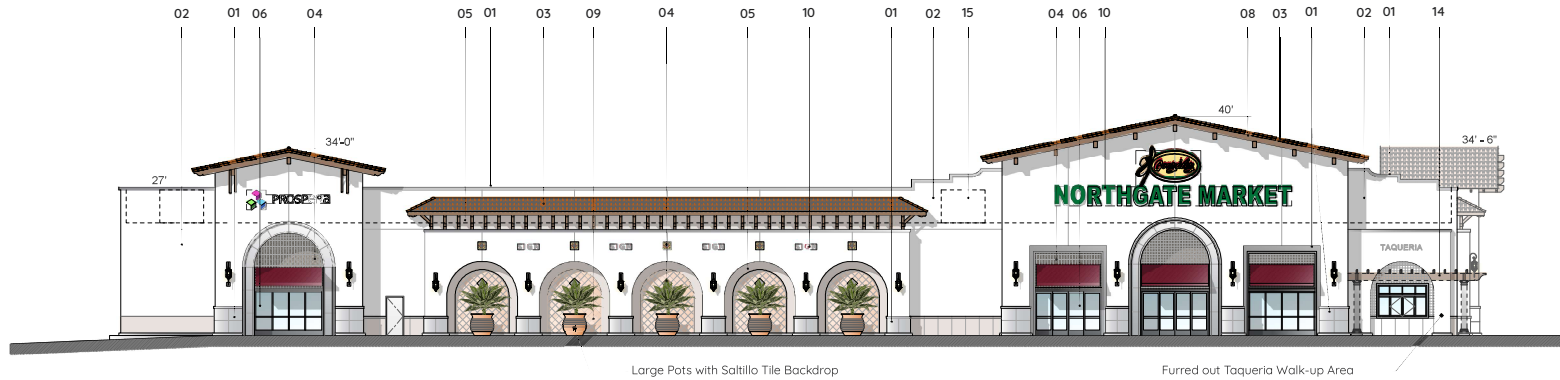
RENDERING

A-6.5

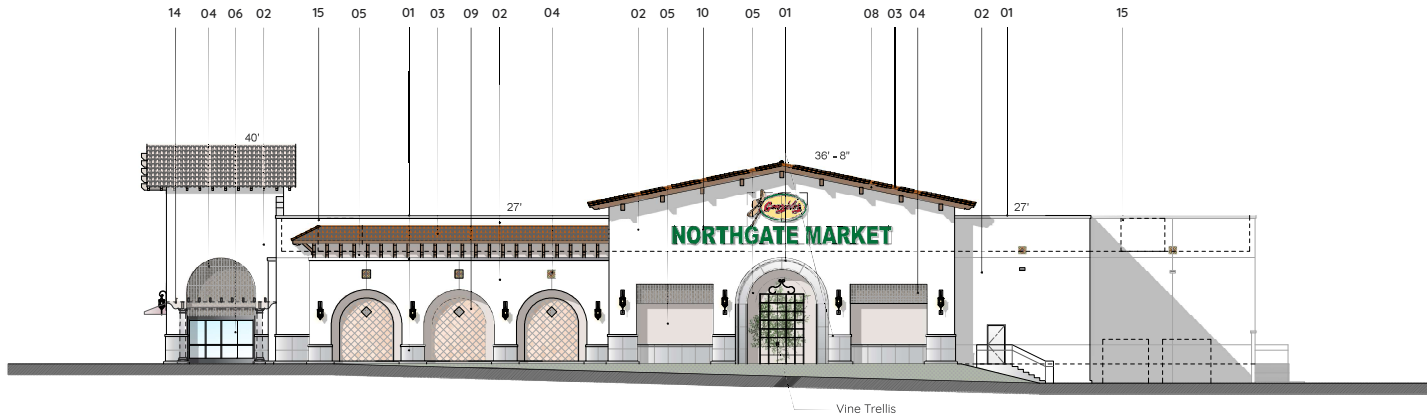
DEVELOPMENT PLAN REVIEW

LEGEND

- 01 CONCRETE FINISH
- 02 THREE COAT PLASTER - WHITE
- 03 SPANISH ROOF TILE
- 04 SPANISH CERAMIC TILE
- 05 THREE COAT PLASTER - GRAY
- 06 STOREFRONT GLAZING
- 07 METAL FINISH
- 08 COMPOSITE WOOD
- 09 SALTILLO CONCRETE TILES
- 10 SIGNAGE
- 11 WROUGHT IRON PATIO SEATING ENCLOSURE
- 12 LOADING DOCK ENCLOSURE
- 13 B.O.H. ENCLOSURE
- 14 DARK WOODEN TRELLIS
- 15 MECHANICAL EQUIPMENT



SOUTH ELEVATION



EAST ELEVATION

NADEL

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F. 310.826.0182
WWW.NADELARC.COM

PROFESSIONAL STAMP(S):

CONSULTANT:

CLIENT:

NORTHGATE MARKETS

1201 N. MAGNOLIA AVENUE
ANAHEIM, CA 92801

PROJECT:

NWC 6TH AND MAIN

TBD - 6TH AND MAIN
CORONA, CALIFORNIA

PUBLIC AGENCY SUBMITTAL: -
ISSUED FOR BIDDING: N/A
ISSUED FOR CONSTRUCTION: N/A

REVISIONS:
ISSUE DESCRIPTION: ISSUE DATE:
NO.: DATE:

DPR2023-0021

NADEL PROJECT NO: 19010
PROJECT DATE: 02.15.2024
DRAWING TITLE:

MARKET ELEVATION

A-3.1

EXHIBIT 9

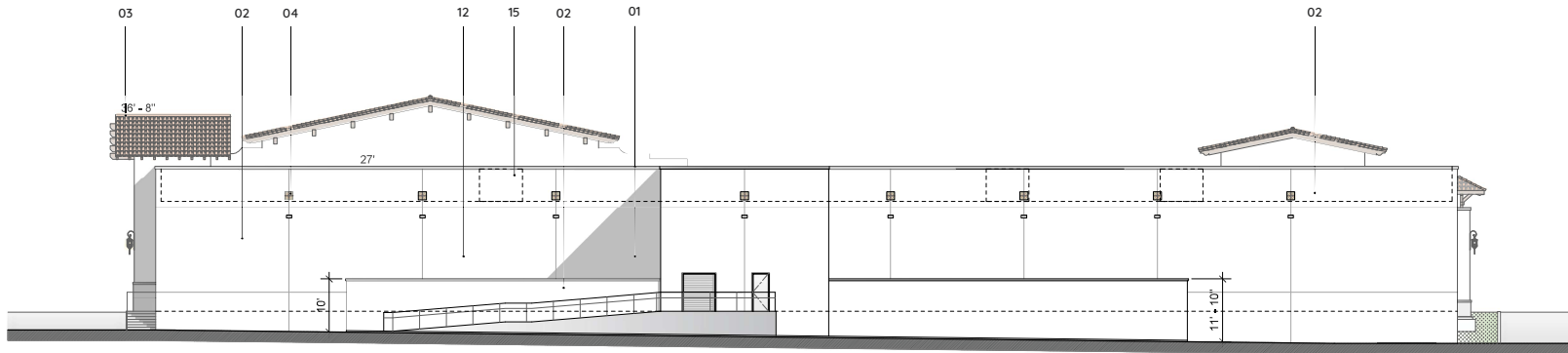
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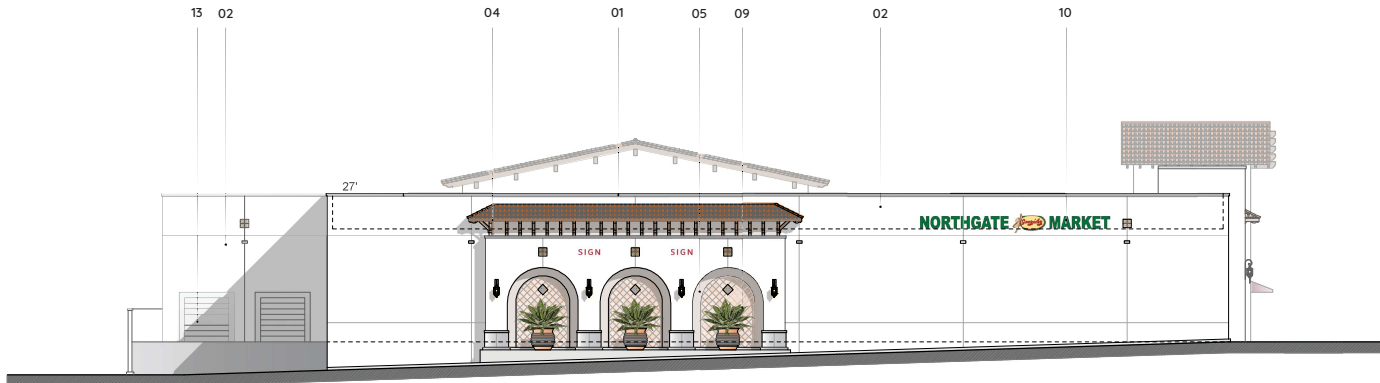
DEVELOPMENT PLAN REVIEW

LEGEND

- 01 CONCRETE FINISH
- 02 THREE COAT PLASTER - WHITE
- 03 SPANISH ROOF TILE
- 04 SPANISH CERAMIC TILE
- 05 THREE COAT PLASTER - GRAY
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- 15 MECHANICAL EQUIPMENT



NORTH ELEVATION



WEST ELEVATION

SCALE: 3/32" = 1'-0"



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TBD - 6TH AND MAIN
CORONA, CALIFORNIA

PUBLIC AGENCY SUBMITTAL: -
ISSUED FOR BIDDING: N/A
ISSUED FOR CONSTRUCTION: N/A

REVISIONS:
ISSUE DESCRIPTION: ISSUE DATE:
NO.: DATE:

DPR2023-0021

NADEL PROJECT NO: 19910
PROJECT DATE: 02.15.2024
DRAWING TITLE:

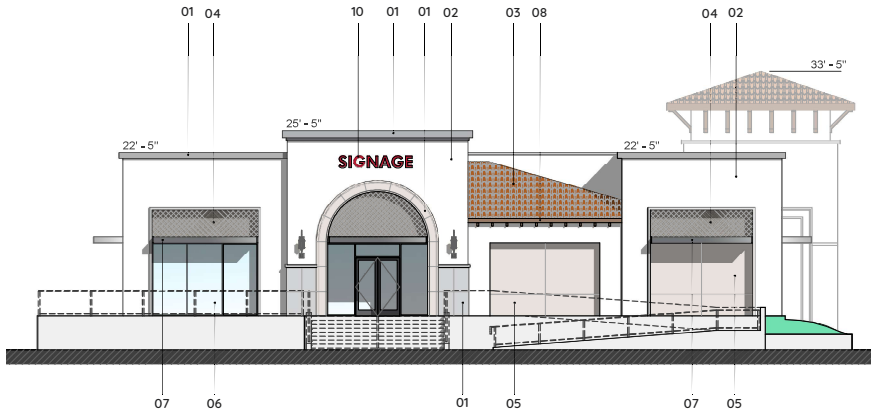
MARKET ELEVATION

A-3.2

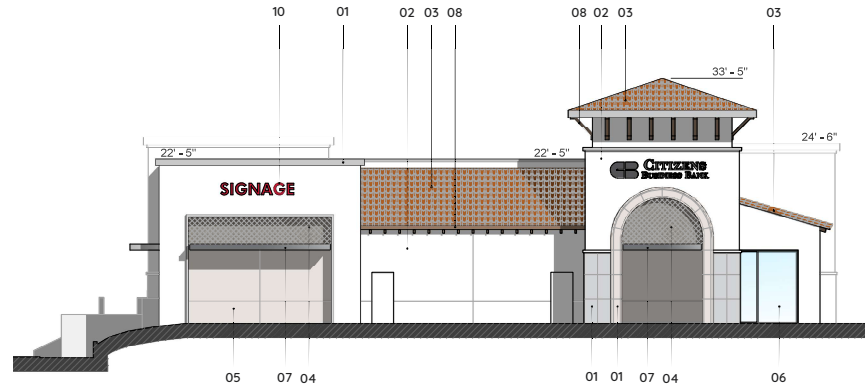
DEVELOPMENT PLAN REVIEW

LEGEND

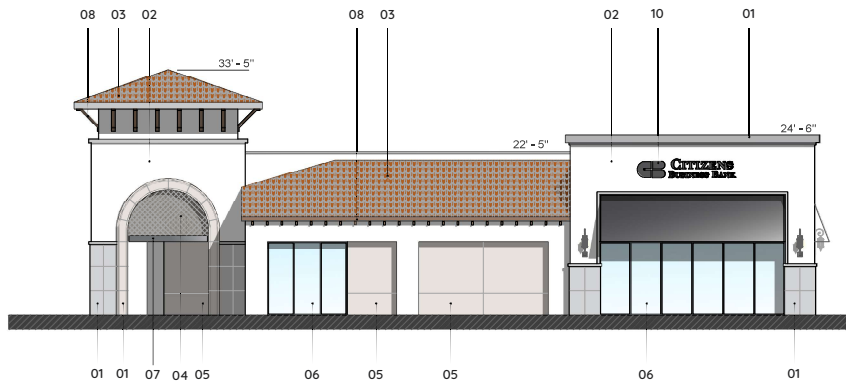
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- 02 THREE COAT PLASTER - WHITE
- 03 SPANISH ROOF TILE
- 04 SPANISH CERAMIC TILE
- 05 THREE COAT PLASTER - GRAY
- 06 STOREFRONT GLAZING
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- 15 MECHANICAL EQUIPMENT



NORTH ELEVATION



WEST ELEVATION



SOUTH ELEVATION



EAST ELEVATION

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REVISIONS:
ISSUE DESCRIPTION: ISSUE
NO.: DATE:

DPR2023-0021

NADEL PROJECT NO: 19010
PROJECT DATE: 02.15.2024

DEVELOPMENT PLAN REVIEW

**BANK &
RESTAURANT
ELEVATIONS**

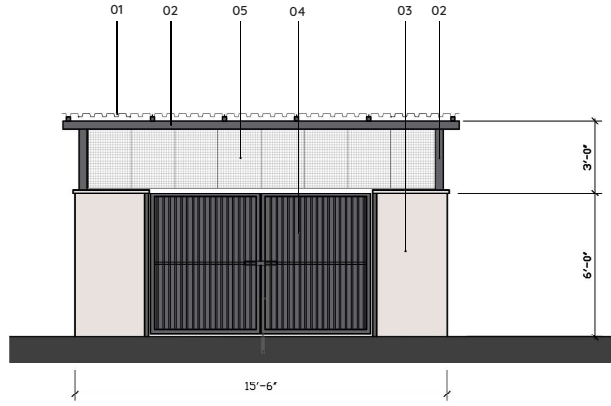
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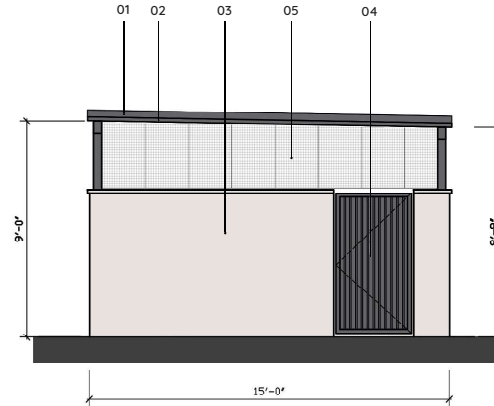


LEGEND

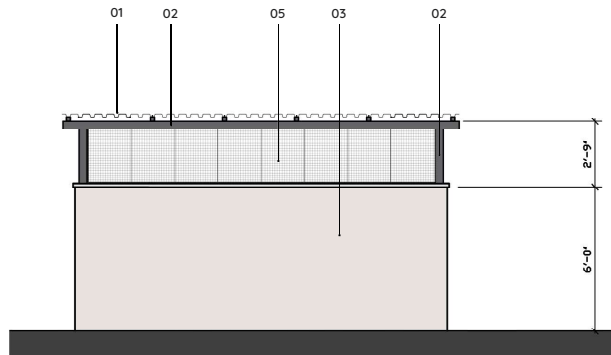
- 01 METAL DECK COVER
- 02 TUBE STEEL FRAME
- 03 THREE COAT PLASTER - GRAY
- 04 METAL GATE
- 05 METAL MESH



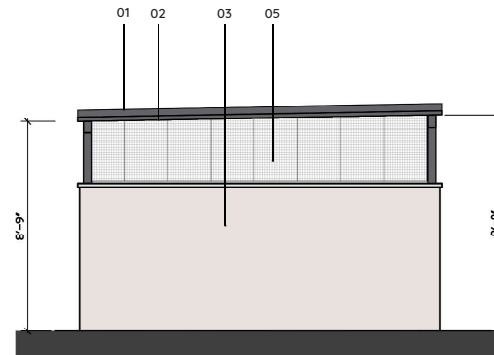
FRONT ELEVATION



RIGHT ELEVATION



REAR ELEVATION



LEFT ELEVATION

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REVISIONS:
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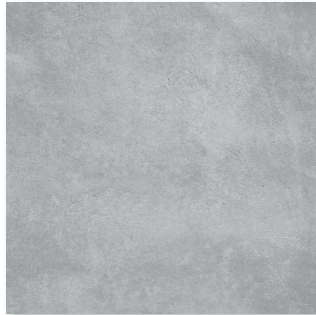
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NADEL PROJECT NO: 19010
PROJECT DATE: 02.15.2024
DRAWING TITLE:

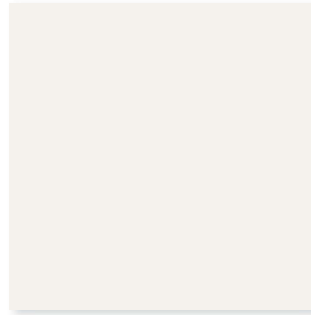
DEVELOPMENT PLAN REVIEW

TRASH ENCLOSURE

A-3.4



1
PRECAST CONCRETE
 Concrete finish



2
THREE COAT PLASTER
 Color to match "Greek Villa"
 - SW 7551
 by Sherwin Williams



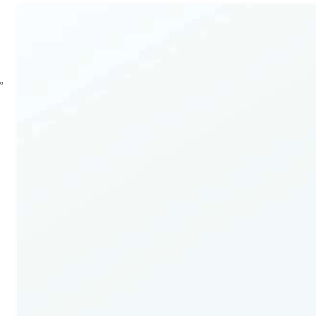
3
SPANISH ROOF TILE
 Clay roof tile in color
 "Montpellier" with 10% boost
 by Claymex



4
SPANISH TILE ACCENT
 Exterior Spanish ceramic tile,
 6" x 6"



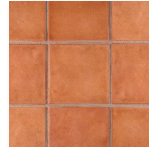
5
THREE COAT PLASTER
 Color to match "Balanced Beige"
 -SW 7037
 by Sherwin Williams



6
STOREFRONT GLAZING
 Clear, Tempered Glass



7
METAL FINISH
 "Tricorn Black" - SW 6258 "Sun Dried Tomato" - SW 7585
 by Sherwin Williams by Sherwin Williams



8
SALTILLO CONCRETE TILES



9
WOODEN TRELLIS
 Stain Wood with Semi-transparent "Blue Shadow" SW 3531
 by Sherwin Williams

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 CORONA, CALIFORNIA

PUBLIC AGENCY SUBMITTAL: -
 ISSUED FOR BIDDING: N/A
 ISSUED FOR CONSTRUCTION: N/A

REVISIONS:
 ISSUE DESCRIPTION: ISSUE DATE:
 NO.: DATE:

DPR2023-0021

NADEL PROJECT NO: 19010

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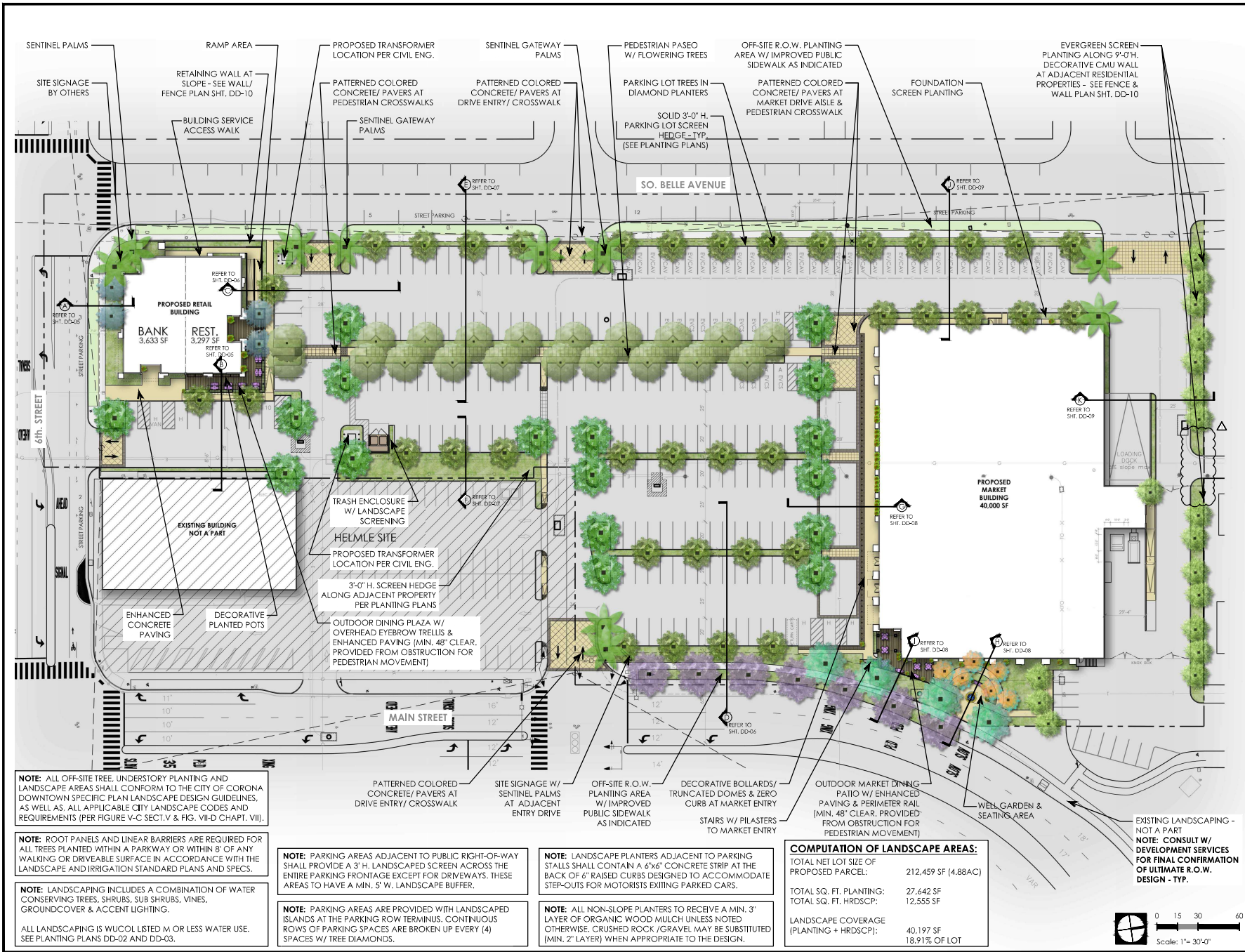
DRAWING TITLE:

COLOR AND MATERIAL PALETTE

A-2.0

DEVELOPMENT PLAN REVIEW

EXHIBIT 10



NOTE: ALL OFF-SITE TREE, UNDERSTORY PLANTING AND LANDSCAPE AREAS SHALL CONFORM TO THE CITY OF CORONA DOWNTOWN SPECIFIC PLAN LANDSCAPE DESIGN GUIDELINES, AS WELL AS, ALL APPLICABLE CITY LANDSCAPE CODES AND REQUIREMENTS (PER FIGURE V-C SECT. V & FIG. VII-D CHAPT. VII).

NOTE: ROOT PANELS AND LINEAR BARRIERS ARE REQUIRED FOR ALL TREES PLANTED WITHIN A PARKWAY OR WITHIN 8' OF ANY WALKING OR DRIVEABLE SURFACE IN ACCORDANCE WITH THE LANDSCAPE AND IRRIGATION STANDARD PLANS AND SPECS.

NOTE: LANDSCAPING INCLUDES A COMBINATION OF WATER CONSERVING TREES, SHRUBS, SUB SHRUBS, VINES, GROUNDCOVER & ACCENT LIGHTING.

ALL LANDSCAPING IS WUCOL LISTED M OR LESS WATER USE. SEE PLANTING PLANS DD-02 AND DD-03.

NOTE: PARKING AREAS ADJACENT TO PUBLIC RIGHT-OF-WAY SHALL PROVIDE A 3' H. LANDSCAPED SCREEN ACROSS THE ENTIRE PARKING FRONTAGE EXCEPT FOR DRIVEWAYS. THESE AREAS TO HAVE A MIN. 5' W. LANDSCAPE BUFFER.

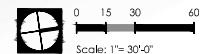
NOTE: PARKING AREAS ARE PROVIDED WITH LANDSCAPED ISLANDS AT THE PARKING ROW TERMINUS. CONTINUOUS ROWS OF PARKING SPACES ARE BROKEN UP EVERY (4) SPACES W/ TREE DIAMONDS.

NOTE: LANDSCAPE PLANTERS ADJACENT TO PARKING STALLS SHALL CONTAIN A 4"x6" CONCRETE STRIP AT THE BACK OF 6" RAISED CURBS DESIGNED TO ACCOMMODATE STEP-OUTS FOR MOTORISTS EXITING PARKED CARS.

NOTE: ALL NON-SLOPE PLANTERS TO RECEIVE A MIN. 3" LAYER OF ORGANIC WOOD MULCH UNLESS NOTED OTHERWISE. CRUSHED ROCK /GRAVEL MAY BE SUBSTITUTED (MIN. 2" LAYER) WHEN APPROPRIATE TO THE DESIGN.

COMPUTATION OF LANDSCAPE AREAS:

TOTAL NET LOT SIZE OF PROPOSED PARCEL:	212,459 SF (4.88AC)
TOTAL SQ. FT. PLANTING:	27,642 SF
TOTAL SQ. FT. HRDSCP:	12,555 SF
LANDSCAPE COVERAGE (PLANTING + HRDSCP):	40,197 SF 18.91% OF LOT



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CONSULTANT:

LAND CREATIVE
 695 Town Center Drive, # 100
 Costa Mesa, CA 92626
 Phone: 714 . 352 . 7588

CLIENT:
NORTHGATE MARKETS
 1201 N. MAGNOLIA AVENUE
 ANAHEIM, CA 92801

PROJECT:
NWC 6TH AND MAIN
 TBD - 6TH AND MAIN
 CORONA, CALIFORNIA

PUBLIC AGENCY SUBMITTAL:
 ISSUED FOR BIDDING: N/A
 ISSUED FOR CONSTRUCTION: N/A

REVISIONS

NO.	ISSUE DESCRIPTION	ISSUE DATE
1	PRECISE PLAN RESUBMITTAL #2	10-08-25

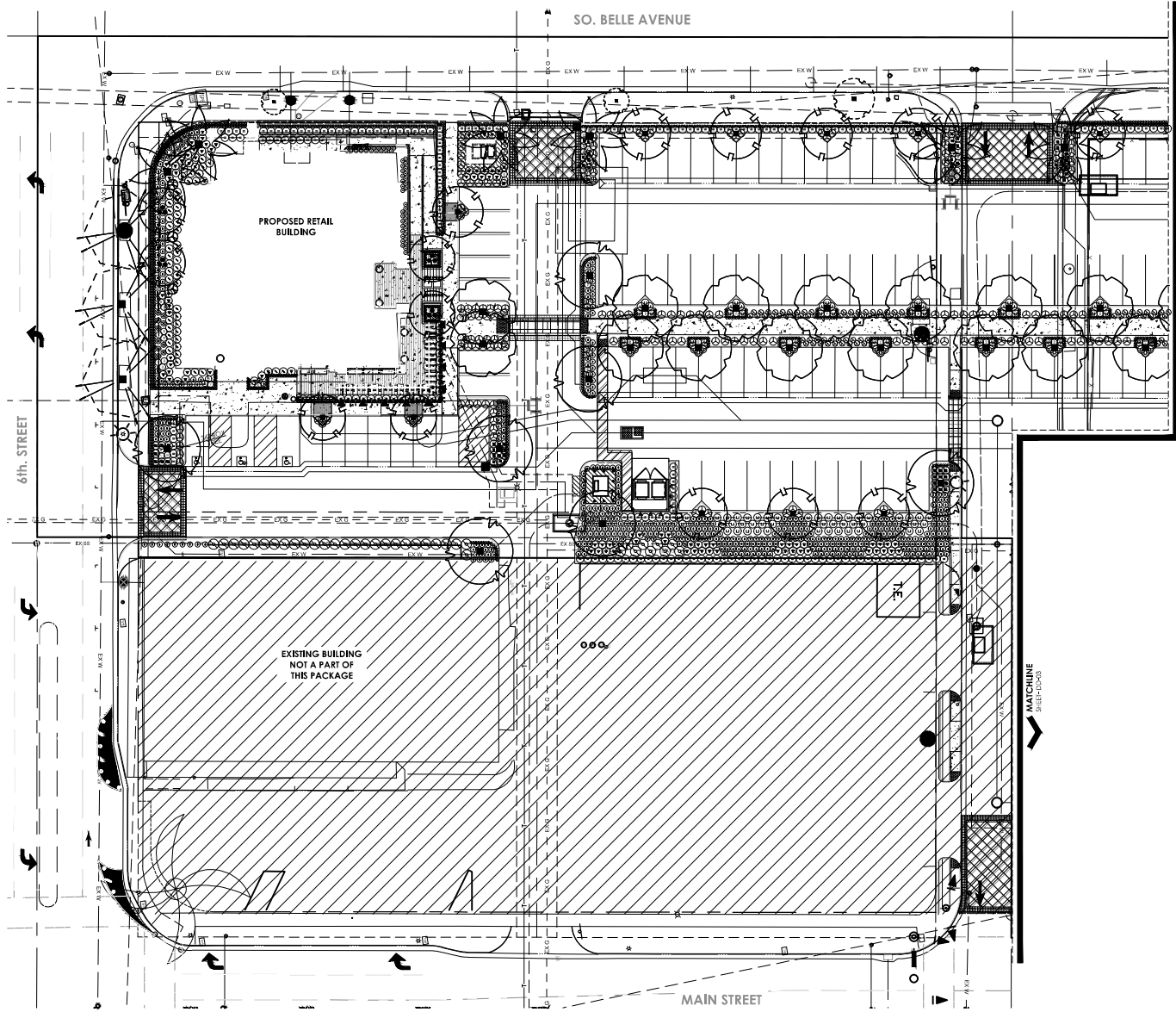
NADEL PROJECT NO:	19010
PROJECT DATE:	10.08.2025
DRAWING TITLE:	PRECISE PLAN RESUBMITTAL #2

Landscape Planting Site Plan

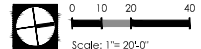
DD-01

PLOT DATE: 10/08/25

EXHIBIT 11



NOTE:
IMPROVEMENTS IN THE R.O.W. ALONG MAIN ST. SHALL ALSO BE IN COMPLIANCE WITH CHAPTER 9 STREET SCENE PLAN OF THE CITY OF CORONA DOWNTOWN REVITALIZATION SPECIFIC PLAN. SEE FIGURE B1A-D.



PLANTING LEGEND				
SYMBOL	PLANT NAME	SIZE	REMARKS	PLANT FACTS
EXISTING OFF-SITE				
R.O.W. CITY STREET TREES TO BE REMOVED:				
⊙	EUCALYPTUS CITRICOLOA	EXISTING	REMOVE PER SIGHT LINE	-
⊙	LEMON SCENTED GUM	EXISTING	REMOVE PER SIGHT LINE	-
⊙	EXISTING - SPECIES VARIES	EXISTING	REMOVE PER SIGHT LINE	-
PROPOSED OFF-SITE				
R.O.W. CITY STREET TREES:				
⊙	JACARANDA HINOCIFOLIA	BOX SIZE PER CITY	STANDARD	AM
⊙	JACARANDA	BOX SIZE PER CITY	STANDARD	AM
TREES				
⊙	PHOENIX DACTYLIFERA "MEDJOL"	20' B.T.H.	MATCHING	L
⊙	MEDJOL DATE PALM			
⊙	CERCIDILUM X. HYBRIDA "DESERT MUSEUM"	15 GAL	MULTI	L
⊙	DESERT MUSEUM PALO VERDE			
⊙	CERCIDILUM X. HYBRIDA "DESERT MUSEUM"	24' BOX	STANDARD	L
⊙	DESERT MUSEUM PALO VERDE			
⊙	CITRUS X. SPP.	15 GAL	STANDARD	AM
⊙	ORANGE / LEMON			
⊙	GLIEDISIA TRICANTHIOS INERMB "SHADEMASTER"	24' BOX	STANDARD	AM
⊙	HORNLESS HONEYLOCUST	36' BOX		
⊙	METROSEDIOS EXCELSA	24' BOX	STANDARD	AM
⊙	NEW ZEALAND CHRISTMAS TREE			
⊙	PODOCARPUS ELONGATUS "MONNALI"	15 GAL	PULL TO GROUND	AM
⊙	KEE BLUE YELLOWWOOD			
⊙	PODOCARPUS GRACILIOR	15 GAL	STANDARD	AM
⊙	FERN FINE		PULL TO GROUND	
⊙	QUERCUS VIRGILIANA	24' BOX	STANDARD	AM
⊙	SOUTHERN LIVE OAK			
SHRUBS:				
⊙	AGAVE ATTENUATA "BOUJIN BLUE"	5 GAL		L
⊙	BLUE FOX TAIL AGAVE			
⊙	AGAVE ATTENUATA "VAREGATA"	5 GAL		L
⊙	VAREGATED FOX TAIL AGAVE			
⊙	ALOE LODYS YELLOW	5 GAL		L
⊙	ALOE			
⊙	ALOE "OUTRACK ORANGE"	5 GAL		L
⊙	ALOE			
⊙	CALLISTEMON VINIVABIS "LITTLE JOHN DWARF"	5 GAL		L
⊙	BOTTLEBRUSH			
⊙	CHONDROPETALUM TECTORIUM	5 GAL		AM
⊙	SMALL CAPE RUSH			
⊙	DODONAEA VISCOSA PURPUREA	5 GAL		AM
⊙	PURPLE HOPSPRED BUSH			
⊙	FESTUCA WAREI	5 GAL		L
⊙	ATLAS FESCUE			
⊙	FURCRAEA FOETIDA	5 GAL		L
⊙	MAUIRILIS "HEU"			
⊙	JUNCUS PATENS "PEK BLUE"	5 GAL		L
⊙	JUNCUS			
⊙	LANTANA X "MONET"	1 GAL		L
⊙	SPEARPOINT SUNSET LANTANA			
⊙	LANTANA MONTEVIDEENSE "NEW GOLD"	1 GAL		L
⊙	TRAILING LANTANA			
⊙	LEUCOPHYLLUM CANDIDUM "HENDER CLOUD"	1 GAL		L
⊙	TRAILING SAGE			
⊙	IGLOSTRUM J. "TEXANUM"	5 GAL		AM
⊙	WAXLEAF FIBRET			
⊙	MULLENBERGIA REGENS	5 GAL		AM
⊙	DEER GRASS			
⊙	PHORNUM SURBER	5 GAL		AM
⊙	NEW ZEALAND FLAX			
⊙	SANTOLINA CHAMAECYPARISUS	1 GAL		L
⊙	LAVENDER COTTON			
GROUNDCOVER:				
⊙	MYOPORIUM PARVIFOLIUM "UTAH CREEK"	1 GAL		L
⊙	MYOPORIUM			
⊙	SEDUM COPPERTONE	1 GAL		L
⊙	STONECROP SEDUM			
⊙	SENECIO SERRIS	1 GAL		L
⊙	BLUE CRANFLOCKS			
VINES:				
⊙	BOUGAINVILLEA "SAN DIEGO RED"	- GAL	NOTES	L
⊙	SAN DIEGO RED			
⊙	CAMPUS RAJICANS	- GAL	NOTES	L
⊙	HONEY VINE			
TREES AND SHRUBS PLANTING COUNTS NOTE:				
1. CONTRACTOR SHALL BE RESPONSIBLE FOR COUNTING AND VERIFYING ALL PLANT QUANTITIES, AS SHOWN ON THE PLANTING PLAN.				
2. CONTRACTOR SHALL SUBMIT PHOTOS OF ALL PLANTS FOR REVIEW AND APPROVAL.				
PLANTING MULCH:				
ALL SHRUB AND PLANTER AREAS SHALL RECEIVE 3" DEEP LAYER OF ROCK MULCH AND/ OR SHREDDED BARK MULCH.				
NOTE:				
ALL TREE SEES AND PLANT MATERIAL SHALL CONFORM TO SP 11-54 (3AND 3) PER THE CITY OF CORONA DOWNTOWN REVITALIZATION SPECIFIC PLAN.				
OFF-SITE PLANTING CITY REQUIREMENTS:				
ALL OFF-SITE R.O.W. LANDSCAPE IMPROVEMENTS (TREE & UNDERSTORY PLANTING) SHALL CONFORM TO THE CITY OF CORONA DOWNTOWN SPECIFIC PLAN LANDSCAPE DESIGN GUIDELINES, AS WELL AS ALL APPLICABLE CITY LANDSCAPE CODES AND REQUIREMENTS. THE LANDSCAPE PLAN SHALL REFLECT THE ACCURATE R.O.W. IMPROVEMENTS PER FIGURE B1C, ROADWAY CROSS SECTIONS OF SECTION 7 TRANSFORMATION STUDY OF THE DOWNTOWN CORONA REVEALATION SP. CONSULT WITH THE DEVELOPMENT SERVICES FOR FINAL CONTRIBUTION OF ULTIMATE R.O.W. SURVEY.				
NOTE:				
DUE TO CORNER CUT OFF VIEW SIGHT LINES, THERE ARE NO PROPOSED OFF-SITE TREES LOCATED ON BELLE AVE. ALSO, EXISTING TREES ON 6TH STREET SHALL BE REMOVED AS THEY FALL INTO SIGHT LINES. SEE PLANTING PLAN FOR EXTENT OF SIGHT LINE LOCATIONS AT ALL ENTRIES AND LOCATIONS OF EXISTING TREES TO BE REMOVED.				
NOTE:				
SEE PLANTING PLANS FOR SOLID 3' HIGH HEDGE (CALLISTEMON VINIVABIS "LITTLE JOHN") NEAR LANDSCAPE SETBACK AREAS ADJACENT TO STREETS (MAIN, 6TH & BELLE AVE) TO SCREEN PARKING AREAS.				

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Phone: 714.332.7588

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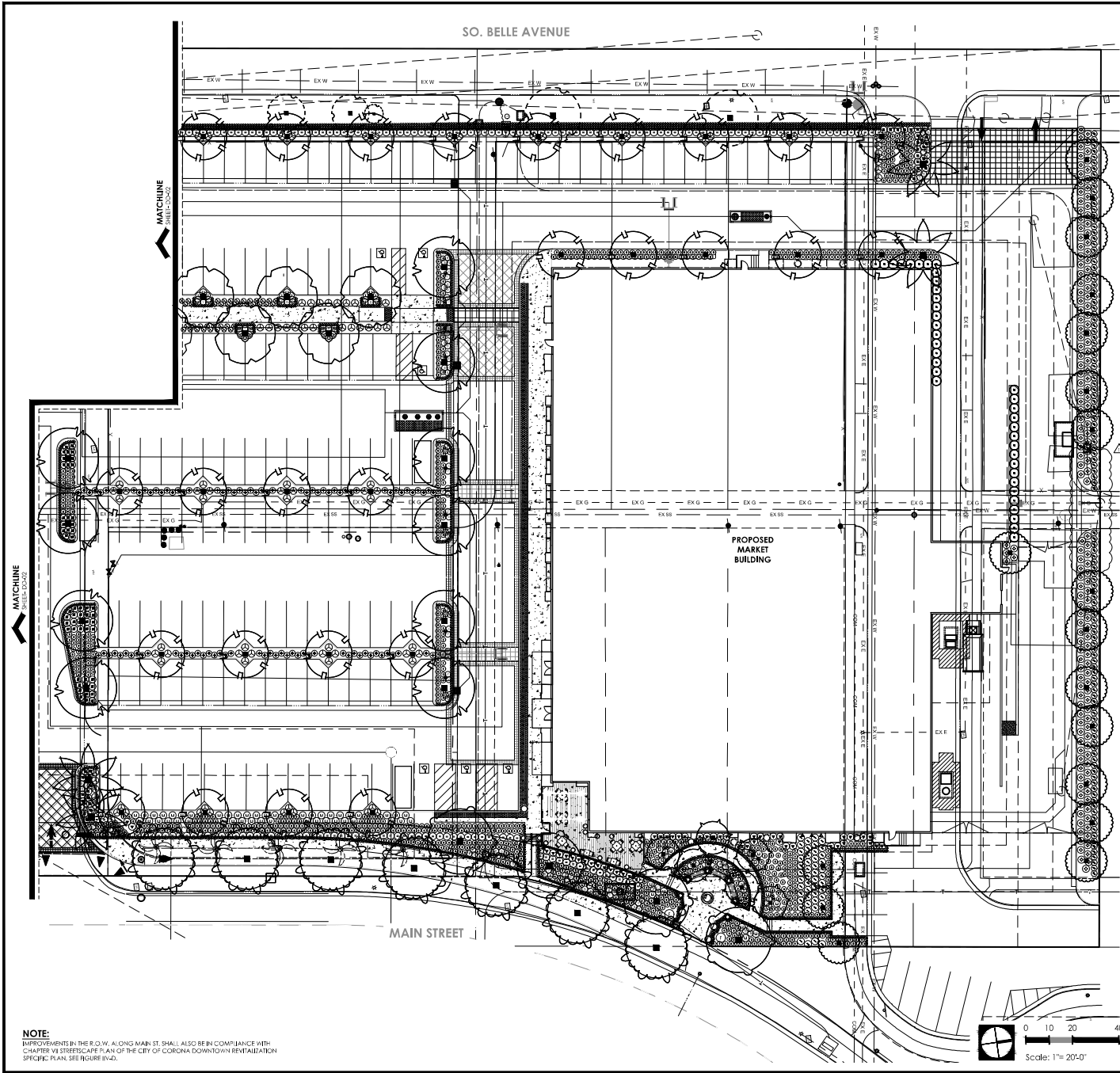
REVISIONS:

NO.	DESCRIPTION:	ISSUE DATE:
1	PRECISE PLAN RESUBMITTAL #2	10-08-25

NADEL PROJECT NO: 19010
PROJECT DATE: 10.08.2023
DRAWING TITLE:

Preliminary Planting Plan

DD-02



PLANTING LEGEND

SYMBOL	PLANT NAME	SIZE	REMARKS	QUANTITY
EXISTING OFF-SITE				
R.O.W. CITY STREET TREES TO BE REMOVED:				
⊙	EUCALYPTUS CITRICOLOA	EXISTING	REMOVE PER SIGHT LINE	-
⊙	LEMON SCENTED GUM	EXISTING	REMOVE PER SIGHT LINE	-
⊙	EXISTING - SPECIES VARIES	EXISTING	REMOVE PER SIGHT LINE	-
PROPOSED OFF-SITE				
R.O.W. CITY STREET TREES:				
⊙	JACARANDA HINOCIOSA	BOX SEED PER CITY REQ.	STANDARD	AM
⊙	JACARANDA	BOX SEED PER CITY REQ.	STANDARD	AM
TREES				
⊙	PHOENIX DACTYLIFFERA "MED JOJO"	20' B.T.H.	MATCHING	L
⊙	MED JOJO DATE PALM			
⊙	CERCIDIMUM X. HYBRIDA "DESERT MUSEUM"	15 GAL	MULTI	L
⊙	DESERT MUSEUM PALO VERDE			
⊙	CERCIDIMUM X. HYBRIDA "DESERT MUSEUM"	24" BOX	STANDARD	L
⊙	DESERT MUSEUM PALO VERDE			
⊙	CITRUS X. SPP.	15 GAL	STANDARD	AM
⊙	ORANGE / LEMON			
⊙	OLEA SIDA "RICANIHOS NERABE" SHADEMASTER	24" BOX	STANDARD	AM
⊙	36" BOX			
⊙	METROSEDIROS EXCELSA	24" BOX	STANDARD	AM
⊙	NEW ZEALAND CHRISTMAS TREE			
⊙	PODOCARPUS ELONGATUS "NORMAN"	15 GAL	FULL TO GROUND	AM
⊙	ICEE BLUE YELLOWWOOD			
⊙	PODOCARPUS GRACILIOR	15 GAL	STANDARD	AM
⊙	FERN FINE		FULL TO CORN	AM
⊙	QUERCUS VIRGIANNA	24" BOX	STANDARD	AM
⊙	SOUTHERN LIVE OAK			
SHRUBS				
⊙	AGAVE ATTENUATA "BOUJIN BLUE"	5 GAL		L
⊙	BLUE FOX TAIL AGAVE			
⊙	AGAVE ATTENUATA "VAREGATA"	5 GAL		L
⊙	VAREGATED FOX TAIL AGAVE			
⊙	ALOE LODDY YELLOW	5 GAL		L
⊙	ALOE			
⊙	ALOE OUTBACK ORANGE	5 GAL		L
⊙	ALOE			
⊙	CALLISTEMON VINIABUS "LITTLE JOHN DWARF"	5 GAL		L
⊙	BOTTLEBRUSH			
⊙	CHONDROPETALUM RECTIFORME	5 GAL		AM
⊙	SMALL CAPE RUSH			
⊙	DODONAEA VISCOSA PURPUREA	5 GAL		AM
⊙	PURPLE HOPBEG BUSH			
⊙	FESTUCA WAREI	5 GAL		L
⊙	ATLAS FESCUE			
⊙	FRAXINAE FOETIDA	5 GAL		L
⊙	MAHURLES "HEP"			
⊙	JUNCUS PATENS "EIK BLUE"	5 GAL		L
⊙	JUNCUS			
⊙	LANTANA X "MONET"	1 GAL		L
⊙	SPREADING SUNSET LANTANA			
⊙	LANTANA MONTEVEDENSE "NEW GOLD"	1 GAL		L
⊙	TRAILING LANTANA			
⊙	LEUCOPHYLLUM CANDIDUM "HENDER CLOUD"	1 GAL	24" W x L	L
⊙	TRAILER SAGE			
⊙	LOSTRUM J. "TEXANUM WAXLEAF PRIST"	5 GAL	STAKED	AM
⊙	WAXLEAF PRIST			
⊙	MULLENBERGIA RIGENS	5 GAL		AM
⊙	28ER GRASS			
⊙	PHORMIUM SUBSER	5 GAL		AM
⊙	NEW ZEALAND FLAX			
⊙	SANTOLINA CHAMAECYPARISUS	1 GAL		L
⊙	LAVENDER COTTON			
GROUNDCOVER				
⊙	MYOPORIUM PARVIFOLIUM "UTAH CREEK"	1 GAL		L
⊙	MYOPORIUM			
⊙	SEDUM COPPERSTONE	1 GAL		L
⊙	STONECROP SEDUM			
⊙	SENECIO SERPENS	1 GAL		L
⊙	BLUE GRASS/GRASS			
VINES				
⊙	BOUGHAVILLA "SAN DIEGO RED"	- GAL	NOTES	L
⊙	SAN DIEGO RED			
⊙	CAMPUS RAJICANS	- GAL	NOTES	L
⊙	HONEY VINE			
TREES AND SHRUBS PLANTING COUNTS NOTE:				
1. CONTRACTOR SHALL BE RESPONSIBLE FOR COUNTING AND VERIFYING ALL PLANT QUANTITIES, AS SHOWN ON THE PLANTING PLAN, PER LANDSCAPE DESIGN GUIDELINES, AS WELL AS ALL APPLICABLE CITY LANDSCAPE CODES AND REQUIREMENTS.				
2. CONTRACTOR SHALL SUBMIT PHOTOS OF ALL PLANTS FOR REVIEW AND APPROVAL.				
PLANTING MULCH:				
ALL SHRUB AND PLANTER AREAS SHALL RECEIVE 3" DEEP LAYER OF ROCK MULCH AND/OR SHREDDED BARK MULCH.				
NOTE:				
ALL TREE SEEDS AND PLANT MATERIAL SHALL CONFORM TO SP 11-54 (3AND 3P) PER THE CITY OF CORONA DOWNTOWN REVITALIZATION SPECIFIC PLAN.				
OFF-SITE PLANTING CITY REQUIREMENTS:				
ALL OFF-SITE R.O.W. LANDSCAPE IMPROVEMENTS (TREE & UNDERSTORY PLANTING) SHALL CONFORM TO THE CITY OF CORONA DOWNTOWN SPECIFIC PLAN LANDSCAPE DESIGN GUIDELINES, AS WELL AS ALL APPLICABLE CITY LANDSCAPE CODES AND REQUIREMENTS.				
THE LANDSCAPE PLAN SHALL REFLECT THE ACCURATE R.O.W. IMPROVEMENTS PER FIGURE 10C, ROADWAY CROSS SECTIONS OF SECTION 7, TRANSFORM WHICH STRIP(S) OF THE DOWNTOWN CORONA REVEALATION SP. CONSULT WITH THE DEVELOPMENT SERVICES FOR FINAL CONTRIBUTION OF ULTIMATE R.O.W. STRIP(S).				
NOTE:				
DUE TO CORNER CUT OFF VIEW SIGHT LINES, THERE ARE NO PROPOSED OFF-SITE TREES LOCATED ON BELLE AVE. ALSO, EXISTING TREES ON MAIN STREET SHALL BE REMOVED AS THEY FALL INTO SIGHT LINES. SEE PLANTING PLAN FOR EXTENT OF SIGHT LINE LOCATIONS AT ALL ENTRIES AND LOCATIONS OF EXISTING TREES TO BE REMOVED.				
NOTE:				
SEE PLANTING PLANS FOR SOLID 3' HIGH HEDGE (CALLISTEMON VINIABUS "LITTLE JOHN") WITH LANDSCAPED STREETBANK AREAS ADJACENT TO STREETS (MAIN, 6TH & BELLE AVE.) TO SCREEN PARKING AREAS.				

NOTE:
IMPROVEMENTS IN THE R.O.W. ALONG MAIN ST. SHALL ALSO BE IN COMPLIANCE WITH CHAPTER 16.1 STREETScape PLAN OF THE CITY OF CORONA DOWNTOWN REVITALIZATION SPECIFIC PLAN. SEE FIGURE 10C.

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ANAHEIM, CA 92801

PROJECT:
NWC 6TH AND MAIN

TBD - 6TH AND MAIN
CORONA, CALIFORNIA

PUBLIC AGENCY SUBMITTAL:

ISSUED FOR BIDDING:	N/A
ISSUED FOR CONSTRUCTION:	N/A

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△	PRECISE PLAN RESUBMITTAL #2	10-08-25

NADEL PROJECT NO:	19010
PROJECT DATE:	10.08.2025
DRAWING TITLE:	PRECISE PLAN RESUBMITTAL #2

Preliminary Planting Plan

DD-03

PLOT DATE: 10/08/25

Proposed Off-Site R.O.W. Tree Planting



JACARANDA MIMOSIFOLIA
JACARANDA

Proposed Tree Planting



PHOENIX DACTYLIFERA 'MEDJOL'
MEDJOL DATE PALM



CERCIDIUM X HYBRIDA 'DESERT MUSEUM'
DESERT MUSEUM PALO VERDE (MULTI-BRANCHED)



CERCIDIUM X HYBRIDA 'DESERT MUSEUM'
DESERT MUSEUM PALO VERDE



CITRUS X SPP.
ORANGE / LEMON



GLEDITSIA TRICANTHOS INERANS 'SHADEMASTER'
THORNLESS HONEY LOCUST



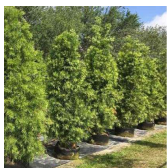
METROSIDEROS EXCELSA
NEW ZEALAND CHRISTMAS TREE



PODOCARPUS ELONGATUS 'MONNAL'
ICEE BLUE YELLOWWOOD



PODOCARPUS GRACILIOR
FERN FINE - TREE FORM



PODOCARPUS GRACILIOR
FERN FINE - SHRUB FORM



QUERCUS VIRGINIANA
SOUTHERN LIVE OAK

Proposed Shrub Planting



AGAVE ATTENUATA 'BOULIN BLUE'
BLUE FOX TAIL AGAVE



AGAVE ATTENUATA 'VAREGATA'
VAREGATED FOX TAIL AGAVE



ALOE LODES 'YELLOW'
ALOES



ALOE 'OUTBACK ORANGE'
ALOES



CALLITEMON VIMINALE 'LITTLE JOHN'
DWARF BOTTLERUSH



CHONDROPETALUM TECTORIUM
SMALL CAPE RUSH



DODONAEA VISCOSA 'PURPUREA'
PURPLE HOPSEED BUSH



FESTUCA MARIEI
ATLAS RESCUE



FURCRAEA FOETIDA
MAUNTILUS HEAD



JUNCLIS PATENS 'TELE BLUE'
JUNCUS



LANTANA X 'SUNSET'
SPREADING SUNSET LANTANA



LANTANA MONTEVEDENSI 'NEW GOLD'
TRAILING LANTANA



LEUCOPHYLLUM CANDIDUM 'THUNDER CLOUD'
TEXAS SAGE



LEUCOSTEMUM J. 'TEXANUM'
WAXLEAF FRUIT



MULLENBERGIA BIGENS
DEER GRASS



PHORADENDRON 'SUNRIPER'
NEW ZEALAND FLAX



SANTOLINA CHAMAECIPARIS
LAVENDER COTTON

Proposed Ground Cover



MYOPORUM PARVIFOLIUM 'PUHA CREEK'
MYOPORUM



SEDUM COPPERTONE
STONECROP SEDUM



SENECIO SERPENS
BLUE CHALKSTICKS

Proposed Vines



BOUGAINVILLEA 'SAN DIEGO RED'
SAN DIEGO RED



CAMPSIS SPECIOSA
TRUMPET VINE

PLANTING LEGEND

SYMBOL	PLANT NAME	SIZE	REMARKS	WOOD FACTOR
	EXISTING OFF-SITE R.O.W. CITY STREET TREES TO BE REMOVED: EUCALYPTUS CITRIFOLIA LEMON SCENTED GUM		EXISTING REMOVE PER SIGHT LINE SIG.	-
	EXISTING - SPECIES VARIES		EXISTING REMOVE PER SIGHT LINE SIG.	-

SYMBOL	PLANT NAME	BOX SIZE PER CITY	STANDARD	IM
	PROPOSED OFF-SITE R.O.W. CITY STREET TREES: JACARANDA MIMOSIFOLIA JACARANDA			

TREES	PLANT NAME	SIZE	STANDARD	IM
	PHOENIX DACTYLIFERA 'MEDJOL' MEDJOL DATE PALM	20' B.I.H.	MATCHING	L
	CERCIDIUM X HYBRIDA 'DESERT MUSEUM' DESERT MUSEUM PALO VERDE	15 GAL	MULTI	L
	CERCIDIUM X HYBRIDA 'DESERT MUSEUM' DESERT MUSEUM PALO VERDE	24' BOX	STANDARD	L
	CITRUS X SPP. ORANGE / LEMON	15 GAL	STANDARD	M
	GLEDITSIA TRICANTHOS INERANS 'SHADEMASTER' THORNLESS HONEY LOCUST	24' BOX 36' BOX	STANDARD	M
	METROSIDEROS EXCELSA NEW ZEALAND CHRISTMAS TREE	24' BOX	STANDARD	M
	PODOCARPUS ELONGATUS 'MONNAL' ICEE BLUE YELLOWWOOD	15 GAL	FULL TO GROW	M
	PODOCARPUS GRACILIOR FERN FINE	15 GAL	STANDARD	M
	QUERCUS VIRGINIANA SOUTHERN LIVE OAK	24' BOX	STANDARD	M

SHRUBS	PLANT NAME	SIZE	STANDARD	IM
	AGAVE ATTENUATA 'BOULIN BLUE' BLUE FOX TAIL AGAVE	5 GAL		L
	AGAVE ATTENUATA 'VAREGATA' VAREGATED FOX TAIL AGAVE	5 GAL		L
	ALOE LODES 'YELLOW' ALOES	5 GAL		L
	ALOE 'OUTBACK ORANGE' ALOES	5 GAL		L
	CALLITEMON VIMINALE 'LITTLE JOHN' DWARF BOTTLERUSH	5 GAL		L
	CHONDROPETALUM TECTORIUM SMALL CAPE RUSH	5 GAL		M
	DODONAEA VISCOSA 'PURPUREA' PURPLE HOPSEED BUSH	5 GAL		M
	FESTUCA MARIEI ATLAS RESCUE	5 GAL		L
	FURCRAEA FOETIDA MAUNTILUS HEAD	5 GAL		L
	JUNCLIS PATENS 'TELE BLUE' JUNCUS	5 GAL		L
	LANTANA X MONTEVEDENSI 'NEW GOLD' TRAILING LANTANA	1 GAL		L
	LANTANA MONTEVEDENSI 'NEW GOLD' TRAILING LANTANA	1 GAL		L
	LEUCOPHYLLUM CANDIDUM 'THUNDER CLOUD' TEXAS SAGE	1 GAL	36" x 6"	L
	LEUCOSTEMUM J. 'TEXANUM' WAXLEAF FRUIT	5 GAL	STAKED	M
	MULLENBERGIA BIGENS DEER GRASS	5 GAL		M
	PHORADENDRON 'SUNRIPER' NEW ZEALAND FLAX	5 GAL		M
	SANTOLINA CHAMAECIPARIS LAVENDER COTTON	1 GAL		L

GROUND COVER:	PLANT NAME	SIZE	STANDARD	IM
	MYOPORUM PARVIFOLIUM 'PUHA CREEK' MYOPORUM	1 GAL		L
	SEDUM COPPERTONE STONECROP SEDUM	1 GAL		L
	SENECIO SERPENS BLUE CHALKSTICKS	1 GAL		L

VINES:	PLANT NAME	SIZE	STANDARD	IM
	BOUGAINVILLEA 'SAN DIEGO RED' SAN DIEGO RED	4 GAL	NOTES	L
	CAMPSIS SPECIOSA TRUMPET VINE	4 GAL	NOTES	L

TREES AND SHRUBS PLANTING COUNTS NOTE:
1. CONTRACTOR SHALL BE RESPONSIBLE FOR COUNTING AND VERIFYING ALL PLANT QUANTITIES, AS SHOWN ON THE PLANTING PLANS.
2. CONTRACTOR SHALL SUBMIT PHOTOS OF ALL PLANTS FOR REVIEW AND APPROVAL.
PLANTING MULCH:
ALL SHRUB AND PLANTER AREAS SHALL RECEIVE 3" DEEP LAYER OF ROCK MULCH AND/OR SIREX MULCH BARK MULCH.

NOTE:
ALL TREE SIZES AND PLANT MATERIAL SHALL CONFORM TO SP 11-54 (3 AND 3) PER THE CITY OF CORONA DOWNTOWN REVITALIZATION SPECIFIC PLAN.
OFF-SITE PLANTING CITY REQUIREMENTS:
ALL OFF-SITE R.O.W. LANDSCAPE IMPROVEMENTS (TREES & UNDERSTORY PLANTING) SHALL CONFORM TO THE CITY OF CORONA DOWNTOWN SPECIFIC PLAN LANDSCAPE DESIGN GUIDELINES, AS WELL AS ALL APPLICABLE CITY LANDSCAPE CODES AND REQUIREMENTS. THE LANDSCAPE PLAN SHALL REFLECT THE ACCESSIBLE R.O.W. IMPROVEMENTS PER FIGURE 14C, ROADWAY CROSS SECTIONS OF SECTION V TRANSPORTATION SYSTEM OF THE DOWNTOWN CORONA REVEALATION SP. CONSULT WITH THE DEVELOPMENT SERVICES FOR FINAL CONTRIBUTION OF ULTIMATE R.O.W. DESIGN.

NOTE:
DUE TO CORNER CUT OFF VIEW SIGHT LINES, THERE ARE NO PROPOSED OFF-SITE TREES LOCATED ON BELLE AVE. ALSO, EXISTING TREES ON JOHN STREET SHALL NEED BE REMOVED AS THEY FALL INTO SIGHT LINES. SEE PLANTING PLAN FOR EXIST OFF-SITE TREE LOCATIONS AT ALL ENTRIES AND LOCATIONS OF EXISTING TREES TO BE REMOVED.

NOTE:
SEE PLANTING PLANS FOR 30" x 36" HIGH HEDGE (CALLITEMON VIMINALE 'LITTLE JOHN') WITH LANDSCAPED STRIPBACK AREAS ADJACENT TO STREETS (DWAYN ST & BELLE AVE) TO SCREEN PARKING AREAS.

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ANAHEIM, CA 92801

PROJECT:

NWC 6TH AND MAIN

TBD - 6TH AND MAIN
CORONA, CALIFORNIA

PUBLIC AGENCY SUBMITTAL:
ISSUED FOR BIDDING: N/A
ISSUED FOR CONSTRUCTION: N/A

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1	PRECISE PLAN RESUBMITTAL #2	10-08-25

PRECISE PLAN RESUBMITTAL #2

NADEL PROJECT NO: 19010

PROJECT DATE: 10.08.2025

DRAWING TITLE:

Planting Imagery

DD-04

PLOT DATE: 10/08/25

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NOTE:
IMPROVEMENTS IN THE R.O.W. ALONG MAIN ST. SHALL ALSO BE IN COMPLIANCE WITH CHAPTER VII STREETSCAPE PLAN OF THE CITY OF CORONA DOWNTOWN REVITALIZATION SPECIFIC PLAN. SEE FIGURE 14D.



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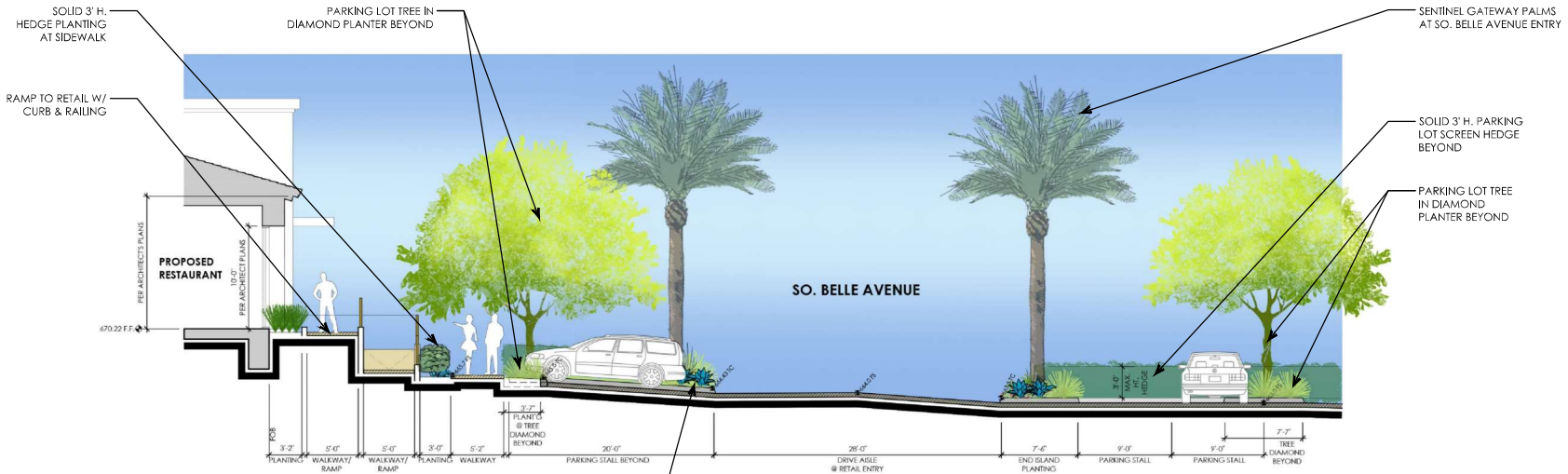
NADEL PROJECT #: 19010
PROJECT DATE: 10.08.2023

PRECISE PLAN RESUBMITTAL #2

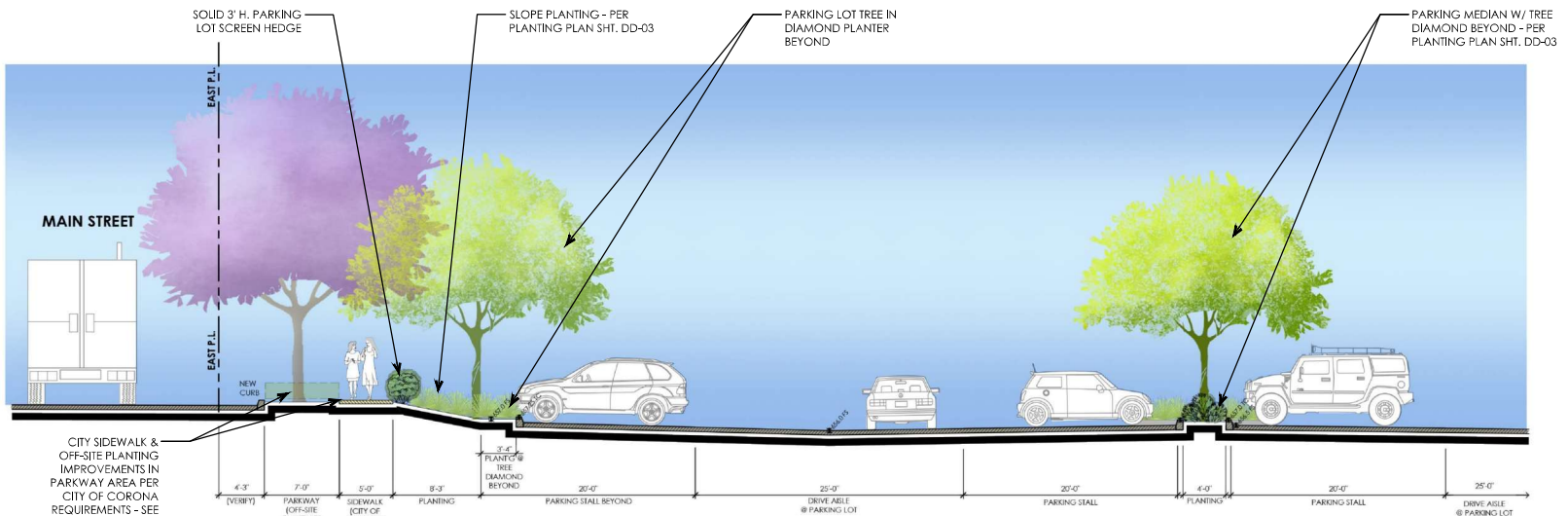
Sections and Elevations

DD-06

PLOT DATE: 10/08/25

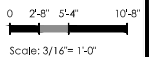


C Retail - Access Ramps
Scale: 3/16"=1'-0"



D Parking Lot - Main Street Adjacent
Scale: 3/16"=1'-0"

TYPICAL OFF-SITE PLANTING CITY REQUIREMENTS:
ALL OFF-SITE R.O.W. LANDSCAPE IMPROVEMENTS (TREE & UNDERSTORY PLANTING) SHALL CONFORM TO THE CITY OF CORONA DOWNTOWN SPECIFIC PLAN LANDSCAPE DESIGN GUIDELINES, AS WELL AS, ALL APPLICABLE CITY LANDSCAPE CODES AND REQUIREMENTS. THE LANDSCAPE PLAN SHALL REFLECT THE ACCURATE R.O.W. IMPROVEMENTS PER FIGURE V-C: ROADWAY CROSS SECTIONS OF SECTION V TRANSPORTATION SYSTEM OF THE DOWNTOWN CORONA REVITALIZATION SP. CONSULT WITH THE DEVELOPMENT SERVICES FOR FINAL CONFIRMATION OF ULTIMATE R.O.W. DESIGN.



ALL DIMENSIONS AND VERTICAL ELEVATIONS UNLESS OTHERWISE SPECIFIED ARE IN FEET AND INCHES. UNLESS OTHERWISE SPECIFIED, ALL DIMENSIONS ARE TO FACE UNLESS NOTED OTHERWISE. THE DESIGNER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES. THE DESIGNER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES. THE DESIGNER SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE APPROPRIATE AGENCIES.



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NWC 6TH AND MAIN

TBD - 6TH AND MAIN
CORONA, CALIFORNIA

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NADEL PROJECT NO: 19010
PROJECT DATE: 10.08.2023

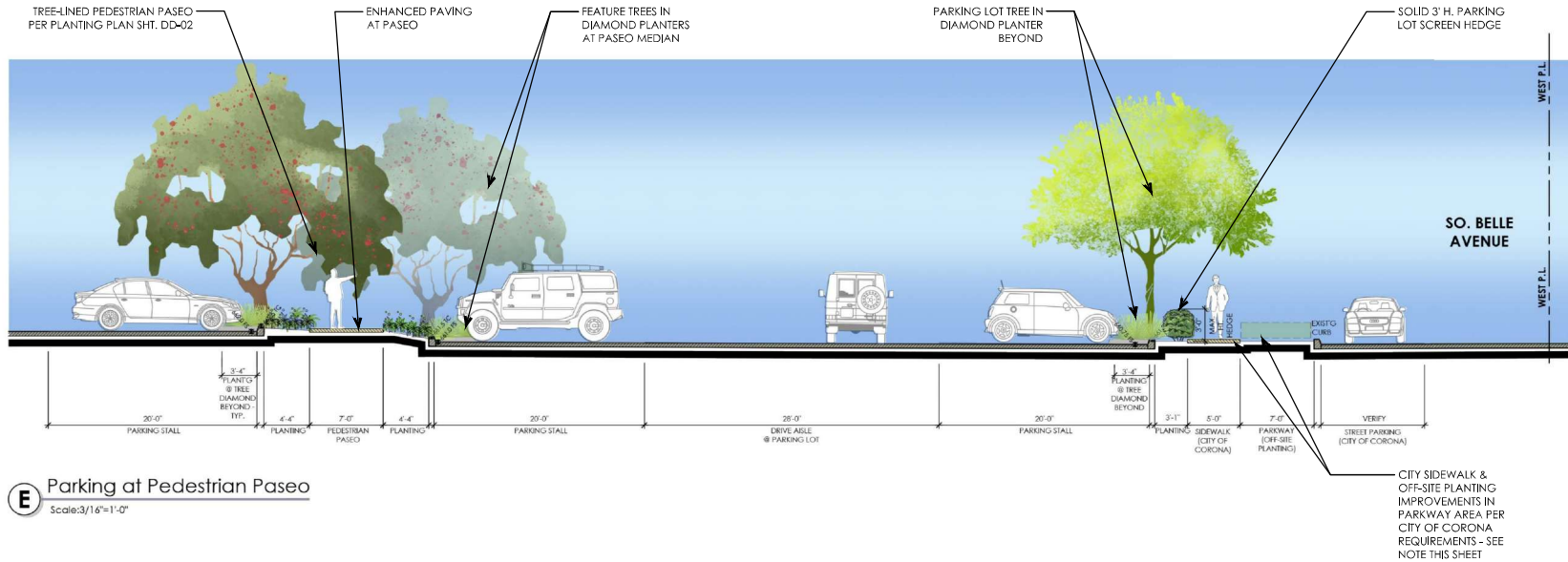
PRECISE PLAN RESUBMITTAL #2

ISSUE:

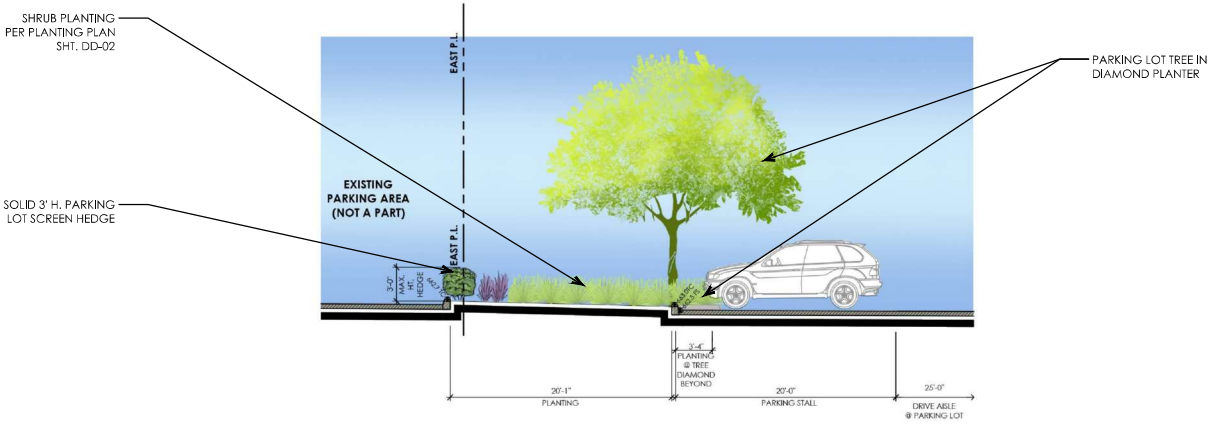
Sections and Elevations

DD-07

PLOT DATE: 10/08/25

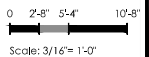


E Parking at Pedestrian Paseo
Scale: 3/16"=1'-0"



F Parking Lot - Existing Parking Lot Adjacent (Not a Part)
Scale: 3/16"=1'-0"

TYPICAL OFF-SITE PLANTING CITY REQUIREMENTS:
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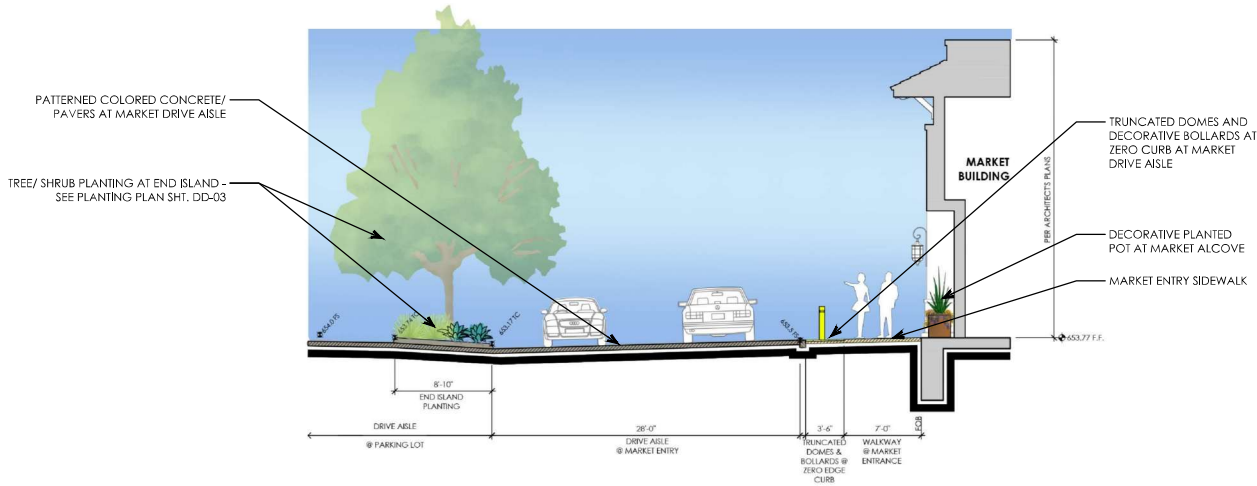
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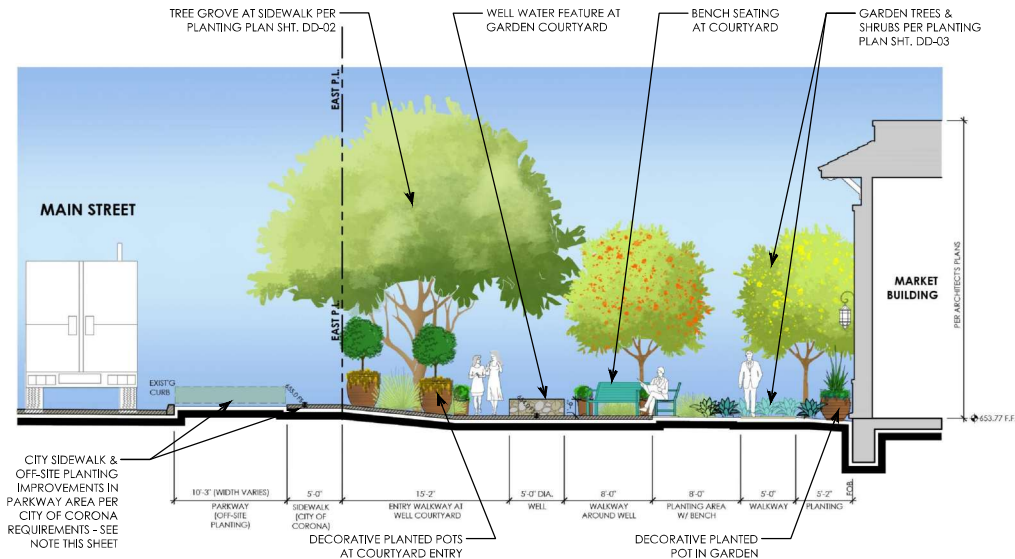
NADEL PROJECT NO: 19010
PROJECT DATE: 10.08.2023

Sections and Elevations

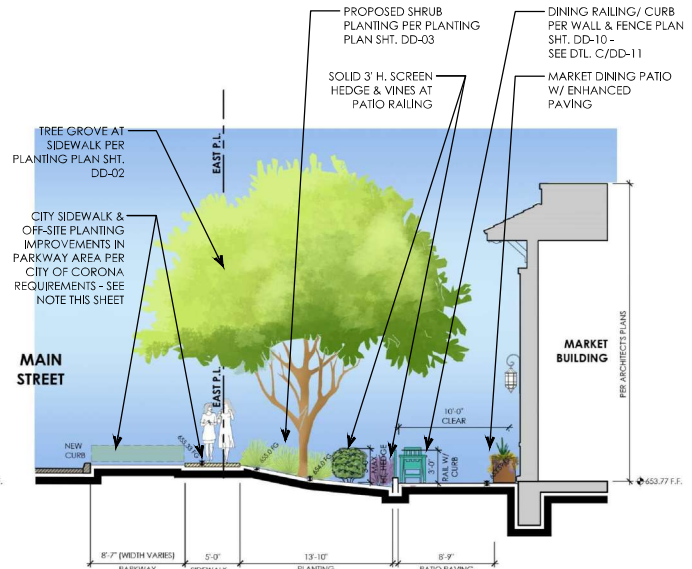
DD-08



G Market Pedestrian Entry
Scale: 3/16"=1'-0"



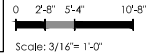
H Courtyard Garden at Well - Main Street Adjacent
Scale: 3/16"=1'-0"

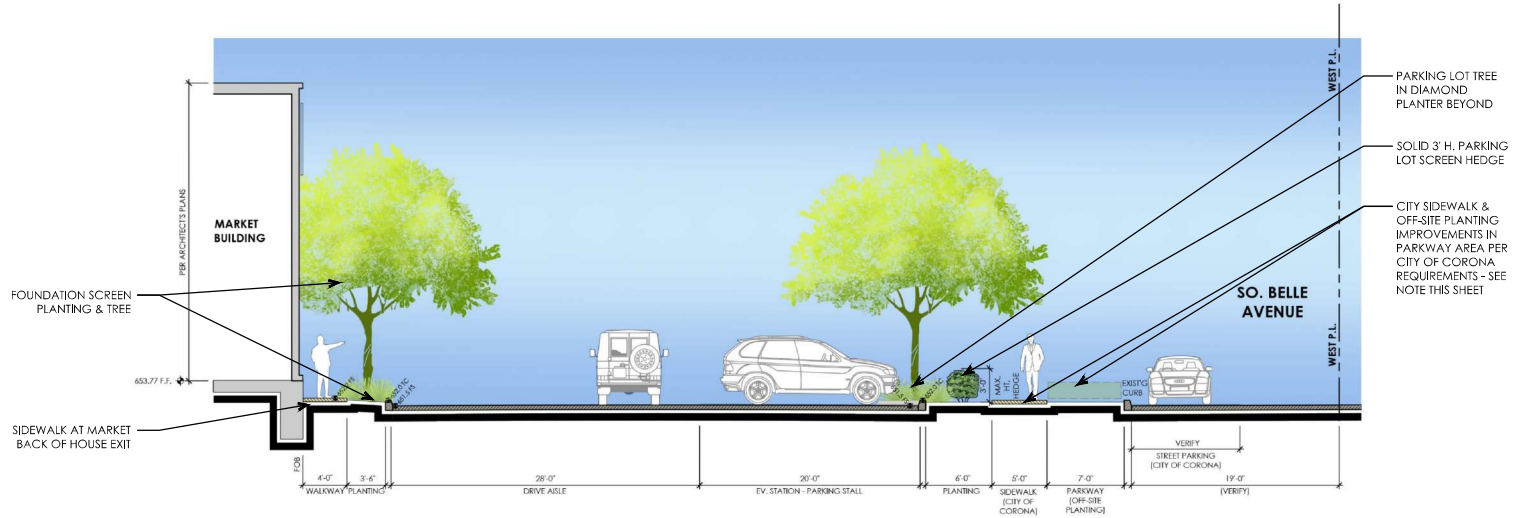


I Market Outdoor Dining - Main Street Adjacent
Scale: 3/16"=1'-0"

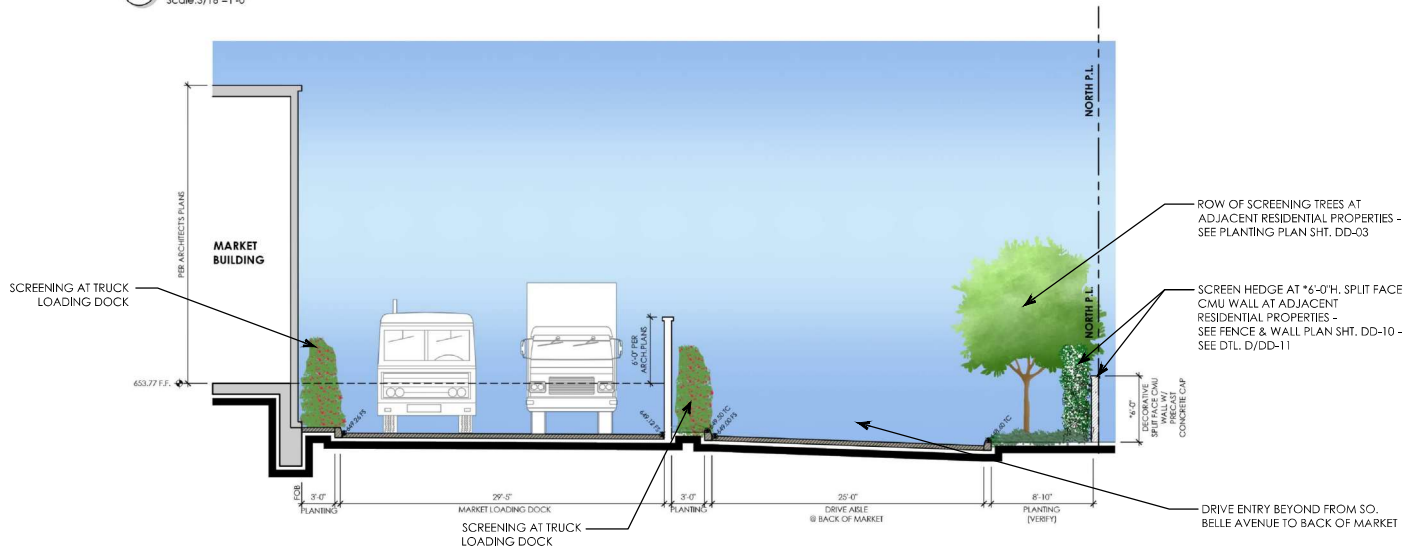
TYPICAL OFF-SITE PLANTING CITY REQUIREMENTS:

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J Parking Lot Drive Aisle at Bell Avenue
Scale: 3/16"=1'-0"

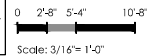


K Loading Dock at Market
Scale: 3/16"=1'-0"

NOTE:
PER THE CITY OF CORONA DOWNTOWN SPECIFIC PLAN SECTION IV DESIGN GUIDELINES (4b and c) THE LAND USE BUFFERING WALL SHOULD BE LOWERED TO A MAXIMUM OF 3'-0" H. IN THE FRONT YARD SETBACK AREA TO ALLOW THE ADJOINING RESIDENTIAL PROPERTY VIEWS FOR TRAFFIC SAFETY. THE WALL SHOULD NOT OBSTRUCT RECIPROCAL ACCESS BETWEEN ADJACENT SITES.

TYPICAL OFF-SITE PLANTING CITY REQUIREMENTS:

ALL OFF-SITE R.O.W. LANDSCAPE IMPROVEMENTS (TREE & UNDERSTORY PLANTING) SHALL CONFORM TO THE CITY OF CORONA DOWNTOWN SPECIFIC PLAN LANDSCAPE DESIGN GUIDELINES, AS WELL AS, ALL APPLICABLE CITY LANDSCAPE CODES AND REQUIREMENTS. THE LANDSCAPE PLAN SHALL REFLECT THE ACCURATE R.O.W. IMPROVEMENTS PER FIGURE V-C: ROADWAY CROSS SECTIONS OF SECTION V TRANSPORTATION SYSTEM OF THE DOWNTOWN CORONA REVITALIZATION SP. CONSULT WITH THE DEVELOPMENT SERVICES FOR FINAL CONFIRMATION OF ULTIMATE R.O.W. DESIGN.



CONSULTANT:



LAND CREATIVE
695 Town Center Drive, #100
Costa Mesa, CA 92626
Phone: 714.352.7588

CLIENT:

NORTHGATE MARKETS

1201 N. MAGNOLIA AVENUE
ANAHEIM, CA 92801

PROJECT:

NWC 6TH AND MAIN

TBD - 6TH AND MAIN
CORONA, CALIFORNIA

PUBLIC AGENCY SUBMITTAL: -

ISSUED FOR BIDDING: N/A

ISSUED FOR CONSTRUCTION: N/A

REVISIONS:

ISSUE DESCRIPTION: ISSUE DATE:

NO. DATE:

PRECISE PLAN RESUBMITTAL #2 10-08-25

PRECISE PLAN RESUBMITTAL #2

PRECISE PLAN RESUBMITTAL #2

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PRECISE PLAN RESUBMITTAL #2

PRECISE PLAN RESUBMITTAL #2

Sections and Elevations

DD-09

PLOT DATE: 10/08/25

PROPOSED WALL LEGEND

TAG:	ITEM:
	RETAINING WALLS AT RETAIL SEE DETAIL A/ SHEET DD-11
	TRELLIS/ RETAINING WALLS AT RETAIL DINING SEE DETAIL B/ SHEET DD-11
	MARKET DINING RAILING/ RETAINING WALL SEE DETAIL C/ SHEET DD-11
	RETAINING SCREEN WALL ALONG PL AT RESIDENTIAL ZONE SEE DETAIL D/ SHEET DD-11

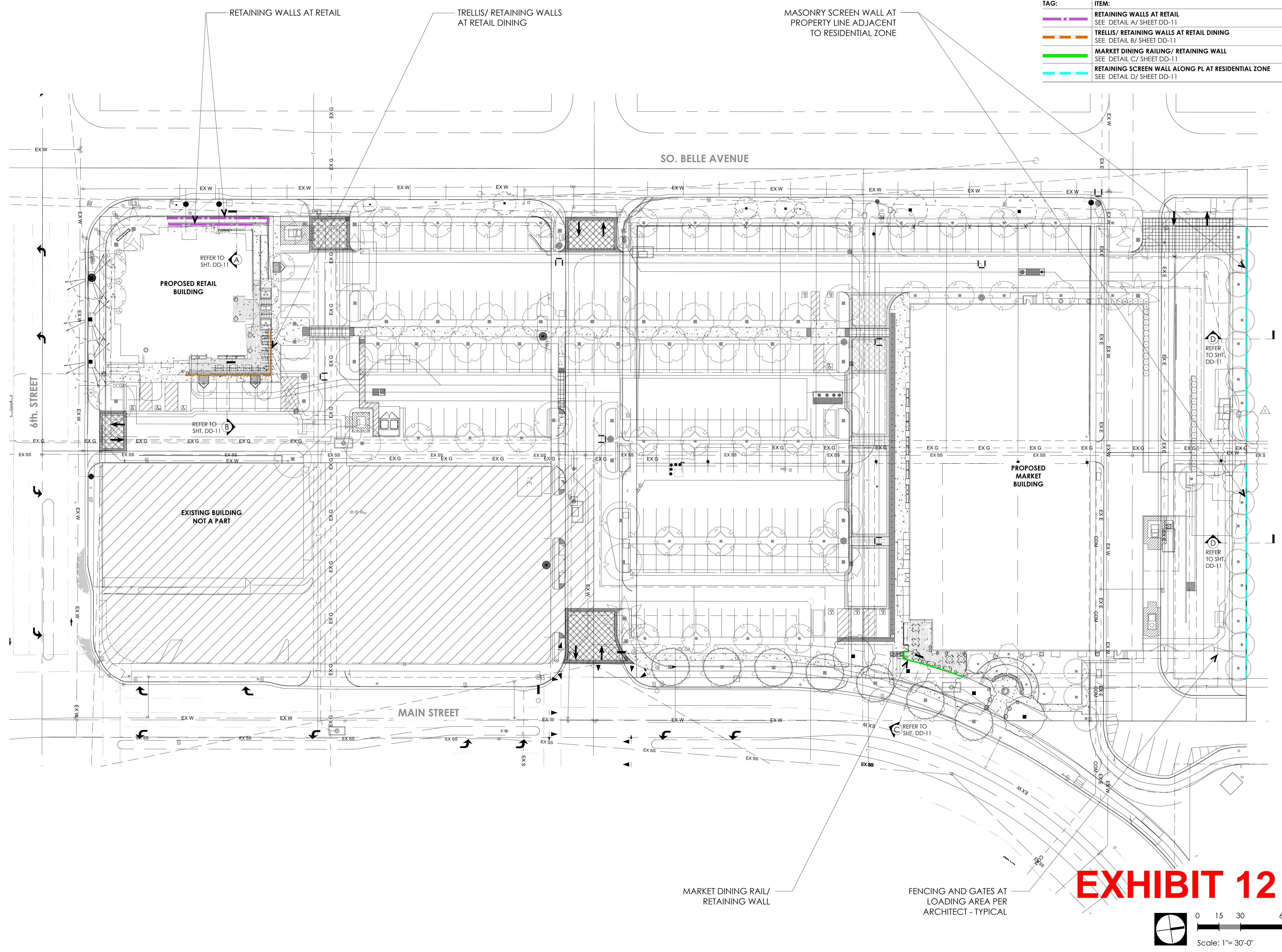
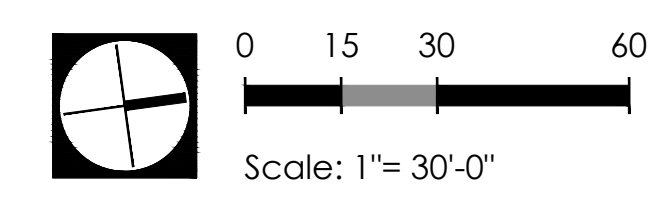


EXHIBIT 12



NADEL

NADEL STUDIO ONE INC

1990 S. BUNDY DRIVE
SUITE 400
LOS ANGELES, CA 90025
T. 310.826.2100
F. 310.826.0182
WWW.NADELARC.COM

PROFESSIONAL STAMP(S):

CONSULTANT:



LAND CREATIVE
695 Town Center Drive, #100
Costa Mesa, CA 92626
Phone: 714 . 352 . 7588

CLIENT:

NORTHGATE MARKETS

1201 N. MAGNOLIA AVENUE
ANAHEIM, CA 92801

PROJECT:

NWC 6TH AND MAIN

TBD - 6TH AND MAIN
CORONA, CALIFORNIA

PUBLIC AGENCY SUBMITTAL: -
ISSUED FOR BIDDING: N/A
ISSUED FOR CONSTRUCTION: N/A

REVISIONS:	ISSUE DESCRIPTION:	ISSUE DATE:
△	PRECISE PLAN RESUBMITTAL #2	10-08-25

NADEL PROJECT No: 19010
PROJECT DATE: 10.08.2025

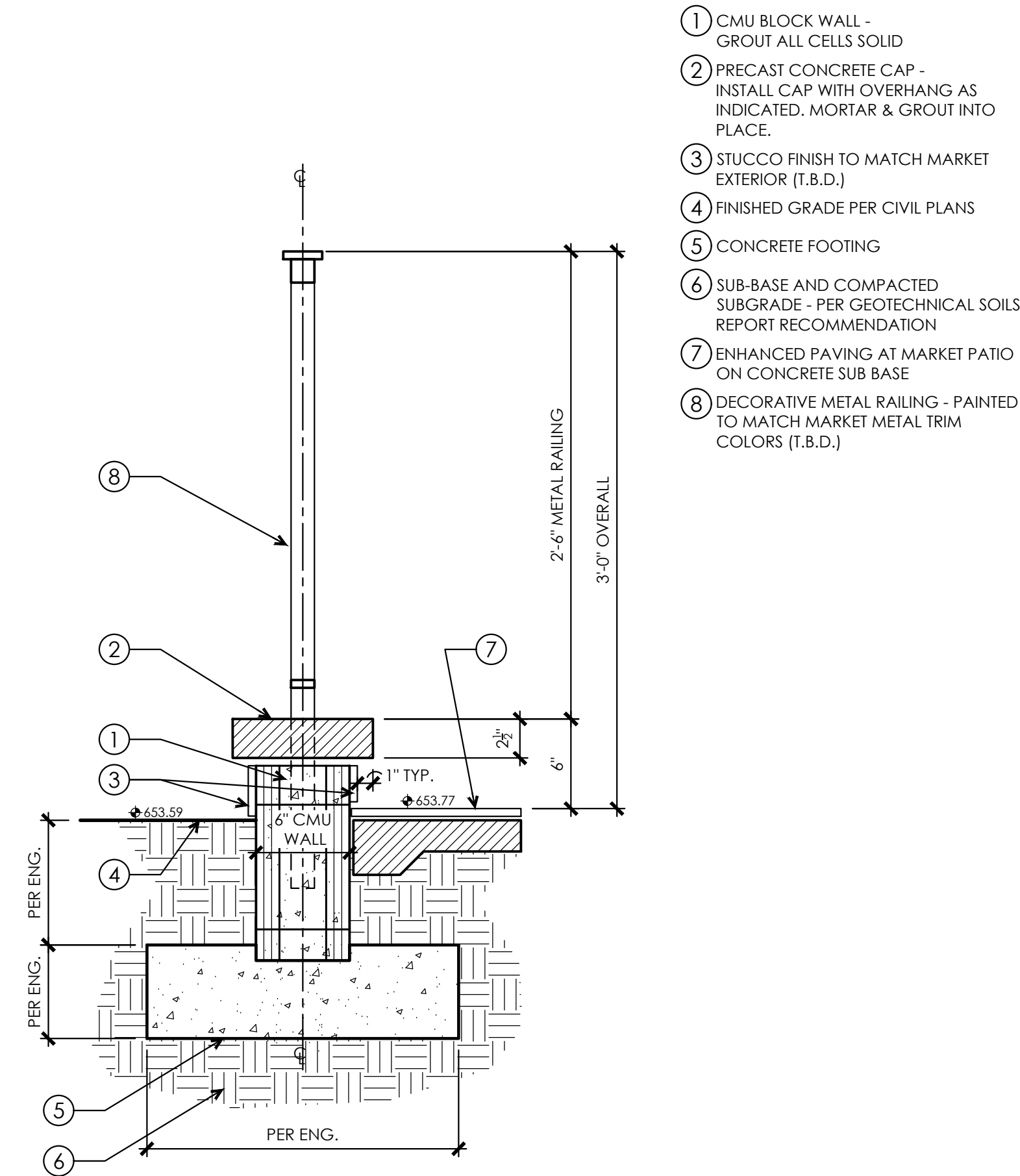
DRAWING TITLE:

Preliminary Wall and Fence Site Plan

DD-10

PLOT DATE: 10/08/25

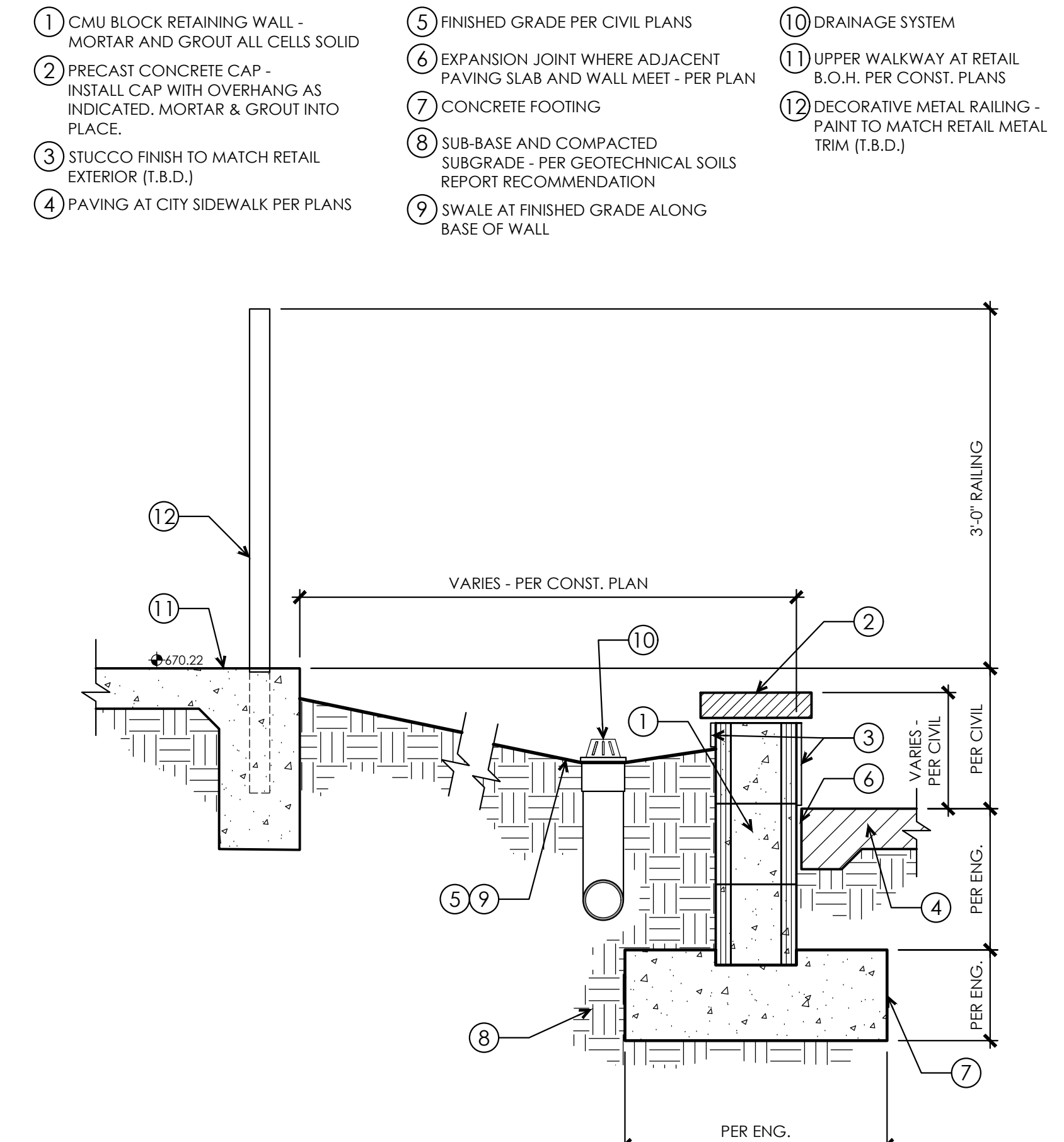
PRECISE PLAN RESUBMITTAL #2



Section
Scale: 1-1/2"=1'-0"

E WALL-WITH-RAIL MARKET DINING RAILING / RET. WALL

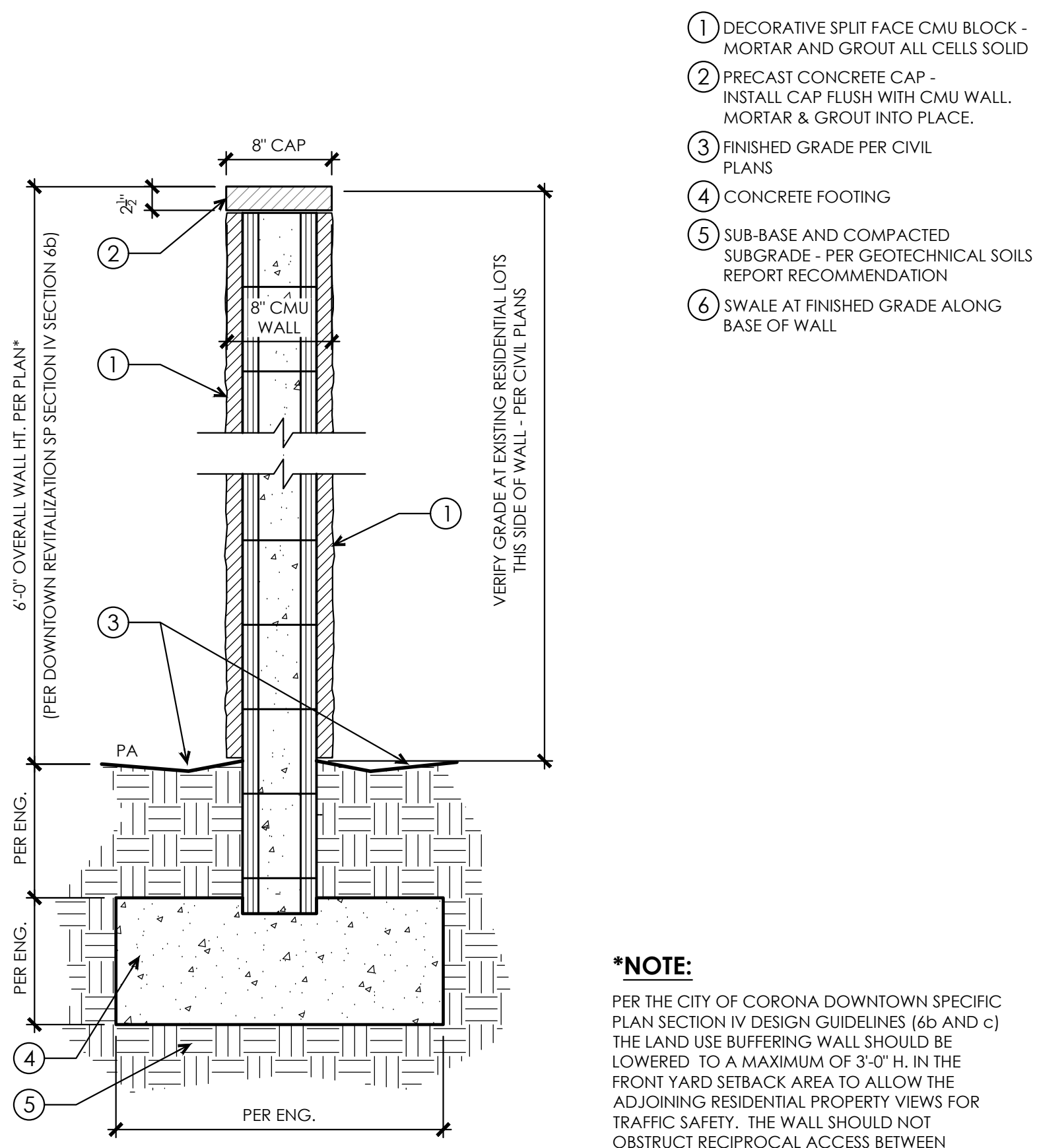
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Section
Scale: 1"=1'-0"

C DOUBLE-RET-WALLS RETAINING WALLS AT RETAIL

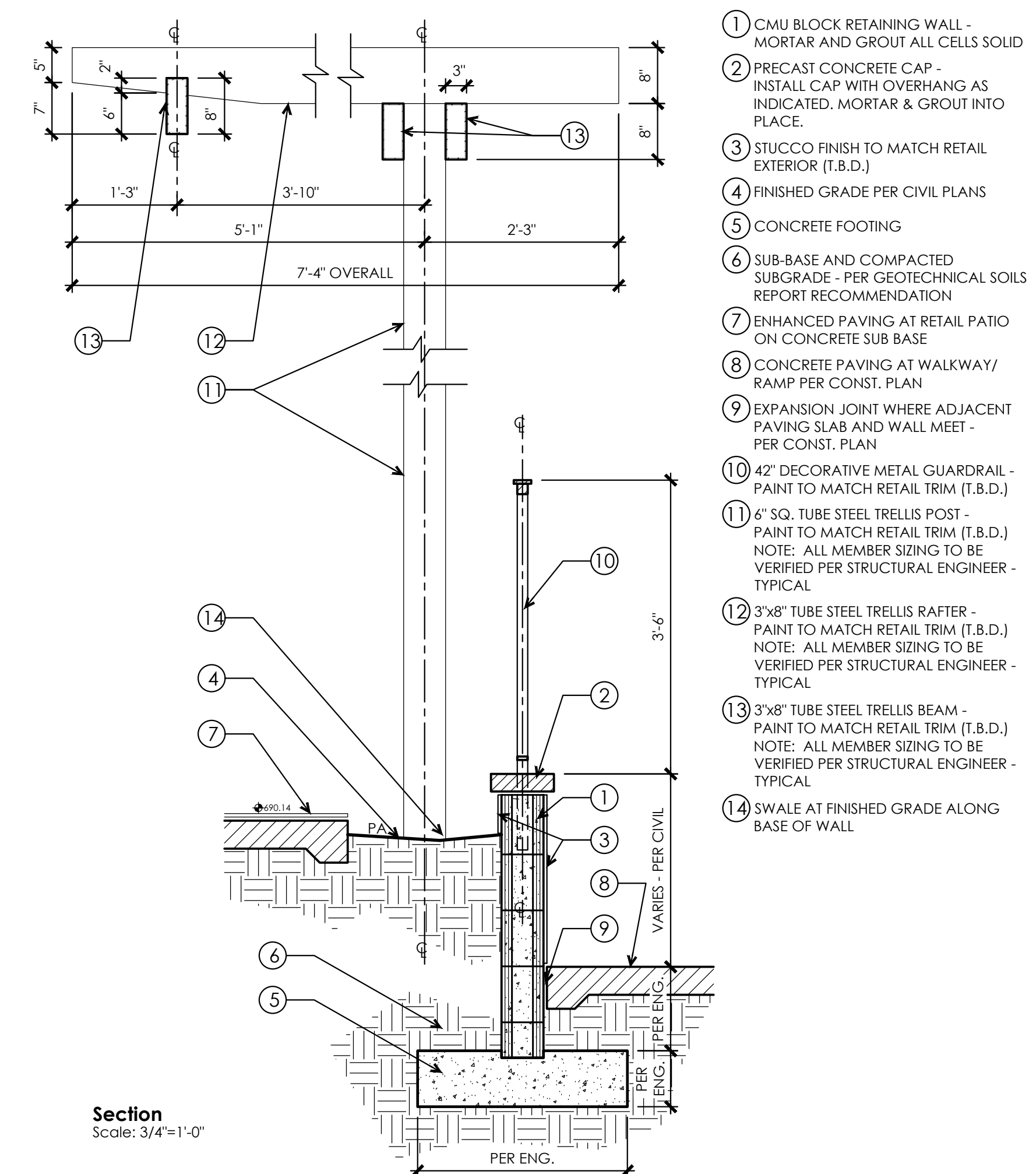
D X-XX-XX-TRELLIS / RET. WALLS AT RETAIL DINING



Masonry Wall Section @ Residential PL
Scale: 1"=1'-0"

F FS-WALL-AT-PL SCREEN WALL AT PROPERTY LINE

XXXXXX



Section
Scale: 3/4"=1'-0"



LAND CREATIVE
695 Town Center Drive, #100
Costa Mesa, CA 92626
Phone: 714 . 352 . 7588

NORTHGATE MARKETS

1201 N. MAGNOLIA AVENUE
ANAHEIM, CA 92801

NWC 6TH AND MAIN

TBD - 6TH AND MAIN
CORONA, CALIFORNIA

PUBLIC AGENCY SUBMITTAL: -
ISSUED FOR BIDDING: N/A
ISSUED FOR CONSTRUCTION: N/A

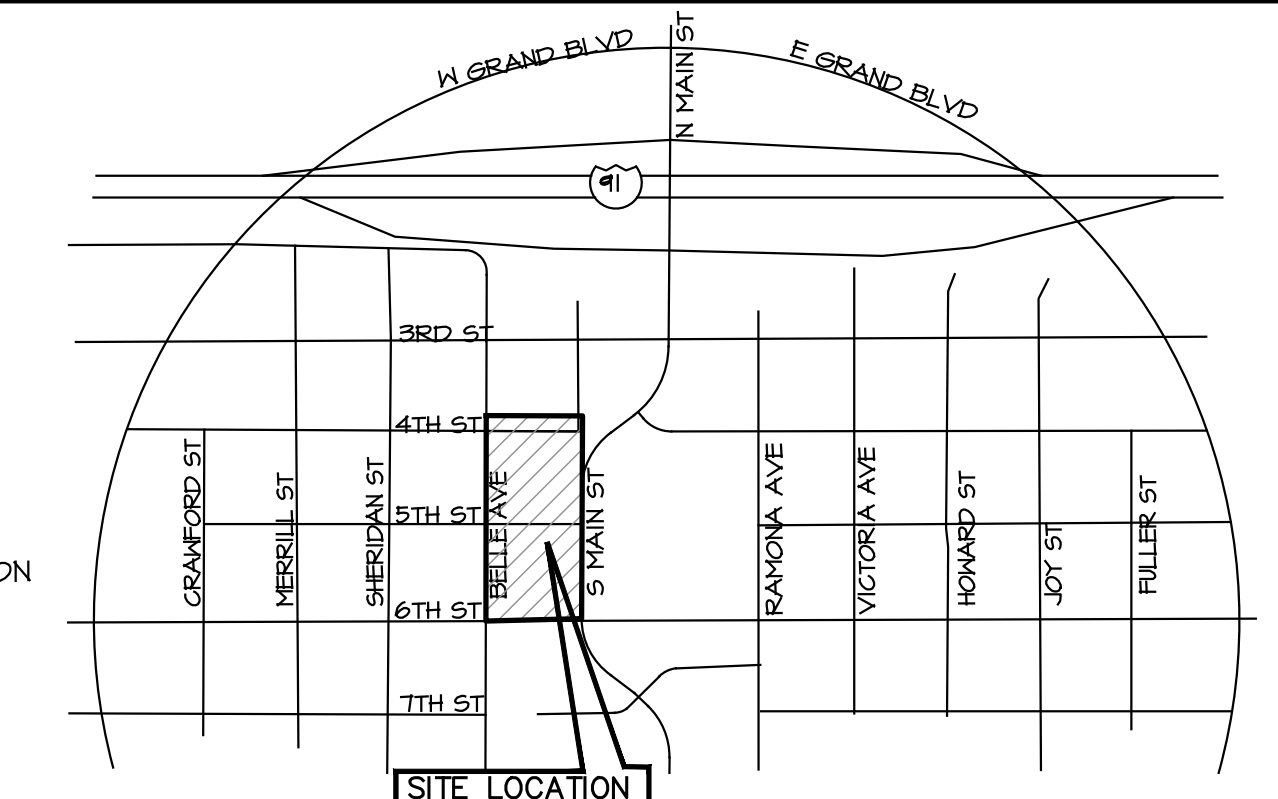
REVISIONS:	ISSUE DESCRIPTION:	ISSUE DATE:
Δ	PRECISE PLAN RESUBMITTAL #2	10-08-25

NADEL PROJECT No: 19010
PROJECT DATE: 10.08.2025

Details

DD-11

IN THE CITY OF CORONA, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA
DEVELOPMENT PLAN REVIEW-2023-0021
NORTHGATE - 6TH & MAIN STREET



LEGAL DESCRIPTION

THIS PROPERTY SURVEYED AND SHOWN HEREON IS THE SAME PROPERTY AS DESCRIBED IN SCHEDULE A OF FIRST AMERICAN TITLE INSURANCE COMPANY PRELIMINARY REPORT ORDER NO. NC5-1200940-SAI, DATED NOVEMBER 21, 2023. THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF CORONA, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

APN(S): 117-144-017
 LOTS 7 THROUGH 12, INCLUSIVE, OF BLOCK 145 OF THE SOUTH RIVERSIDE TOWNSITE, IN THE CITY OF CORONA, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN BY MAP RECORDED IN BOOK 9, PAGES 6 AND 8 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAN BERNARDINO COUNTY, TOGETHER WITH ALL THAT PORTION OF THE EAST-WEST UNNAMED ALLEY (16 FEET WIDE), AS VACATED AND CLOSED TO PUBLIC USE BY RESOLUTION NO. 16-70, RECORDED JUNE 24, 1916 AS INSTRUMENT NO. 90734 AND THE EASTERLY 6 FEET OF BELLE AVENUE VACATED AND CLOSED TO PUBLIC USE BY RESOLUTION 16-81, RECORDED JULY 16, 1916 AS INSTRUMENT NO. 103437, BOTH OF OFFICIAL RECORDS OF RIVERSIDE COUNTY.

APN(S): 117-142-006
 THE NORTH 25 FEET OF LOT 10 IN BLOCK 154 OF SOUTH RIVERSIDE COLONY LANDS, AS SHOWN BY MAP ON FILE IN BOOK 9, PAGES 6 AND 8 OF MAPS, SAN BERNARDINO COUNTY RECORDS.

APN(S): 117-142-007
 THE SOUTH 25 FEET OF LOT 10, BLOCK 154, SOUTH RIVERSIDE TOWNSITE, IN THE CITY OF CORONA, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN BY MAP ON FILE IN BOOK 9, PAGES 6 AND 8 OF MAPS, SAN BERNARDINO COUNTY RECORDS.

APN(S): 117-142-008 & 117-142-009
 LOTS 11 AND 12, BLOCK 154, OF THE SOUTH RIVERSIDE TOWNSITE, IN THE CITY OF CORONA, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 9, PAGES 6 AND 8, OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAN BERNARDINO COUNTY. EXCEPTING THEREFROM THE EASTERLY RECTANGULAR 70 FEET OF LOTS 11 AND 12.

APN(S): 117-142-015
 LOT 7 OF BLOCK 154, LANDS OF SOUTH RIVERSIDE LAND AND WATER COMPANY, IN THE CITY OF CORONA, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN BY MAP ON FILE IN BOOK 9, PAGES 6 AND 8 OF MAPS, RECORDS OF SAN BERNARDINO COUNTY, CALIFORNIA.

TOGETHER WITH THAT PORTION OF FOURTH STREET, AS DESCRIBED IN RESOLUTION NO. 91-122 OF THE CITY COUNCIL OF THE CITY OF CORONA, ABANDONING A PORTION OF FOURTH STREET, RECORDED JUNE 30, 1993 AS INSTRUMENT NO. 252481, OF OFFICIAL RECORDS, WHICH WOULD PASS WITH SAID LAND BY OPERATION OF LAW.

ALSO TOGETHER WITH THAT PORTION OF THE NORTH/SOUTH ALLEY ALONG THE EASTERLY BOUNDARY OF SAID LAND, AS DESCRIBED IN RESOLUTION NO. 91-122 OF THE CITY COUNCIL OF THE CITY OF CORONA, ABANDONING A PORTION OF SAID ALLEY, RECORDED JUNE 30, 1993 AS INSTRUMENT NO. 252481, OF OFFICIAL RECORDS, WHICH WOULD PASS WITH SAID LAND BY OPERATION OF LAW.

APN(S): 117-142-016
 LOT 6 IN BLOCK 154 OF THE SOUTH RIVERSIDE TOWNSITE, AS SHOWN BY MAP ON FILE IN BOOK 9, PAGE 8 OF MAPS, RECORDS OF SAN BERNARDINO COUNTY, CALIFORNIA.

TOGETHER WITH THAT PORTION OF PARCELS A AND B VACATED BY RESOLUTION NO. 91-122, RECORDED JUNE 30, 1993 AS INSTRUMENT NO. 252481 OF OFFICIAL RECORDS, THAT WOULD PASS BY OPERATION OF LAW.

APN(S): 117-142-017
 LOTS 4 AND 5 IN BLOCK 154 OF THE SOUTH RIVERSIDE TOWNSITE, AS SHOWN BY MAP ON FILE IN BOOK 9, PAGE 8 OF MAPS, RECORDS OF SAN BERNARDINO COUNTY, CALIFORNIA;

TOGETHER WITH THAT PORTION OF THE WEST HALF OF WASHBURN AVENUE (NOW KNOWN AS MAIN STREET), ADJOINING SAID LOTS 4 AND 5 ON THE EAST, AS VACATED AND CLOSED TO PUBLIC USE BY RESOLUTION NO. 3818, RECORDED NOVEMBER 10, 1910 AS INSTRUMENT NO. 113224 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.

AND TOGETHER WITH THAT PORTION OF PARCELS A AND B VACATED BY RESOLUTION NO. 91-122, RECORDED JUNE 30, 1993 AS INSTRUMENT NO. 252481 OF OFFICIAL RECORDS, THAT WOULD PASS BY OPERATION OF LAW.

APN(S): 117-142-018
 LOT 8, BLOCK 154, OF SOUTH RIVERSIDE TOWNSITE, IN THE CITY OF CORONA, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS PER MAP RECORDED IN BOOK 9, PAGES 6 AND 8, INCLUSIVE OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

TOGETHER WITH THAT PORTION OF THE NORTH/SOUTH ALLEY ALONG THE EASTERLY BOUNDARY OF SAID LAND, AS DESCRIBED IN RESOLUTION NO. 91-122 OF THE CITY COUNCIL OF THE CITY OF CORONA, ABANDONING A PORTION OF SAID ALLEY, RECORDED JUNE 30, 1993 AS INSTRUMENT NO. 252481, OF OFFICIAL RECORDS, WHICH WOULD PASS WITH SAID LAND BY OPERATION OF LAW.

APN(S): 117-142-019
 LOT 9 OF BLOCK 154, OF SOUTH RIVERSIDE LAND AND WATER COMPANY, IN THE CITY OF CORONA, COUNTY OF RIVERSIDE, STATE OF CALIFORNIA, AS SHOWN BY MAP ON FILE IN BOOK 9, PAGES 6 AND 8 OF MAPS, RECORDS OF SAN BERNARDINO COUNTY, CALIFORNIA.

TOGETHER WITH THAT PORTION OF THE NORTH/SOUTH ALLEY ALONG THE EASTERLY BOUNDARY OF SAID LAND, AS DESCRIBED IN RESOLUTION NO. 91-122 OF THE CITY COUNCIL OF THE CITY OF CORONA, ABANDONING A PORTION OF SAID ALLEY, RECORDED JUNE 30, 1993 AS INSTRUMENT NO. 252481, OF OFFICIAL RECORDS, WHICH WOULD PASS WITH SAID LAND BY OPERATION OF LAW.

APN(S): 117-142-020
 LOTS 1, 2 AND 3 IN BLOCK 154 OF SOUTH RIVERSIDE TOWNSITE, AS SHOWN BY MAP ON FILE IN BOOK 9, PAGES 6 THROUGH 8 OF MAPS, RECORDS OF SAN BERNARDINO COUNTY, CALIFORNIA.

APN(S): 117-103-026
 LOT 12 IN BLOCK 168 OF SOUTH RIVERSIDE LAND AND WATER COMPANY, AS SHOWN BY MAP ON FILE IN BOOK 9 PAGES 6, 7 AND 8 OF MAPS, RECORDS OF SAN BERNARDINO COUNTY, CALIFORNIA;

TOGETHER WITH THE PORTION OF THE WESTERLY HALF OF THE ALLEY ADJOINING SAID LOT 12 ON THE EAST AS VACATED BY RESOLUTION NO. 91-122, RECORDED JUNE 30, 1993 AS INSTRUMENT NO. 93-252481 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA;

AND TOGETHER WITH THAT PORTION OF THE NORTH HALF OF FOURTH STREET LYING BETWEEN THE SOUTHERLY PROLONGATIONS OF THE WESTERLY LINE OF SAID LOT 12 AND THE EASTERLY LINE OF THE WESTERLY HALF OF SAID ALLEY AS VACATED BY SAID RESOLUTION NO. 91-122.

APN 117-103-027
 LOT 1 IN BLOCK 168 OF SOUTH RIVERSIDE TOWNSITE, AS SHOWN BY MAP ON FILE IN BOOK 9, PAGE(S) 6 OF MAPS, RECORDS OF RIVERSIDE COUNTY, CALIFORNIA;

TOGETHER WITH THE PORTION OF THE EASTERLY HALF OF THE ALLEY ADJOINING SAID LOT 1 ON THE WEST AS VACATED BY RESOLUTION NO. 91-122, RECORDED JUNE 30, 1993 AS INSTRUMENT NO. 93-252481 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA;

AND TOGETHER WITH THAT PORTION OF THE NORTH HALF OF FOURTH STREET LYING BETWEEN THE SOUTHERLY PROLONGATIONS OF THE EASTERLY HALF OF SAID ALLEY AND THE EASTERLY LINE OF SAID LOT 1 AS VACATED BY SAID RESOLUTION NO. 91-122;

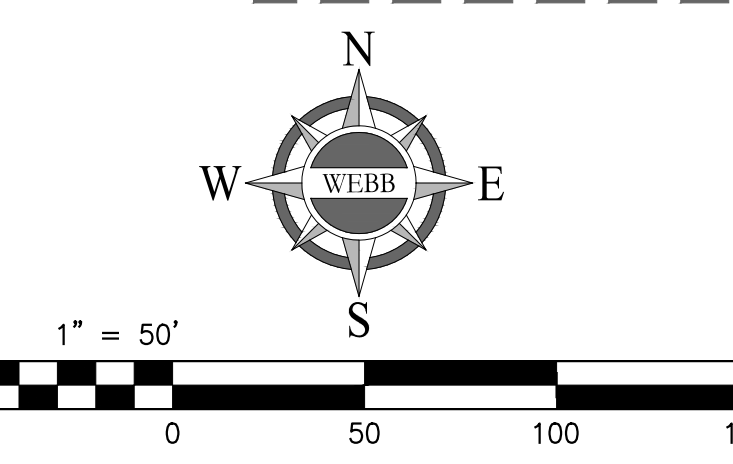
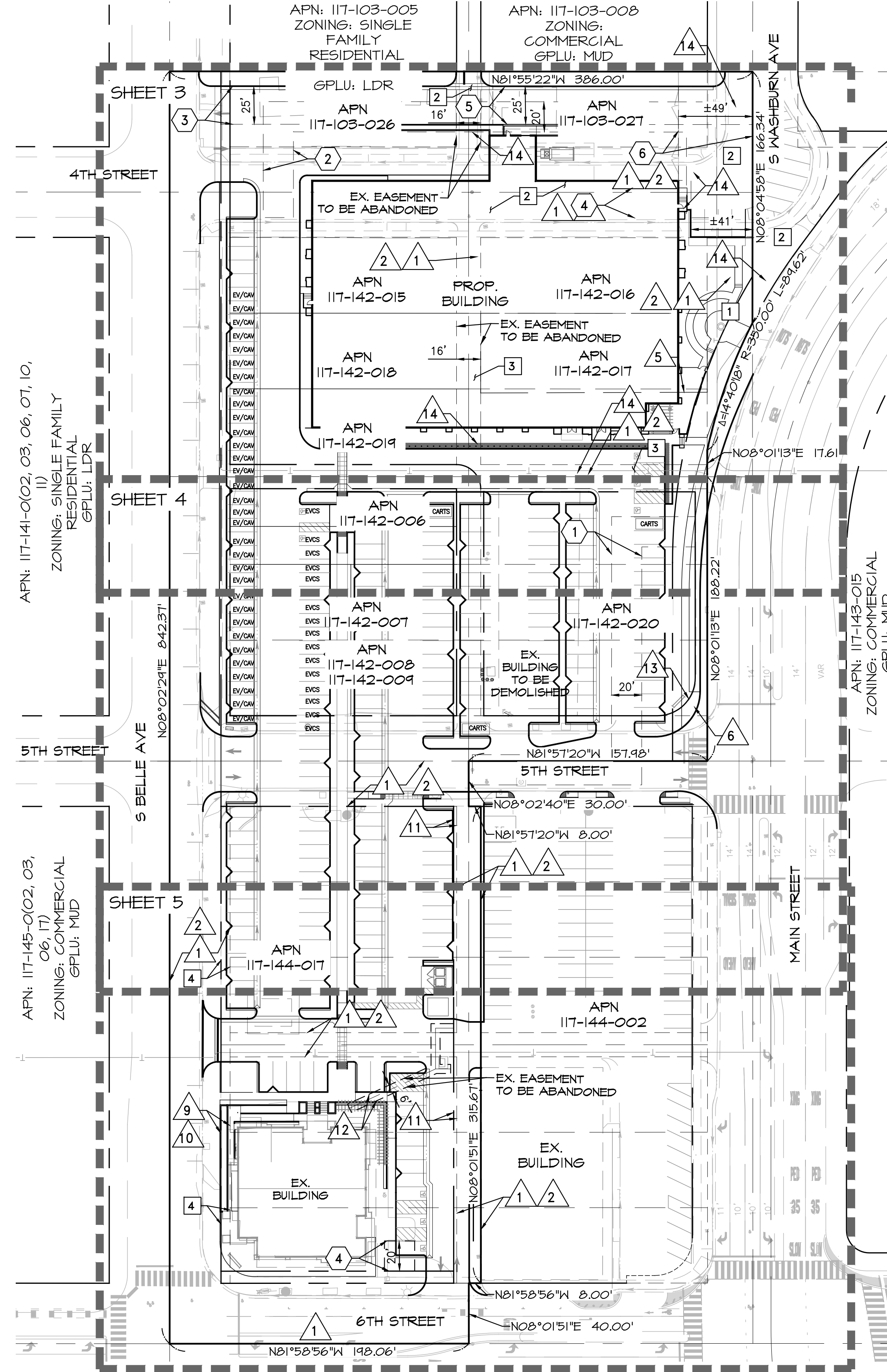
AND TOGETHER WITH THAT PORTION OF THE WEST HALF OF WASHBURN AVENUE ADJOINING LOT 1 ON THE EAST AS VACATED BY SAID RESOLUTION NO. 91-122.

DRAINAGE NOTE

PROJECT PROPOSES STORM DRAIN CONNECTIONS AT MAIN STREET EXISTING STORM DRAIN (DRAWING NO. 1486 DATED 8-30-1967).

EXISTING UTILITY NOTE

AS-BUILT PLANS FOR MAJORITY OF EXISTING UTILITIES ARE NOT AVAILABLE FOR THIS PROJECT SITE PER CITY OF CORONA AND COUNTY OF RIVERSIDE. CONCEPTUAL UTILITY POINTS OF CONNECTIONS SHOWN AT BELL AVENUE AND MAIN STREET FOR INITIAL COORDINATION PURPOSES.



UTILITY COMPANIES

WATER: CITY OF CORONA (951) 736-2234
 CITY OF CORONA (951) 736-2234
 SEWER: SOUTHERN CALIFORNIA EDISON 1-(800) 655-4555
 SO. CAL. GAS COMPANY 1-(877) 238-0042
 TELEPHONE: CHARTER COMMUNICATIONS 1-(833) 644-4254

ENGINEER

ALBERT A. WEBB ASSOCIATES
 3788 MCGRAY STREET
 RIVERSIDE, CA 92506
 CONTACT: JENNIFER GILLEN
 TEL: 951-320-6041

PROPOSED SITE

NET SITE AREA 44.88 AC
 BUILDING TOTAL 46,430 SF
 SITE COVERAGE 22.1%
 FAR 0.22

BUILDING AREAS

MARKET 40,000 SF
 BANK 3,633 SF
 REST 3,247 SF
 TOTAL 46,930 SF

SHEET INDEX

SHEET 1: TITLE SHEET
 SHEET 2: PROPOSED SITE PLAN
 SHEET 3-5: CONCEPTUAL GRADING PLANS
 SHEET 6: CROSS SECTIONS
 SHEET 7: CONCEPTUAL WET UTILITY PLAN

APPLICANT/DEVELOPER

NORTHGATE GONZALEZ REAL ESTATE
 1201 N. MAGNOLIA AVE
 ANAHEIM, CA 92801
 CONTACT: ELIZABETH RESENDIZ
 TEL: 714-687-1031

PROPOSED PARKING

PARKING REQUIRED 258 STALLS
 PARKING PROVIDED 271 STALLS
 STREET PARKING 35 STALLS
 LOT PARKING 236 STALLS
 PARKING RATIO 5.69/1000

ARCHITECT

NADEL ARCHITECTURE & PLANNING
 1940 S. BUNDY DRIVE, SUITE 400
 LOS ANGELES, CA 90025
 CONTACT: DAVID ANDERSON
 TEL: 310-826-2100

LAND USE

EXISTING LAND USE: COMMERCIAL
 PROPOSED LAND USE: COMMERCIAL
 EXISTING ZONING: DOWNTOWN
 SINGLE FAMILY, GATEWAY BUSINESS
 PROPOSED ZONING: DOWNTOWN
 SPECIFIC PLAN: (SP-48-01) DOWNTOWN CORONA REVITALIZATION
 EXISTING GENERAL PLAN LAND USE: MUD, GC AND LDR
 PROPOSED GENERAL PLAN LAND USE: MUD (MIXED USE DOWNTOWN)

PARCEL AREA TABLE		
PARCEL NO.	GROSS AREA	NET AREA
PARCEL 1	5.68 ACRES	4.88 ACRES

EASEMENT TABLE

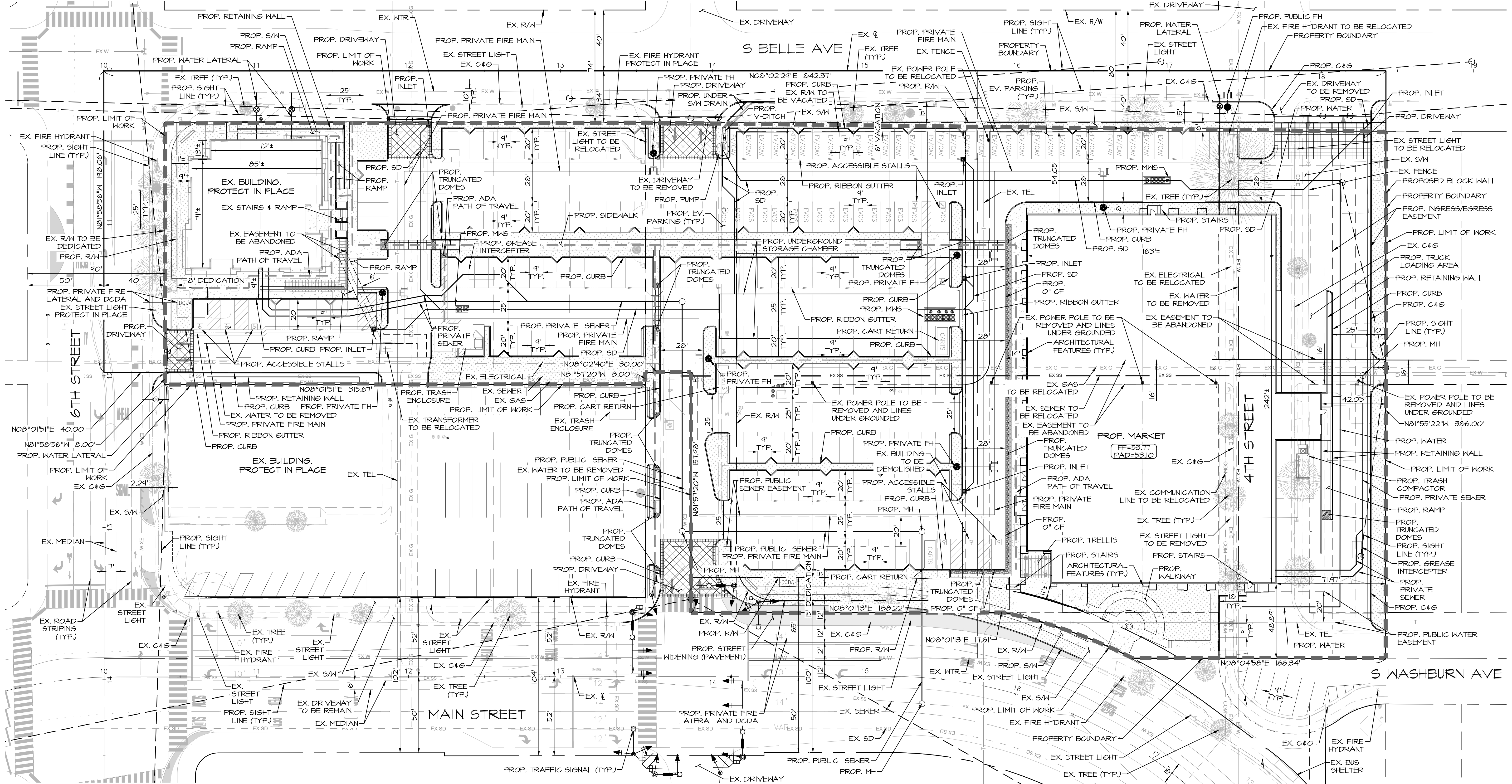
- 1 A RIGHT OF WAY RESERVED TO THE SOUTH RIVERSIDE LAND AND WATER COMPANY, ITS SUCCESSORS OR ASSIGNS, FOR THE CONSTRUCTION AND MAINTENANCE OF ALL NECESSARY WATER PIPES, DITCHES, FLUMES AND CONDUITS, FOR ALL PURPOSES OF IRRIGATION AND DOMESTIC USE, THE EXACT LOCATION THEREOF NOT BEING DISCLOSED BY THE RECORDS. [AFFECTS STREETS AND ALLEYS]
- 2 AN EASEMENT SHOWN OR DEDICATED ON THE MAP OF SOUTH RIVERSIDE TOWNSITE RECORDED DECEMBER 12, 1891 AND ON FILE IN BOOK 9, PAGE(S) 6 AND 8, OF SAN BERNARDINO COUNTY TRACT MAPS. FOR: RIGHT TO LAY PIPES AND MAKE WATER DITCHES FOR PURPOSES OF CONVEYING WATER AND RUN STREET CARS, PROPULSED BY HORSES, STEAM, ELECTRICITY OR OTHER MOTIVE POWER AND INCIDENTAL PURPOSES. [AFFECT STREETS AND ALLEY]
- 5 THE EFFECT OF AN AGREEMENT RELATING TO A PARTY WALL ON THE NORTHERLY LINE OF THE LAND ADJOINING ON THE SOUTH, EXECUTED BY T.S. MCNAIR AND HELEN V. MCNAIR, HUSBAND AND WIFE, AS OWNERS OF THE PROPERTY HEREIN DESCRIBED, AND ALFRED C. WILSON AND OPAL M. WILSON, HUSBAND AND WIFE, AS OWNERS OF SAID ADJOINING PROPERTY, RECORDED JANUARY 27, 1954 IN BOOK 1548, PAGE 565 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA. REFERENCE HEREBY BEING MADE TO THE RECORD FOR FULL PARTICULARS.
- 6 AN EASEMENT FOR STREETS AND INCIDENTAL PURPOSES IN THE DOCUMENT RECORDED JUNE 03, 1968 AS INSTRUMENT NO. 1468-51414 OF OFFICIAL RECORDS.. [CANNOT BE PLOTTED FROM RECORD INFORMATION DUE TO MISSING DISTANCES] [APPROXIMATE LOCATION SHOWN HEREON]
- 8 THE FACT THAT THE LAND LIES WITHIN THE BOUNDARIES OF THE WASHBURN AVENUE (NOW KNOWN AS MAIN STREET) REDEVELOPMENT PROJECT AREA, AS DISCLOSED BY THE DOCUMENT RECORDED NOVEMBER 10, 1910 AS INSTRUMENT NO. 113224 OF OFFICIAL RECORDS.
- 9 AN EASEMENT FOR CONSTRUCT, MAINTAIN, OPERATE, REPLACE, ENLARGE, REMOVE AND RENEW SANITARY SEWERS AND STORM DRAINS AND APPURTENANT STRUCTURES IN, UPON, OVER, BENEATH, UNDER, BELOW, ABOVE AND ACROSS SAID STREET OR PORTION THEREOF PROPOSED HEREIN TO BE VACATED AND PURSUANT TO ANY EXISTING FRANCHISES OR RENEWALS THEREOF OR OTHERWISE, AND TO CONSTRUCT, MAINTAIN, OPERATE, REPLACE, ENLARGE, REMOVE AND RENEW LINES OF PIPE, CONDUITS, CABLES, WIRES, GAS MAINS, UNDERGROUND CONDUITS, POLES, ANCHORS, WATER MAINS, AND OTHER CONVENIENT STRUCTURES, EQUIPMENT AND FIXTURES FOR THE OPERATION OF WATER, SEWAGE, AND GAS PIPELINES AND FOR THE TRANSPORTATION AND DISTRIBUTION OF WATER, ELECTRIC ENERGY, SEWAGE, GAS, TELEPHONE AND OTHER COMMUNICATION SERVICES AND INCIDENTAL PURPOSES, RECORDED JUNE 24, 1916 AS INSTRUMENT NO. 90734 OF OFFICIAL RECORDS, IN FAVOR OF SOUTHERN CALIFORNIA EDISON COMPANY, SOUTHERN CALIFORNIA GAS COMPANY AND PACIFIC TELEPHONE COMPANY.
- 10 THE RIGHTS, IF ANY, OF A CITY, PUBLIC UTILITY OR SPECIAL DISTRICT TO PRESERVE A PUBLIC EASEMENT IN BELLE AVENUE AS THE SAME WAS VACATED BY THE DOCUMENT RECORDED JULY 16, 1916 AS INSTRUMENT NO. 103437 OF OFFICIAL RECORDS.
- 11 AN EASEMENT FOR PUBLIC STREET AND INCIDENTAL PURPOSES, RECORDED NOVEMBER 29, 1916 AS INSTRUMENT NO. 16-182042 OF OFFICIAL RECORDS, IN FAVOR OF CITY OF CORONA, A MUNICIPAL CORPORATION AND A POLITICAL SUBDIVISION OF THE STATE OF CALIFORNIA.
- 12 AN EASEMENT FOR UNDERGROUND ELECTRICAL SUPPLY SYSTEMS AND COMMUNICATION SYSTEMS AND INCIDENTAL PURPOSES, RECORDED MARCH 5, 1932 AS INSTRUMENT NO. 38151 OF OFFICIAL RECORDS, IN FAVOR OF SOUTHERN CALIFORNIA EDISON COMPANY, A CORPORATION.
- 13 AN EASEMENT FOR PUBLIC ALLEY, PUBLIC STREET AND INCIDENTAL PURPOSES, RECORDED AUGUST 18, 1906 AS INSTRUMENT NO. 1486-191561 OF OFFICIAL RECORDS, IN FAVOR OF THE CITY OF CORONA.
- 14 AN EASEMENT FOR UTILITIES AND RIGHTS OF WAY AND INCIDENTAL PURPOSES, RECORDED JUNE 30, 1993 AS INSTRUMENT NO. 252481 OF OFFICIAL RECORDS, IN FAVOR OF CITY OF CORONA.
- 1 PROPOSED PUBLIC SEWER EASEMENT.
- 2 PROPOSED PUBLIC WATER EASEMENT.
- 3 PROPOSED INGRESS AND EGRESS EASEMENT.
- 4 PROPOSED DCCA EASEMENT.
- 5 PROPOSED EMERGENCY ACCESS EASEMENT.
- 6 PROPOSED PUBLIC ACCESS EASEMENT.

NOTES:

- 1 VACATED AND CLOSED TO PUBLIC USE BY RESOLUTION NO. 3818, RECORDED NOVEMBER 10, 1910 AS INSTRUMENT NO. 113224 OF OFFICIAL RECORDS OF RIVERSIDE COUNTY, CALIFORNIA.
- 2 PORTIONS OF PARCEL "A" VACATED BY RESOLUTION NO. 91-122, RECORDED JUNE 30, 1993 AS INSTRUMENT NO. 252481 O.R.
- 3 PORTIONS OF PARCEL "B" VACATED BY RESOLUTION NO. 91-122, RECORDED JUNE 30, 1993 AS INSTRUMENT NO. 252481 O.R.
- 4 VACATED PER INST. #1916-04734, RECORDED 06/24/1916 & INST. #1916-103437, RECORDED 07/16/1916.

	ENGINEERING CONSULTANTS 3788 MCGRAY STREET RIVERSIDE, CA 92506 PH. (951) 686-1070 FAX (951) 788-1256	CITY OF CORONA - DEVELOPMENT PLAN REVIEW 2023-0021 TITLE SHEET	NO. 22-0253 SHEET / OF 7 SHEETS DWS. NO.
	SCALE: 1" = 50' DATE: 7/1/25 DESIGNED: RH CHECKED: JRG PLN CK REF: F.B.	PLOT DATE: 7/1/25	NORTHGATE - 6TH & MAIN STREET

PRELIMINARY



LEGEND

[Pattern]	PROPOSED LANDSCAPE AREA
[Pattern]	PROPOSED CONCRETE
[Pattern]	TRUNCATED DOME
---(XX)---	EXISTING CONTOUR
---XX---	PROPOSED CONTOUR
---	PROPOSED/EXISTING RIGHT OF WAY
---	PROPERTY BOUNDARY
---	ADA PATH OF TRAVEL
---	PROPOSED SEWER
---	PROPOSED WATER
---	PROPOSED STORM DRAIN
---	PROPOSED RETAINING WALL
---	PROPOSED EASEMENT

ABBREVIATIONS

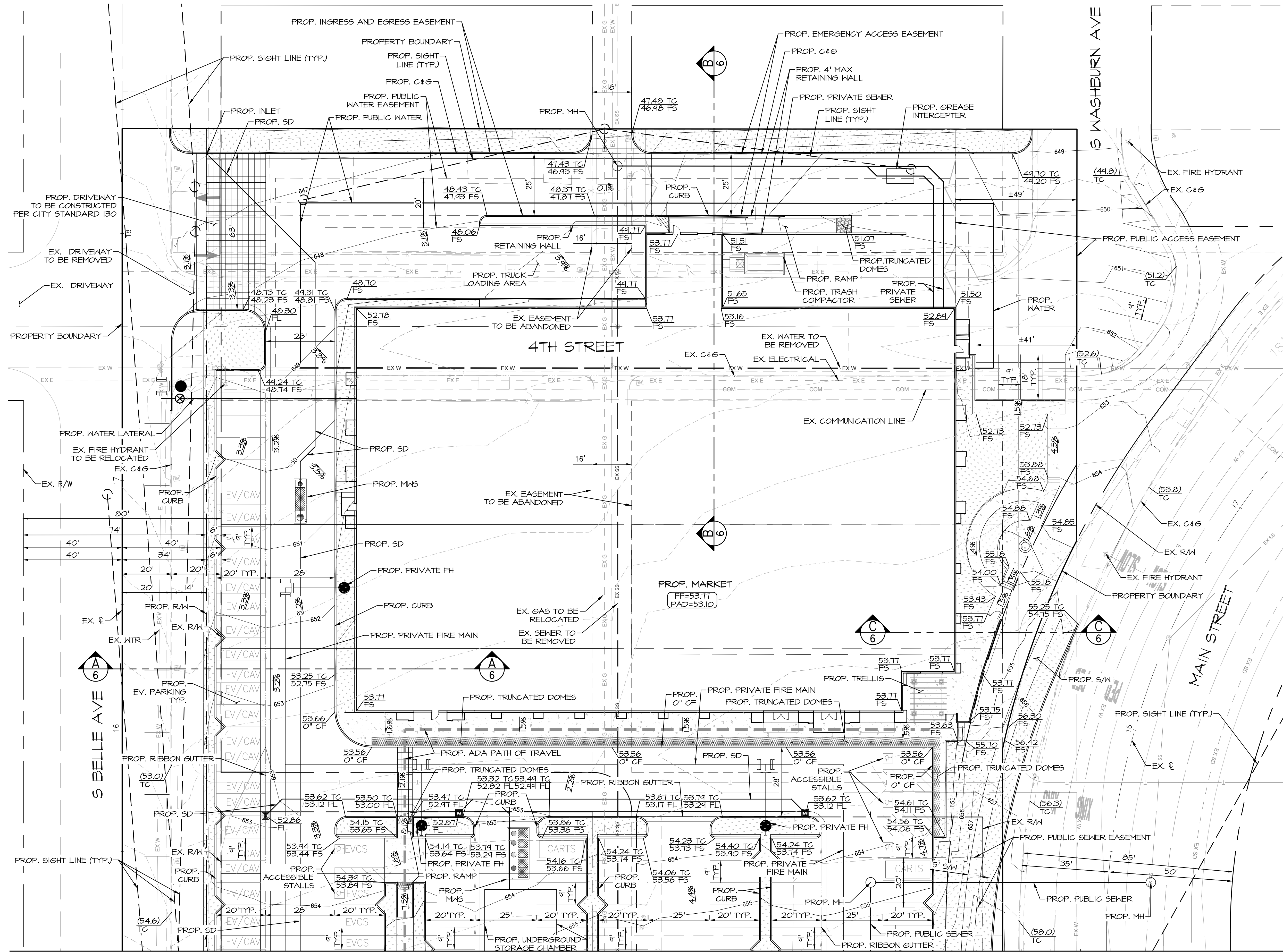
C&G	CURB AND GUTTER
EG	EXISTING GROUND
ESMT	EASEMENT
EX	EXISTING
FG	FINISH GRADE
FH	FIRE HYDRANT
FL	FLOW LINE
F5	FINISH SURFACE
GB	GRADE BREAK
HP	HIGH POINT
LA	LANDSCAPE AREA
LP	LOW POINT
M&S	MODULAR WETLAND SYSTEM
P/L	PARCEL LINE
PP	POWER POLE
PROP.	PROPOSED
R/W	RIGHT OF WAY
SCE	SOUTHERN CALIFORNIA EDISON
SD	STORM DRAIN
SHR	SEWER
S/W	SIDEWALK
TC	TOP OF CURB
APP.	TYPICAL
TPM	TENTATIVE PARCEL MAP
WTR	WATER

REVISIONS	DATE	BY

SCALE: 1" = 30'
 DATE: 1/1/25
 DESIGNED: RH
 CHECKED: JRG
 PLN CK REF: F.B.
ALBERTA A. WEBB ASSOCIATES
 ENGINEERING CONSULTANTS
 3788 MCGRAY STREET
 RIVERSIDE CA, 92506
 PH. (951) 686-1070
 FAX (951) 788-1256
 PLOT DATE: 1/1/25

CITY OF CORONA - DEVELOPMENT PLAN REVIEW 2023-0021
PROPOSED SITE PLAN
 NORTHGATE - 6TH & MAIN STREET
 SHEET 2 OF 7 SHEETS
 DWG. NO. 22-0253

PRELIMINARY
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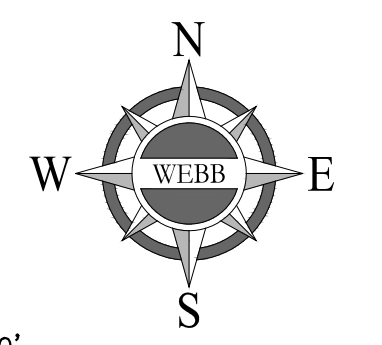


- LEGEND**
- PROPOSED LANDSCAPE AREA
 - PROPOSED CONCRETE
 - TRUNCATED DOME
 - EXISTING CONTOUR
 - PROPOSED CONTOUR
 - PROPOSED/EXISTING RIGHT OF WAY
 - PROPERTY BOUNDARY
 - ADA PATH OF TRAVEL
 - PROPOSED SEWER
 - PROPOSED WATER
 - PROPOSED STORM DRAIN
 - PROPOSED RETAINING WALL
 - PROPOSED EASEMENT

EASEMENT TABLE

- 1 PROPOSED PUBLIC SEWER EASEMENT.
- 2 PROPOSED PUBLIC WATER EASEMENT.
- 3 PROPOSED INGRESS AND EGRESS EASEMENT.
- 4 PROPOSED DCDA EASEMENT.
- 5 PROPOSED EMERGENCY ACCESS EASEMENT.
- 6 PROPOSED PUBLIC ACCESS EASEMENT.

SEE SHEET 4



REVISIONS	DATE	BY

SCALE: 1" = 20'
 DATE: 7/1/25
 DESIGNED: RH
 CHECKED: JRG
 PLN CK REF: F.B.
ALBERTA A. WEBB ASSOCIATES
 ENGINEERING CONSULTANTS
 3788 MCCRAY STREET
 RIVERSIDE CA, 92506
 PH. (951) 686-1070
 FAX (951) 788-1256
 PLOT DATE: 7/1/25

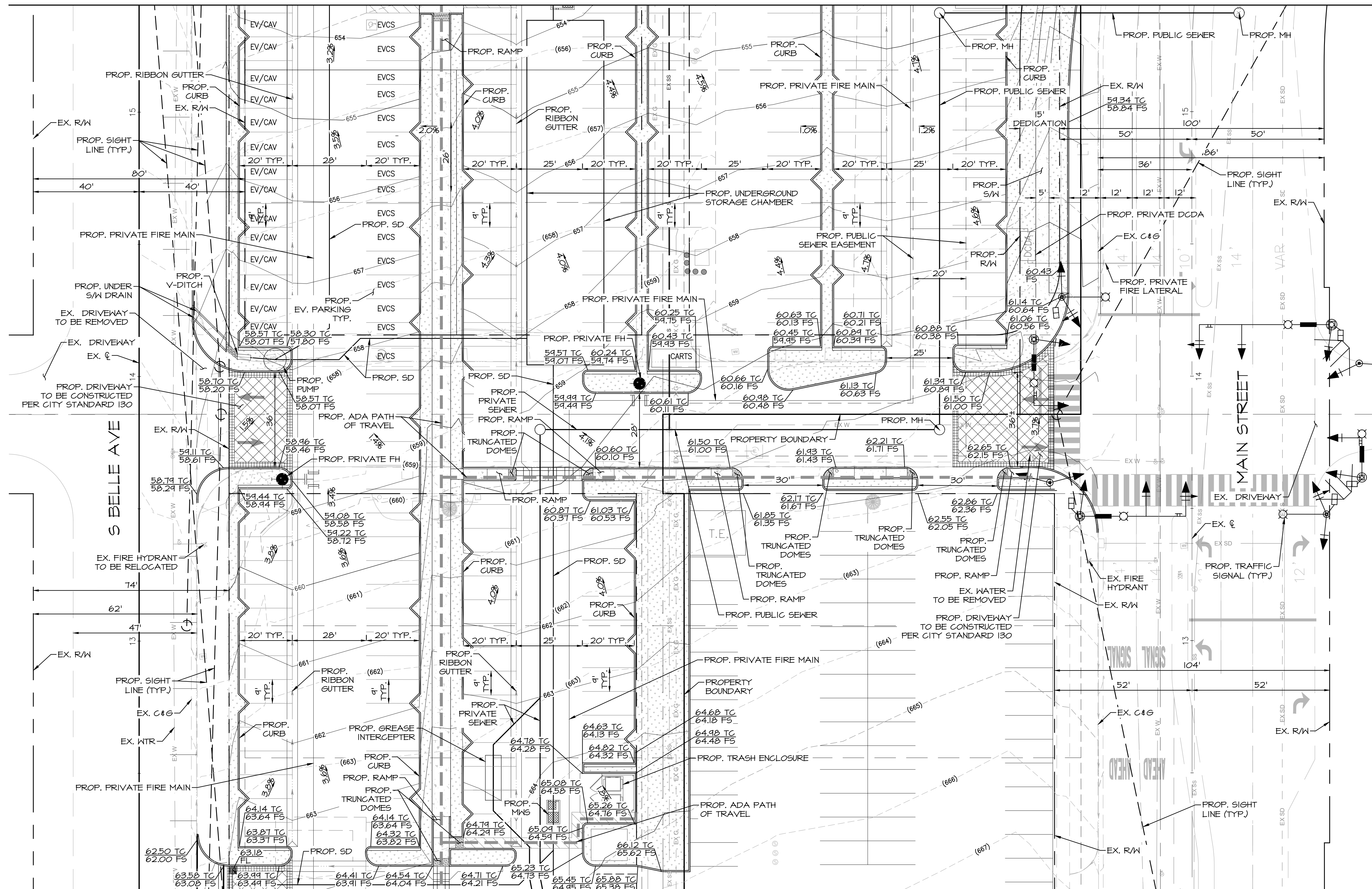
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 NORTHGATE - 6TH & MAIN STREET
 W.O. 22-0253
 SHEET 3
 OF 7 SHEETS
 DWS. NO.

PRELIMINARY

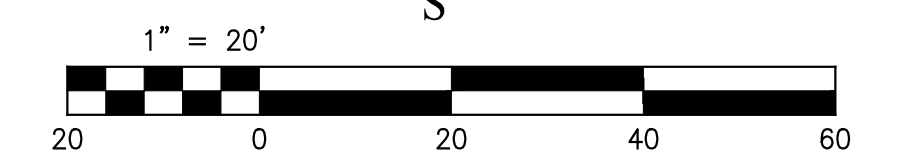
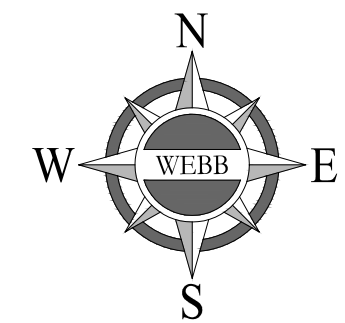
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SEE SHEET 3

SEE SHEET 5



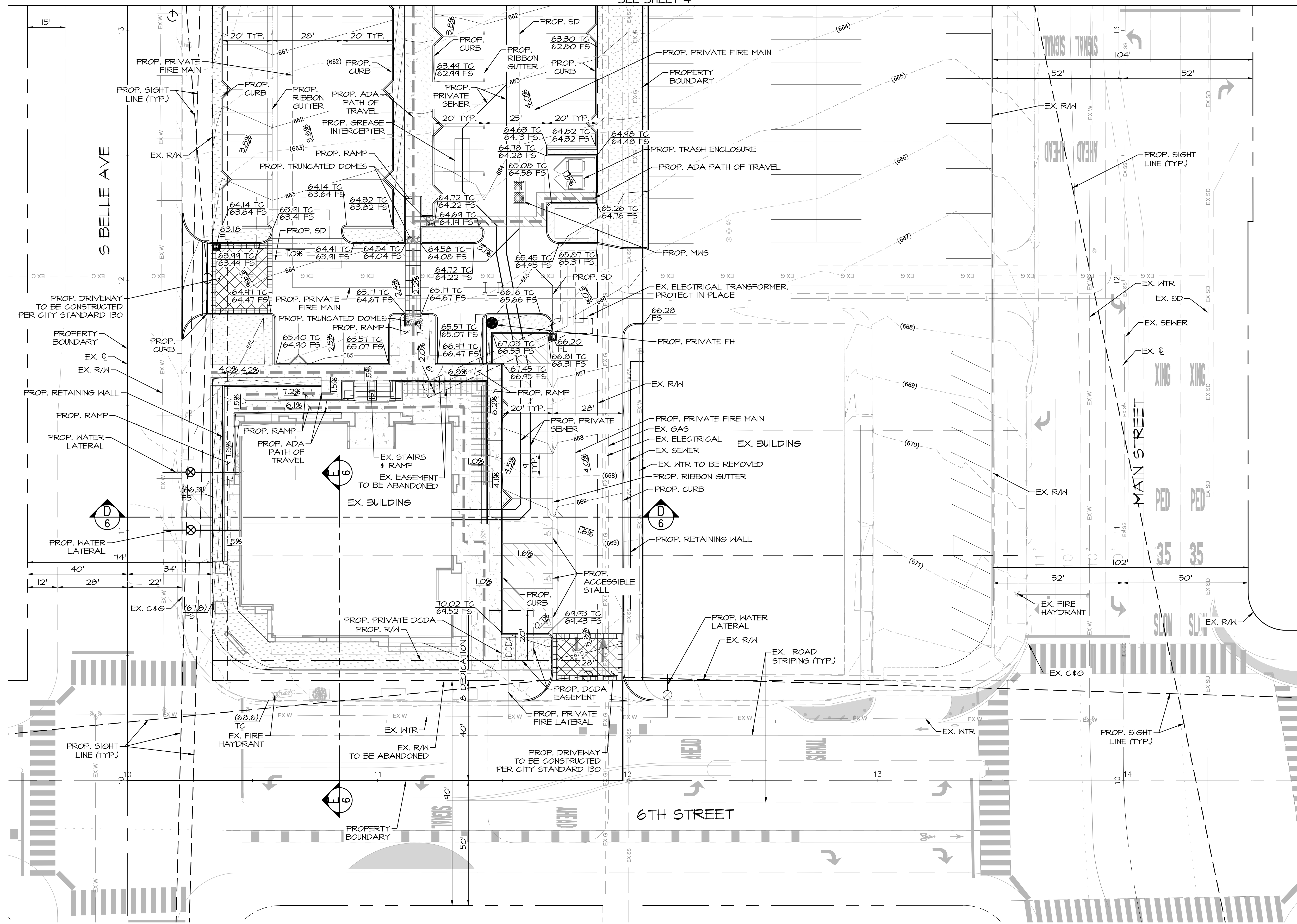
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 - PROPOSED CONCRETE
 - TRUNCATED DOME
 - EXISTING CONTOUR
 - PROPOSED CONTOUR
 - PROPOSED/EXISTING RIGHT OF WAY
 - PROPERTY BOUNDARY
 - ADA PATH OF TRAVEL
 - PROPOSED SEWER
 - PROPOSED WATER
 - PROPOSED STORM DRAIN
 - PROPOSED RETAINING WALL
 - PROPOSED EASEMENT



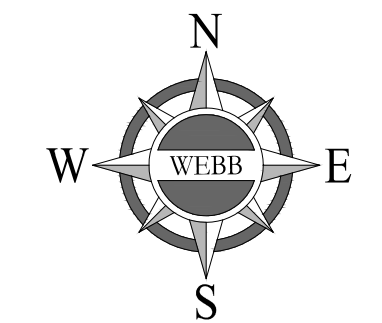
REVISIONS	SCALE: 1" = 20'	ALBERT A. WEBB ASSOCIATES ENGINEERING CONSULTANTS 3788 MCCRAY STREET RIVERSIDE CA, 92506 PH. (951) 686-1070 FAX (951) 788-1256	CITY OF CORONA - DEVELOPMENT PLAN REVIEW 2023-0021 CONCEPTUAL GRADING PLAN NORTHGATE - 6TH & MAIN STREET	NO. 22-0253
	DATE: 7/1/25			SHEET 4
	DESIGNED: RH	PLN CK REF: F.B.		OF 7 SHEETS
	CHECKED: JRG	DATE: 7/1/25		DWS. NO.

PRELIMINARY

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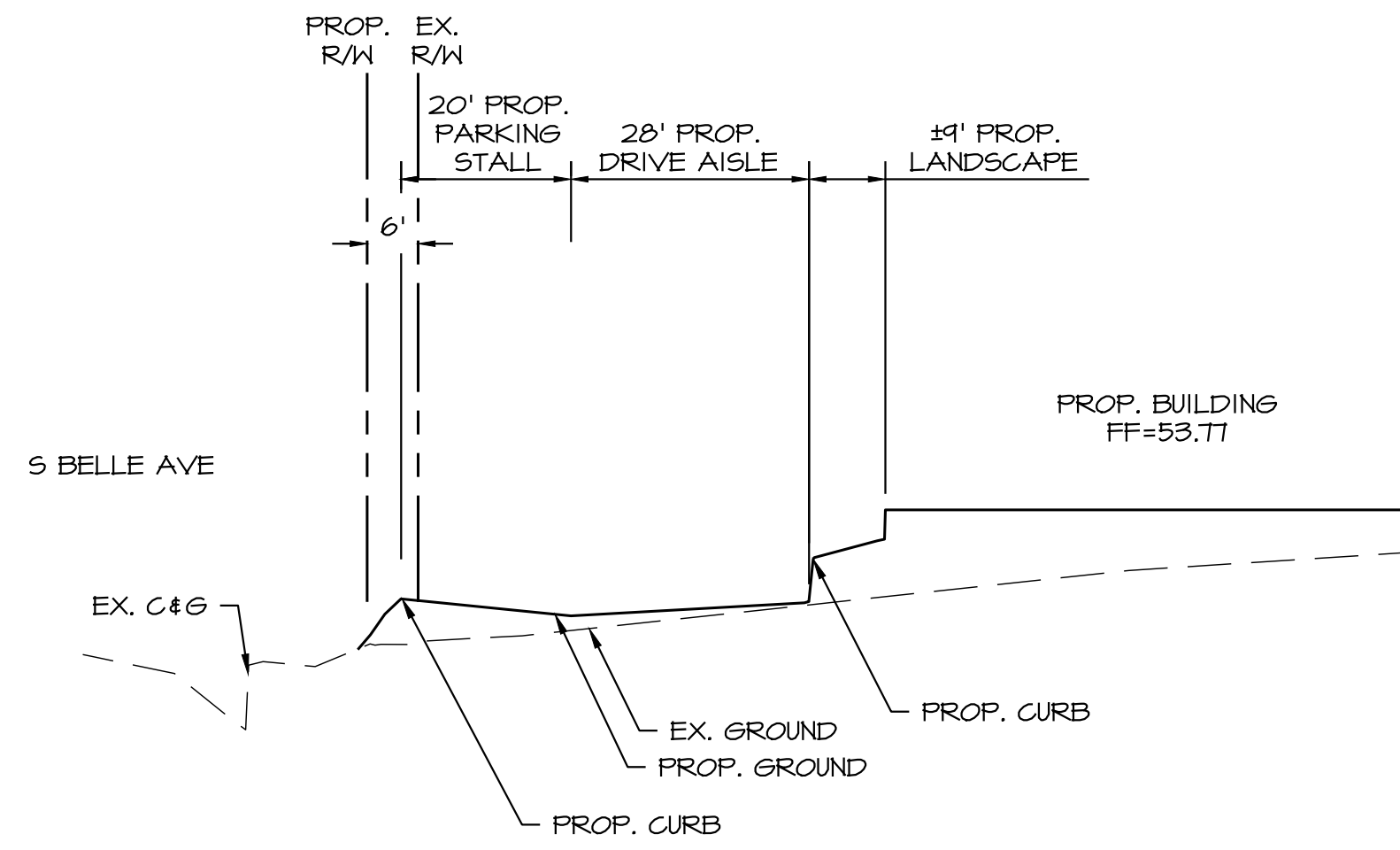
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- PROPOSED LANDSCAPE AREA
 - PROPOSED CONCRETE
 - TRUNCATED DOME
 - EXISTING CONTOUR
 - PROPOSED CONTOUR
 - PROPERTY/EXISTING RIGHT OF WAY
 - PROPERTY BOUNDARY
 - ADA PATH OF TRAVEL
 - PROPOSED SEWER
 - PROPOSED WATER
 - PROPOSED STORM DRAIN
 - PROPOSED RETAINING WALL



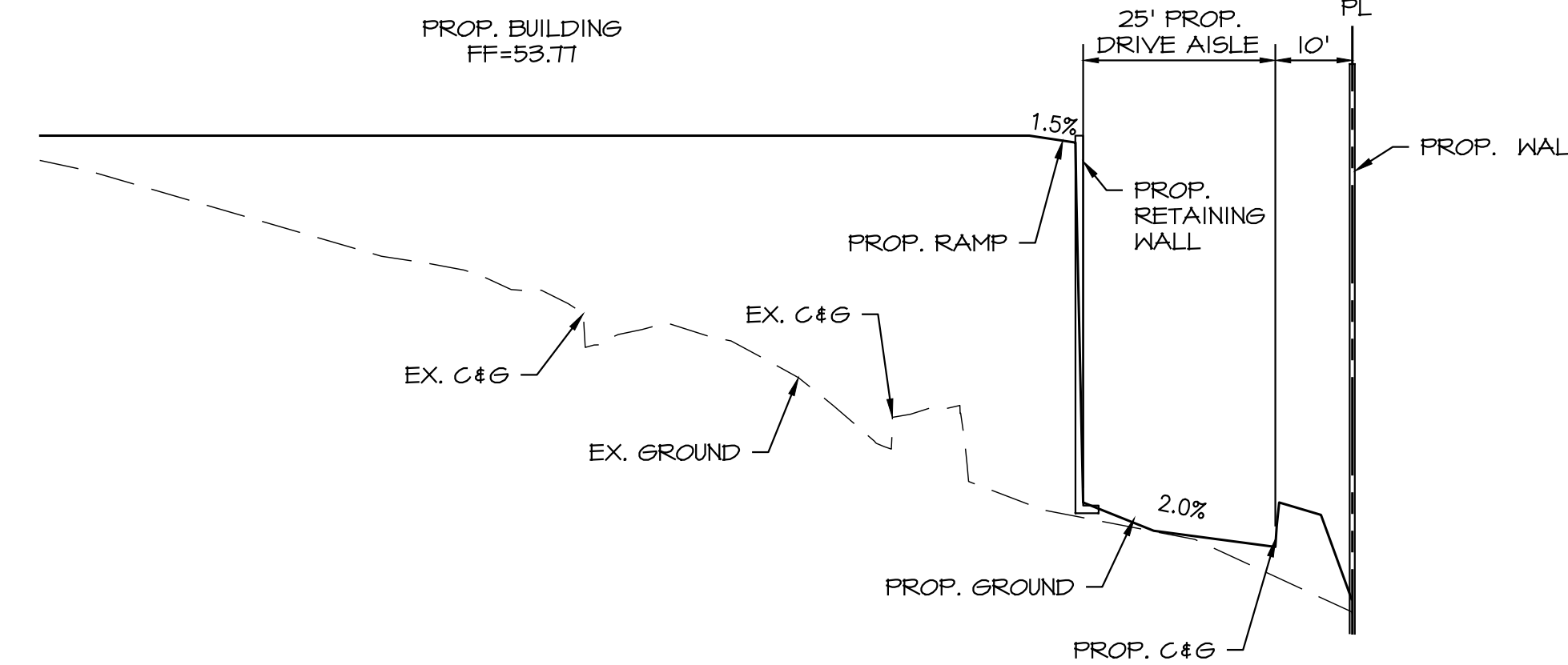
REVISIONS	SCALE: 1" = 20'	ALBERT A. WEBB ASSOCIATES ENGINEERING CONSULTANTS 3788 MCCRAY STREET RIVERSIDE CA, 92506 PH. (951) 686-1070 FAX (951) 788-1256	CITY OF CORONA - DEVELOPMENT PLAN REVIEW 2023-0021 CONCEPTUAL GRADING PLAN NORTHGATE - 6TH & MAIN STREET	NO. 22-0253
	DATE: 1/1/25			SHEET 5
	DESIGNED: RH	PLN CK REF: F.B.	PLOT DATE: 1/1/25	OF 7 SHEETS
	CHECKED: JRJ			DWG. NO.

PRELIMINARY

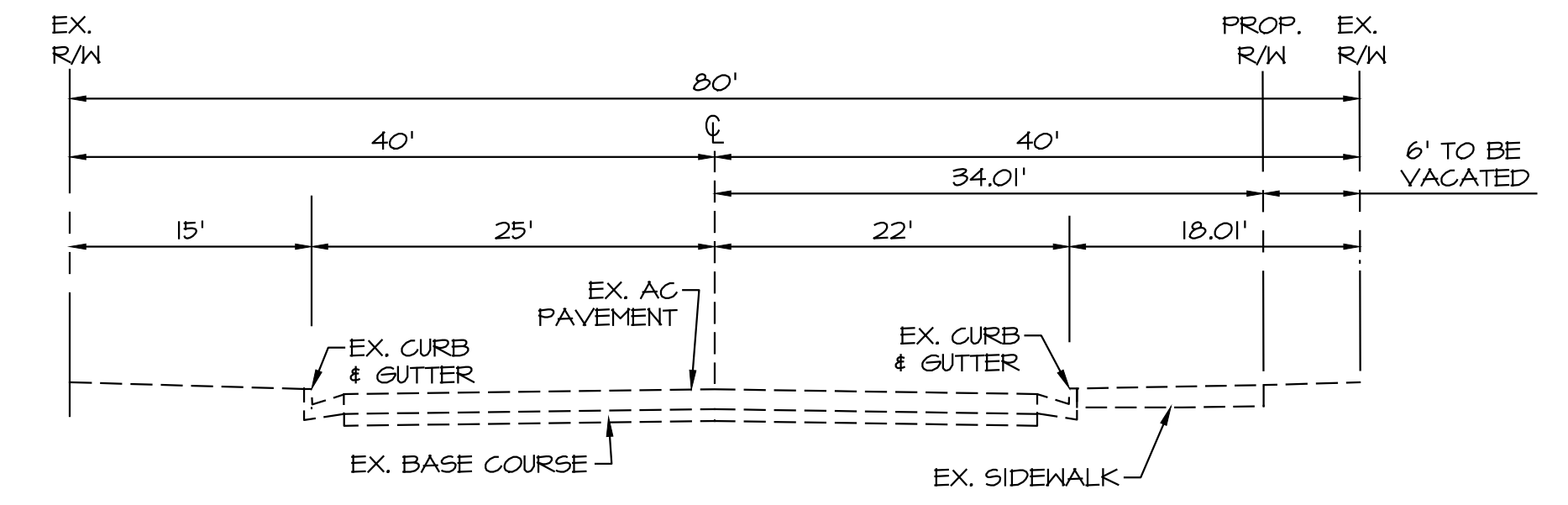
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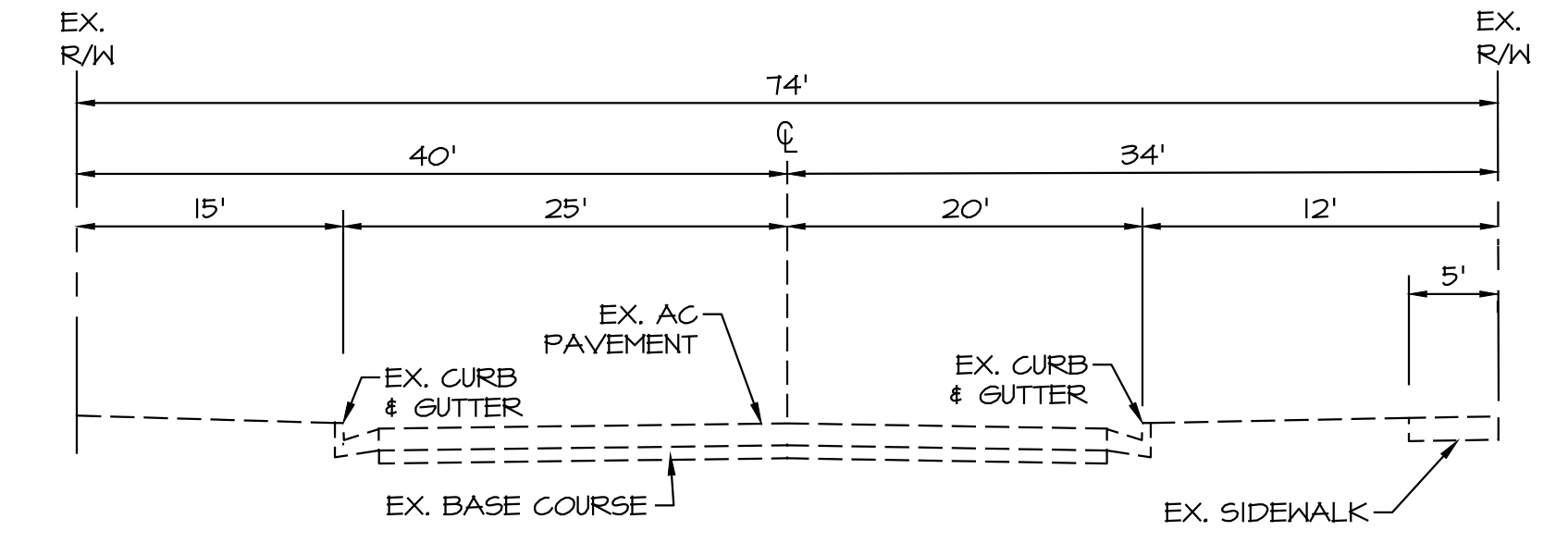
SECTION A-A
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VERTICAL EXAGGERATION: 10



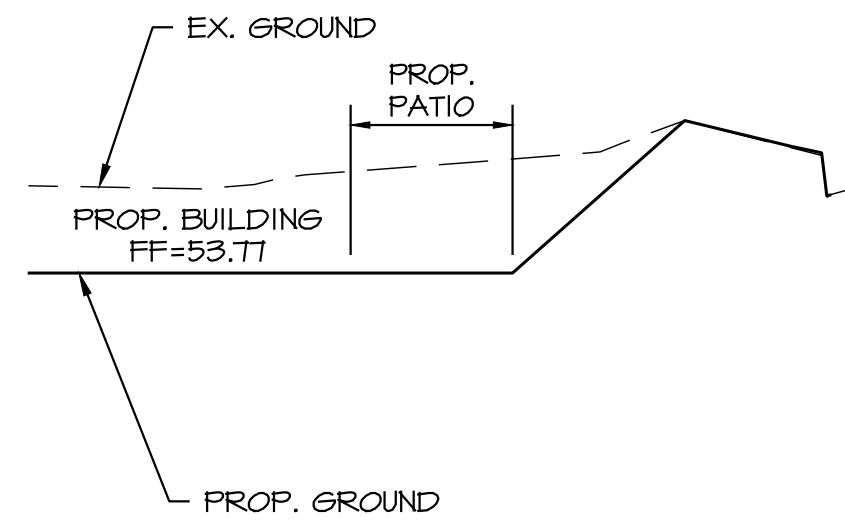
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VERTICAL EXAGGERATION: 10



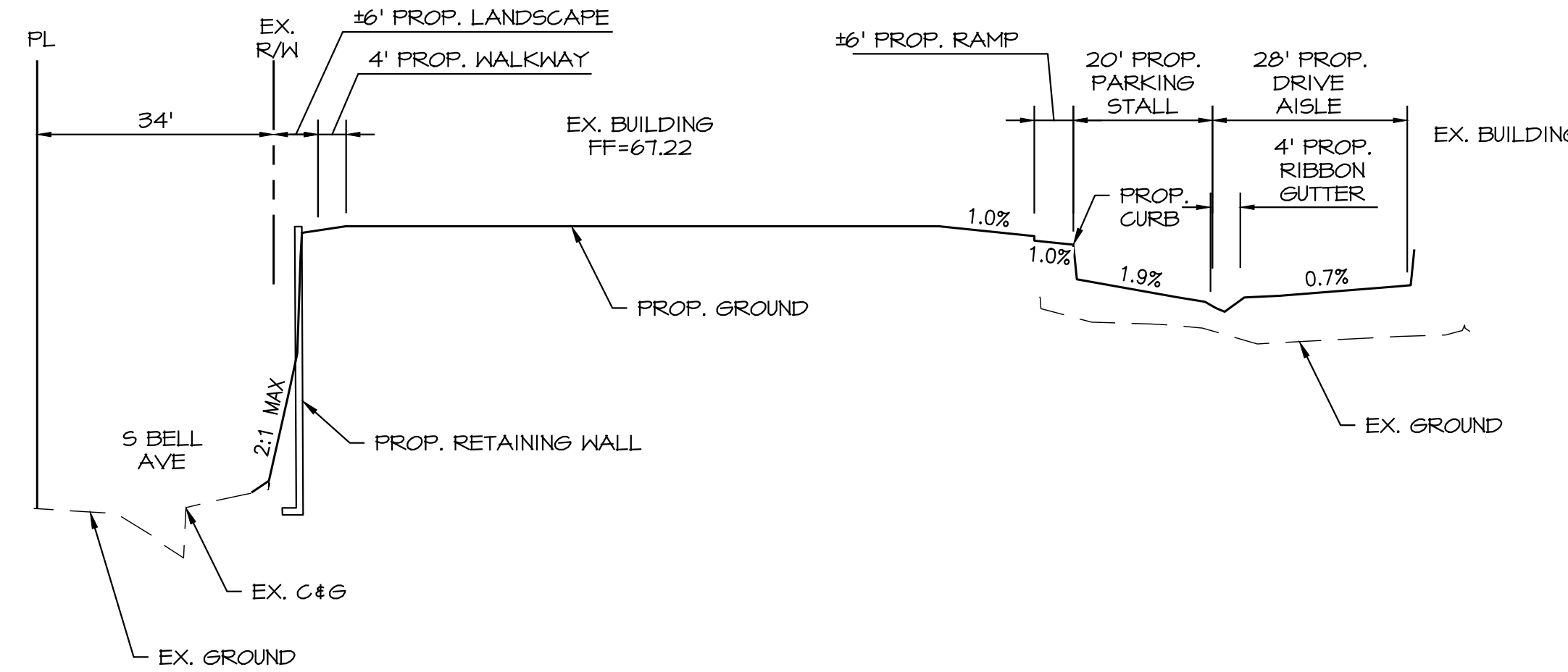
S BELLE AVE
NORTH OF 5TH STREET
NTS



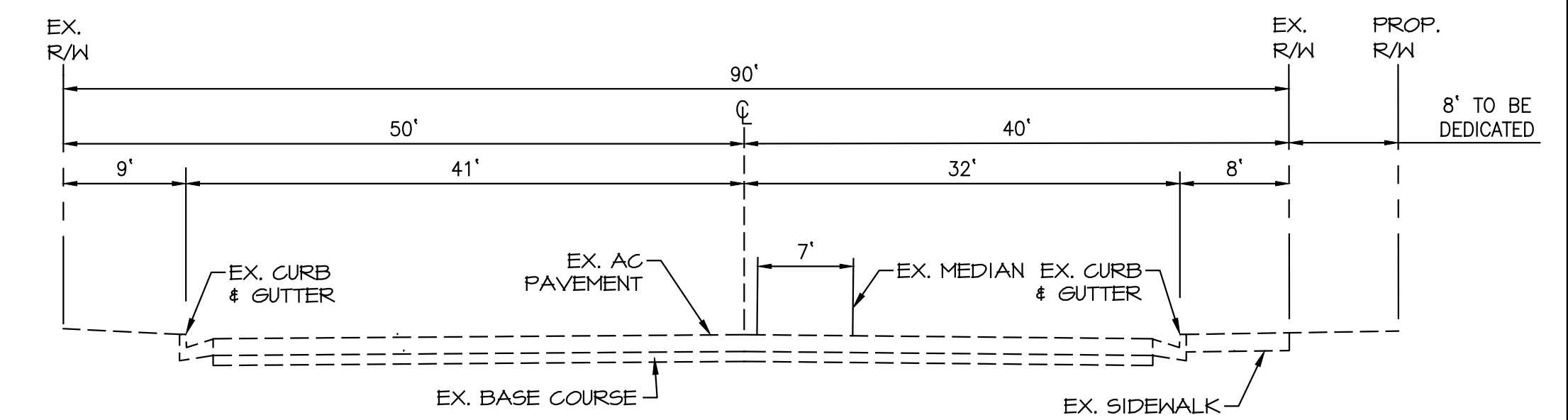
S BELLE AVE
SOUTH OF 5TH STREET
NTS



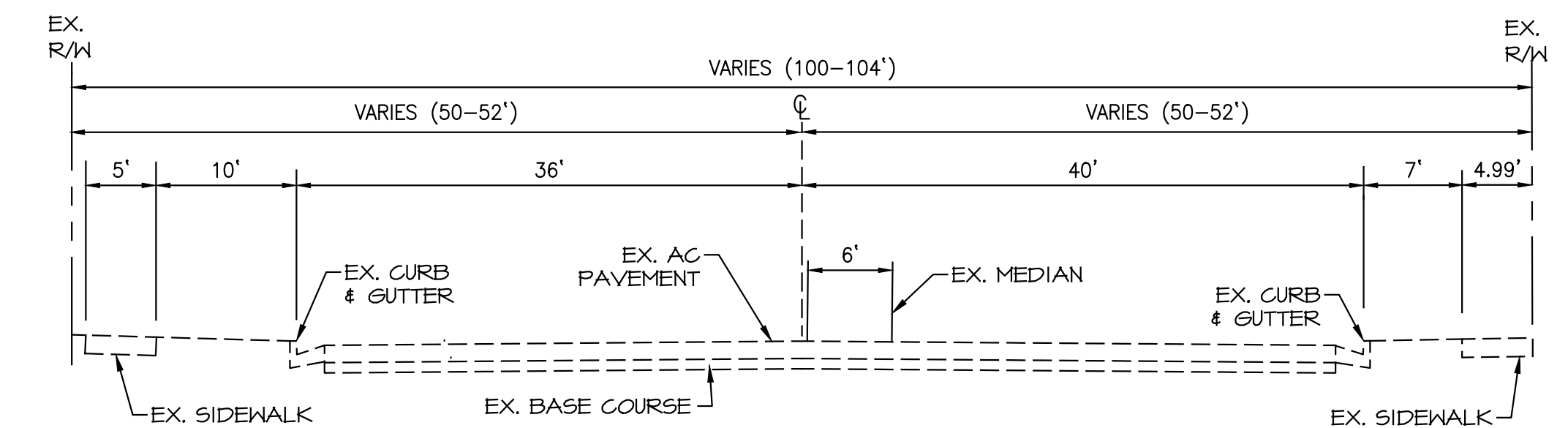
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VERTICAL EXAGGERATION: 10



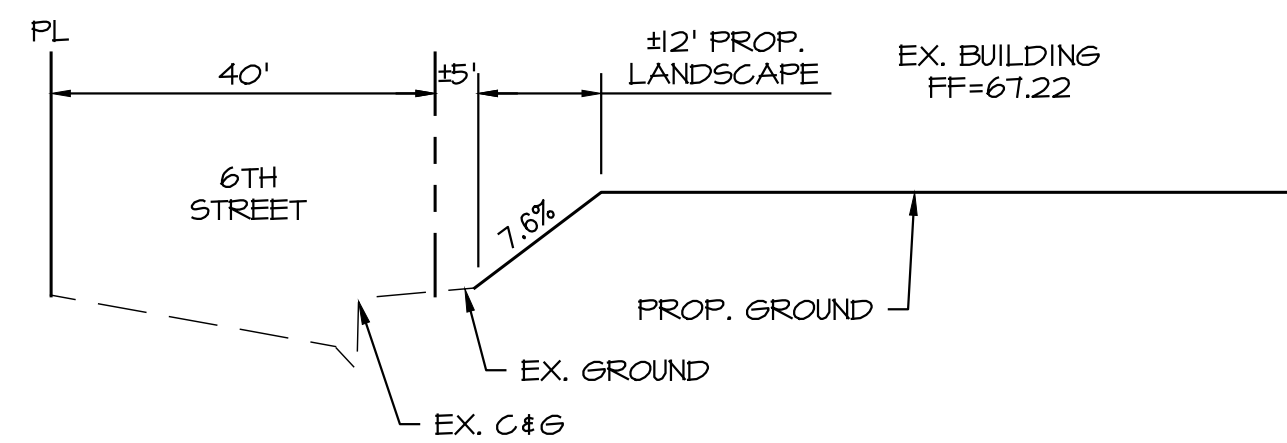
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SCALE: 1" = 20'
VERTICAL EXAGGERATION: 10



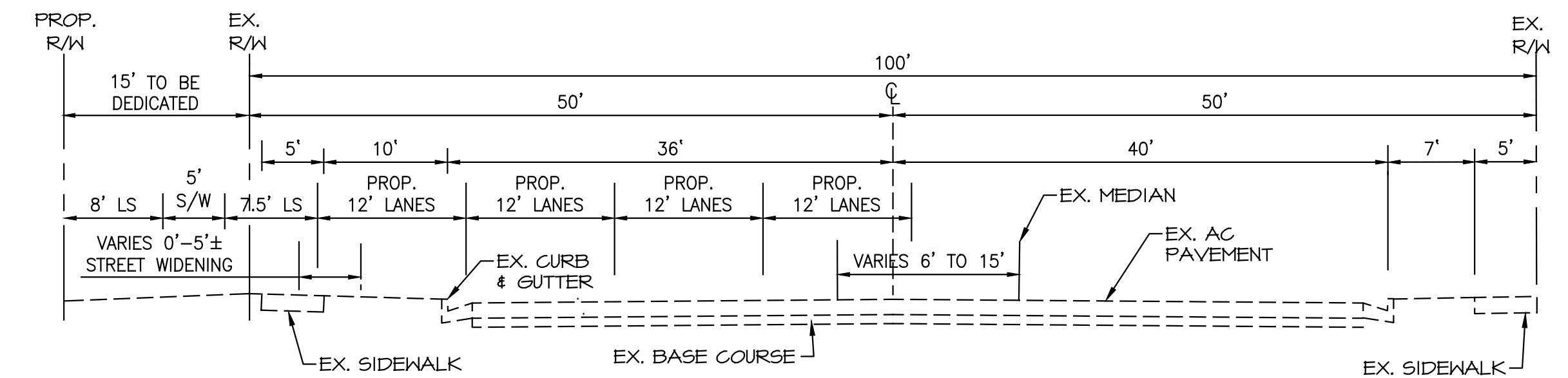
6TH STREET
NTS



MAIN STREET
NTS



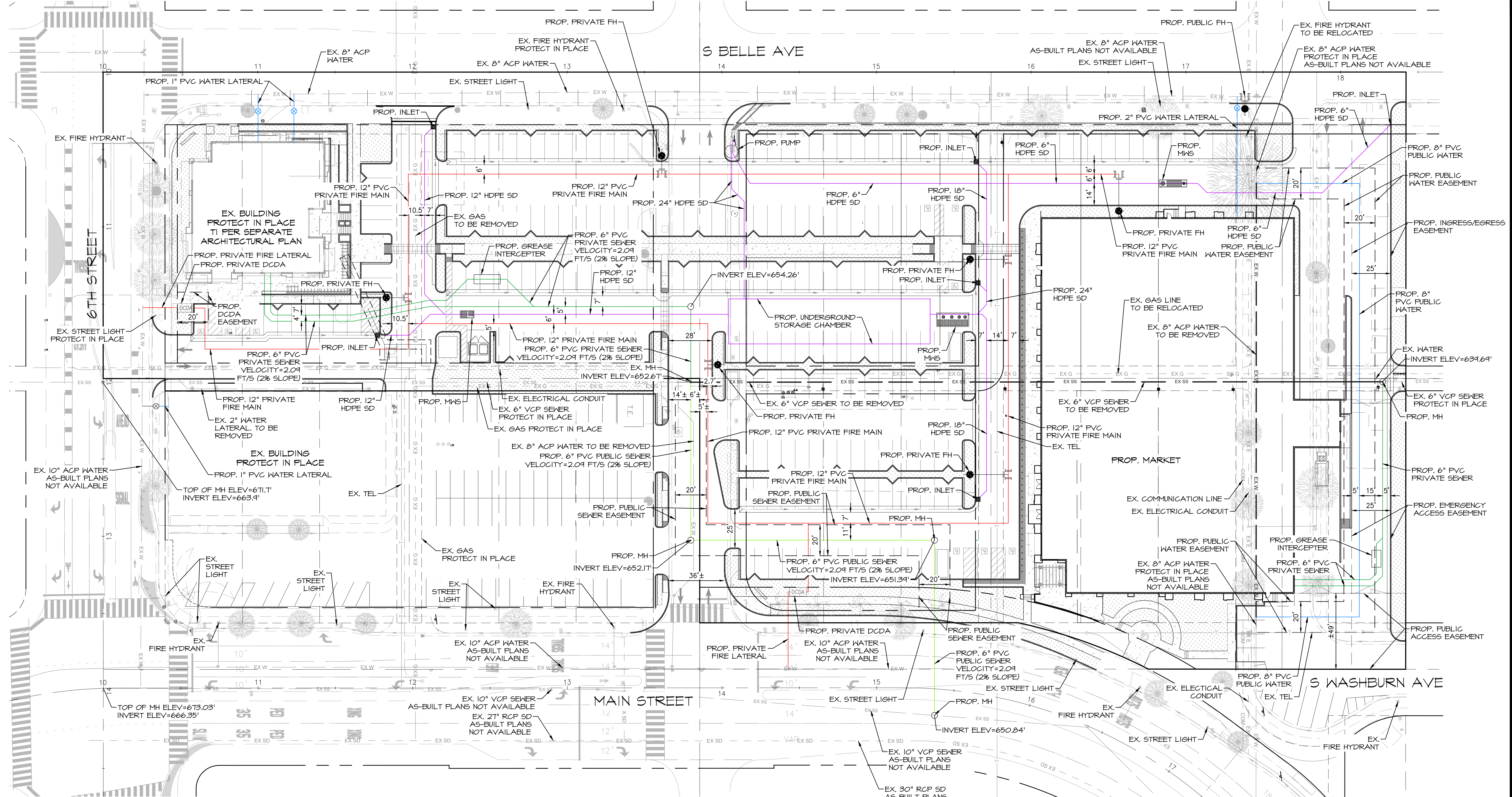
SECTION E-E
SCALE: 1" = 20'
VERTICAL EXAGGERATION: 10



MAIN STREET
NORTH OF 5TH STREET
R/W VACATION ALONG FRONTAGE
NTS

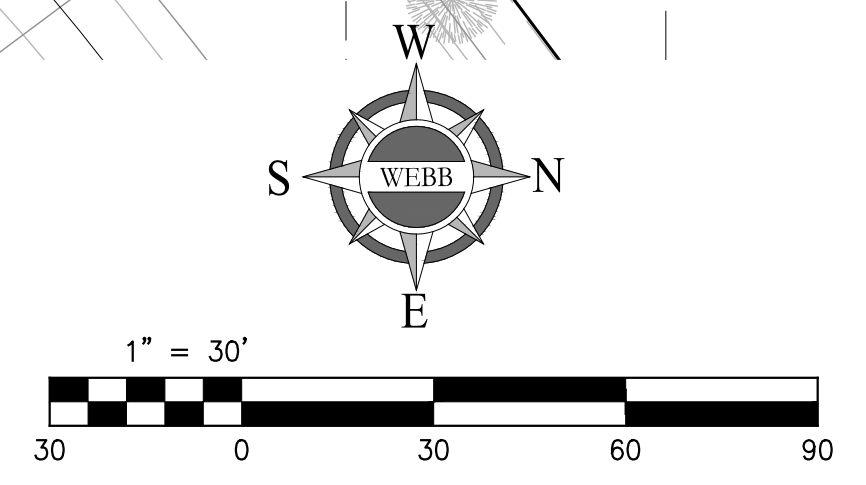
<p>REVISIONS</p>		<p>SCALE: 1" = 20'</p> <p>DATE: 7/1/25</p> <p>DESIGNED: RH</p> <p>CHECKED: JRG</p> <p>PLN CK REF: F.B.</p>	<p>ALBERT A. WEBB ASSOCIATES</p> <p>ENGINEERING CONSULTANTS 3788 MCCRAY STREET RIVERSIDE CA, 92506 PH. (951) 686-1070 FAX (951) 788-1256</p>	<p>CITY OF CORONA - DEVELOPMENT PLAN REVIEW 2023-0021</p> <p>CROSS SECTIONS</p> <p>NORTHGATE - 6TH & MAIN STREET</p>	<p>NO. 22-0253</p> <p>SHEET 6</p> <p>OF 7 SHEETS</p> <p>DWG. NO.</p>
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PRELIMINARY



LEGEND

	PROPOSED LANDSCAPE AREA
	PROPOSED CONCRETE
	TRUNCATED DOME
	EXISTING CONTOUR
	PROPOSED CONTOUR
	PROPOSED/EXISTING RIGHT OF WAY
	PROPERTY BOUNDARY
	ADA PATH OF TRAVEL
	PROPOSED SEWER
	PROPOSED WATER
	PROPOSED STORM DRAIN
	PROPOSED RETAINING WALL



REVISIONS	DATE	BY	SCALE: 1" = 30'	ALBERT A. WEBB ASSOCIATES ENGINEERING CONSULTANTS 3788 MCCRAY STREET RIVERSIDE CA, 92506 PH. (951) 686-1070 FAX (951) 788-1256	CITY OF CORONA - DEVELOPMENT PLAN REVIEW 2023-0021 CONCEPTUAL WET UTILITY PLAN NORTHGATE - 6TH & MAIN STREET	NO. 22-0253
			DATE: 7/1/25 DESIGNED: RH CHECKED: JRG PLN CK REF: F.B. PLOT DATE: 7/1/25			SHEET 7 OF 7 SHEETS DWS. NO.

PRELIMINARY



NORTHGATE MARKET

COMPREHENSIVE SIGN PROGRAM

WEST 6TH STREET &
SOUTH MAIN STREET
CORONA, CA

**Promotional
Signs**

3301 S. Susan St.
Santa Ana, CA 92704
(714) 540-5454 PHONE (714) 540-5959 FAX

EXHIBIT 14

INTRODUCTION

The purpose of this program is to establish sign design standards and general guidelines that assure consistency in quality and necessary to balance maximum identification within an overall harmony of design for the project.

Conformance to this sign program criteria will be strictly enforced. Any installed non-conforming or unapproved signs shall be brought into conformance at the expense of the tenant and/or sign contractor.

GENERAL REQUIREMENTS

- a. All work is to be performed by a professional sign company. Said sign company must be in possession of a current state contractor’s license and a City business license to perform such work. Said company must maintain a minimum of \$1,000,000 insurance to be working on property.
- b. Each tenant shall submit or cause to be submitted to the landlord and the City of Corona for approval before fabrication four (4) copies of detailed drawings, to scale, indicating the location, size, layout, design and color of the proposed signs, including all lettering and or graphics. These drawings should be submitted along with a site plan indicating the location of the lease space on the site, and elevation showing sign placement and lease space width. All sign layouts shall include a digital image of the building frontage with proposed sign in a scaled format to include adjacent tenant signage, if applicable.
- c. All signs shall be reviewed and approved in writing by the landlord for conformance with this sign program and overall design quality as well as tenant’s lease agreement.
- d. All signs must comply with current City of Corona planning, zoning, building and electrical codes.
- e. City Approval and Permits: Upon approval by the landlord, tenant shall secure a sign permit from the City of Santa Ana by bringing four (4) color copies of the proposed drawings (as approved by the landlord) to the Planning Division. All permits required by the City of Corona for signs and their installation must be obtained and paid for by the tenant prior to installation.
- f. All building signs shall be constructed and installed at tenant’s expense. Tenant is also responsible for maintenance of building signs.
- g. In the event a tenant vacates the leased premises, tenant shall be responsible for the removal of any wall and/or logo sign, with all holes being repaired and repainted to match the building exterior.
- h. Tenant’s sign contractor shall repair any damage to any work caused during installation of sign. Incomplete repairs are the ultimate responsibility of the tenant.

APPROVAL PROCEDURE

All signs installed or displayed on the premises of 6th & Main St, must have written approval of the Landlord and the City of Corona. The aesthetic characteristics of the signs (i.e., placement, size, proportion, color, textures, method of fabrication, etc.) are subject to the discretionary approval of the landlord and the City of Corona within the context of this sign program.

Prior to submission to the City of Corona for permits, and prior to construction and installation of any sign covered by this sign program, the tenant must submit four (4) sets of professionally prepared drawings of their proposed signage meeting this sign program for review and approval by the landlord and the Planning Division.

- i. Upon notice by the City of Corona or landlord, tenant shall be required to repair or refurbish their sign structure, sign face and/or sign illumination within seven (7) working days.
- j. Tenant is responsible for maintenance and cost of electricity for their own signage. The tenant or landlord shall be responsible to provide a designated electrical circuit from electrical panel for their signage. Tenant shall provide primary wiring from electrical sub-panel to J-box at sign location.
- k. Tenant shall be fully responsible for the operations of its contractor and shall indemnify, defend and hold harmless the landlord and his/her agents from damages or liabilities resulting from its contractor’s work.
- l. Registered trademarks, herein referred to as Registered Corporate Identity, pertains to corporate logos and logotypes that have been registered and are on file with the UNITED STATES PATENT AND TRADEMARK OFFICE in Washington, D.C.
- m. Signs not covered by this sign program are not allowed.
- n. All signs must have the sign manufacturers name, address, and telephone number conspicuously and permanently attached on the rear exterior of the sign.
- o. Leasing signs shall be restricted to the window of the available suite. These signs shall follow the same criteria as window signs.
- p. For businesses without a registered corporate logo/typeface, the Helvetica Neue is permitted. Approved color(s) are black or white depending on the color of the fascia upon which the sign is placed. Internal white LED face illumination.



3301 S. Susan St.
Santa Ana, CA 92704
714-540-5454 Ph
714-540-5959 Fax

Client: _____



NORTHGATE MARKET

Address: _____

6TH ST. & MAIN ST.

City: _____

CORONA, CA.

Drawn By: _____ Salesperson: _____

JM DC

Drawing Number: _____ Option: _____

SP-NORTHGATE-CORONA

Drawing Date: _____

07/06/23

Revision: _____

- 1. 07/19/23
- 2. 08/10/23
- 3. 02/16/23
- 4. 07/04/24

CUSTOMER / LANDLORD APPROVAL

SIGNATURE _____

DATE _____

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Contractors License No. 636512

Page: _____ Of: _____

PROHIBITED SIGNS

- a. No sign is permitted that:
 - i. Is dangerous or confusing to motorists on the public right-of-way, including any sign which by its color, wording design, location or illumination resembles or conflicts with any official traffic-control device or which impedes the safe and efficient flow of traffic.
 - ii. Is in a condition which presents a danger of injury to the public.
 - iii. Incorporates mechanical movement or in any way gives the illusion of motion, moving parts, rotation or any flashing, moving or intermittent lighting, other than a sign providing a time-and temperature or similar public service display.
 - iv. Impedes free ingress and egress from any door, window or exit way required by building or fire regulations.
 - v. Emits sound, smoke, visible particles or odors, except that of speakers on drive-through facilities shall be permitted
 - vi. Is attached to or maintained upon any public utility pole or structure, or tree.
 - vii. Is an A-frame, sandwich board or other portable, temporary advertising display.
 - viii. Is temporary or special event flags, banners, festoons, flag canopies or other displays, except as permitted by a special event sign permit by the Planning Division.
 - ix. Is a sculptured, molded or otherwise fabricated representational object used for the purpose of visually conveying business identification or product advertising.
 - x. Is an aerial or balloon type of sign.
 - xi. Duplicates or repeats copy on the same sign.
 - xii. Is a graphic of paint or other material on a building for the purpose of amplifying or directing attention to a sign, unless approved by the planning director.

OTHER RESTRICTIONS

a. **Illegal Signs**
Any sign that is deemed not to be in conformance with this Sign Program or erected without approval by the City of Corona is considered an illegal sign. The landlord, at its sole discretion and at occupant's expense may correct, replace or remove any illegal sign.

b. **Abandonment of Signs**
A sign is considered abandoned when its use is discontinued because the premises upon which it is located becomes vacated and unoccupied for a period of more than 90 days.

WINDOW SIGNAGE

No permanent sign affixed to or incorporated into an exterior window shall exceed twenty-five (25) percent of each window area.



3301 S. Susan St.
Santa Ana, CA 92704
714-540-5454 Ph
714-540-5959 Fax

Client: _____



Address: _____

6TH ST. & MAIN ST.

City: _____

CORONA, CA.

Drawn By: _____ Salesperson: _____

JM **DC**

Drawing Number: _____ Option: _____

SP-NORTHGATE-CORONA

Drawing Date: _____

07/06/23

Revision: _____

- 1. 07/19/23
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- 4. 07/04/24

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3 **9**

LEGEND

- A** MAJOR TENANT PRIMARY
- B** MAJOR TENANT SECONDARY
- C** MAJOR TENANT SUB-LEASE
- D** PAD TENANT ID
- E** MULTI-TENANT FREESTANDING



3301 S. Susan St.
 Santa Ana, CA 92704
 714-540-5454 Ph
 714-540-5959 Fax

Client: _____



NORTHGATE MARKET

Address: _____

6TH ST. & MAIN ST.

City: _____

CORONA, CA.

Drawn By: Salesperson: _____

JM **DC**

Drawing Number: Option: _____

SP-NORTHGATE-CORONA

Drawing Date: _____

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Revision: _____

1. 07/19/23
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3. 02/16/23
4. 07/04/24

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SIGNATURE _____

DATE _____

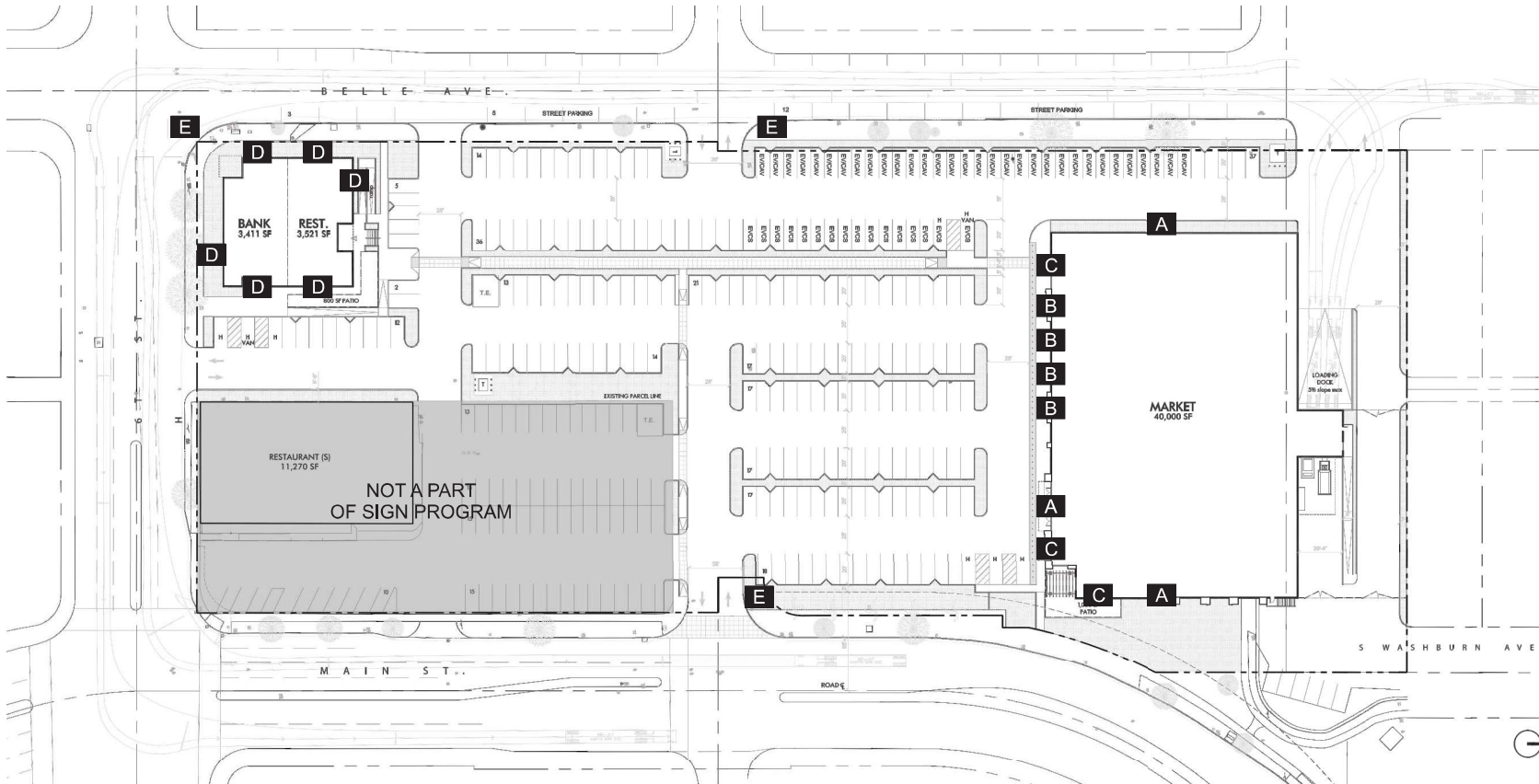
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4

9



Client:



NORTHGATE MARKET

Address:

6TH ST. & MAIN ST.

City:

CORONA, CA.

Drawn By: Salesperson:

JM **DC**

Drawing Number: Option:

SP-NORTHGATE-CORONA

Drawing Date:

07/06/23

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1. 07/19/23
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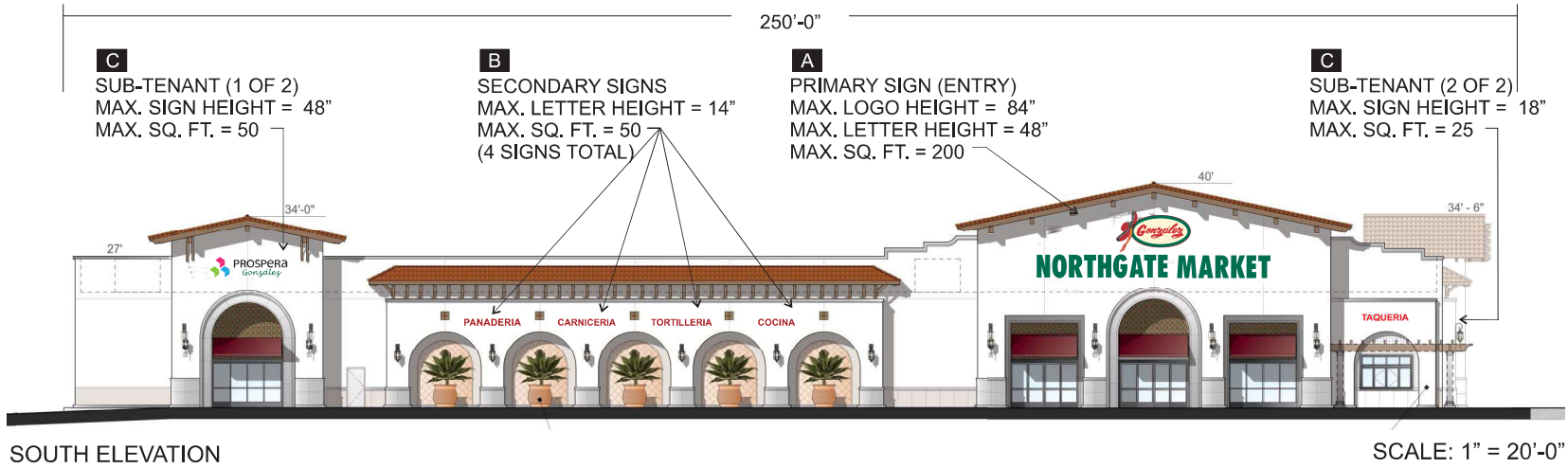
Page: _____ Of: _____

Major Tenant Wall Signs - Sign Types **A** **B** & **C**

Quantity: (3) primary signs and (4) four secondary & (2) two sub-tenant signs.

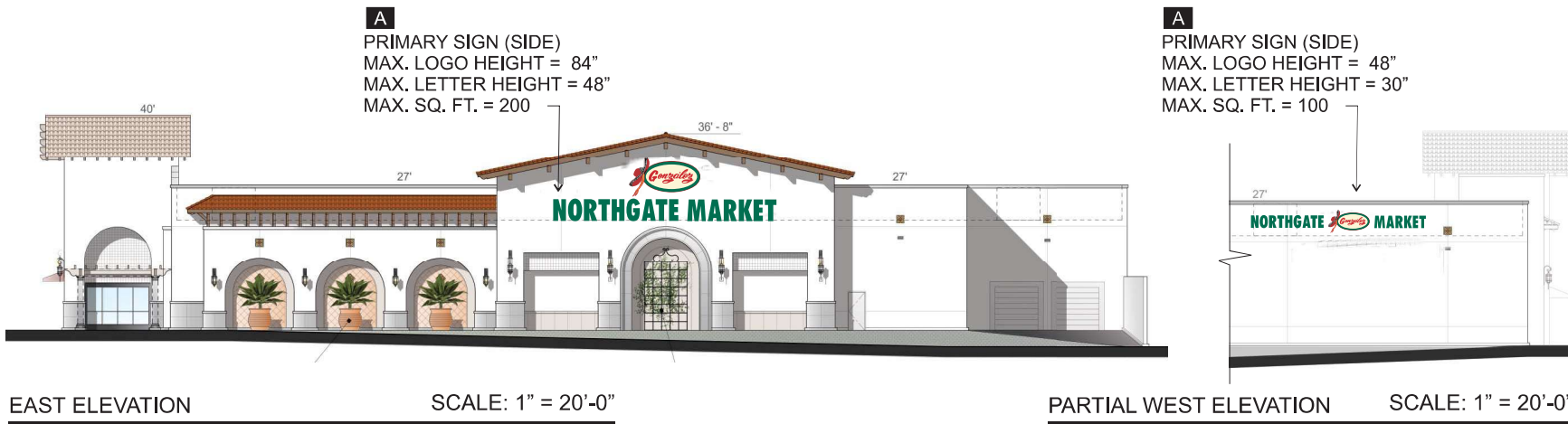
Area: 2 square foot per linear foot of lease frontage per street or parking lot facing elevation .

Sign Type: Internally illuminated channel letters/logos



SOUTH ELEVATION

SCALE: 1" = 20'-0"



EAST ELEVATION

SCALE: 1" = 20'-0"

PARTIAL WEST ELEVATION

SCALE: 1" = 20'-0"

Pad Tenant Wall Signs - Sign Type **D**

Quantity: (3) signs per tenant leasehold.
Area: 1.5 square feet per linear foot of each street facing or parking lot facing elevation.
Max Height: Logo and Letters not to exceed 48".
Sign Type: Internally illuminated channel letters/logos



NORTH ELEVATION

SCALE: 1/16" = 1'-0"



SOUTH ELEVATION

SCALE: 1/16" = 1'-0"

Client:



NORTHGATE MARKET

Address:

6TH ST. & MAIN ST.

City:

CORONA, CA.

Drawn By: Salesperson:

JM DC

Drawing Number: Option:

SP-NORTHGATE-CORONA

Drawing Date:

07/06/23

Revision:

1. 07/19/23
2. 08/10/23
3. 02/16/23
4. 07/04/24

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Pad Tenant Wall Signs - Sign Type **D** Continued

Quantity: (3) signs per tenant leasehold.

Area: 1.5 square feet per linear foot of each street facing or parking lot facing elevation.

Max Height: Logo and Letters not to exceed 48".

Sign Type: Internally illuminated channel letters/logos



Client:



Address:

6TH ST. & MAIN ST.

City:

CORONA, CA.

Drawn By: Salesperson:

JM DC

Drawing Number: Option:

SP-NORTHGATE-CORONA

Drawing Date:

07/06/23

Revision:

1. 07/19/23
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Freestanding Sign **E** Northgate Market

Quantity: (3) three double-sided signs at a minimum of 100'-0" apart.

Area: 45 square feet per face

Height: Not to exceed 7'-0"

Sign Type: Internally illuminated freestanding

Client:



NORTHGATE MARKET

Address:

6TH ST. & MAIN ST.

City:

CORONA, CA.

Drawn By: Salesperson:

JM DC

Drawing Number: Option:

SP-NORTHGATE-CORONA

Drawing Date:

07/06/23

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DATE _____

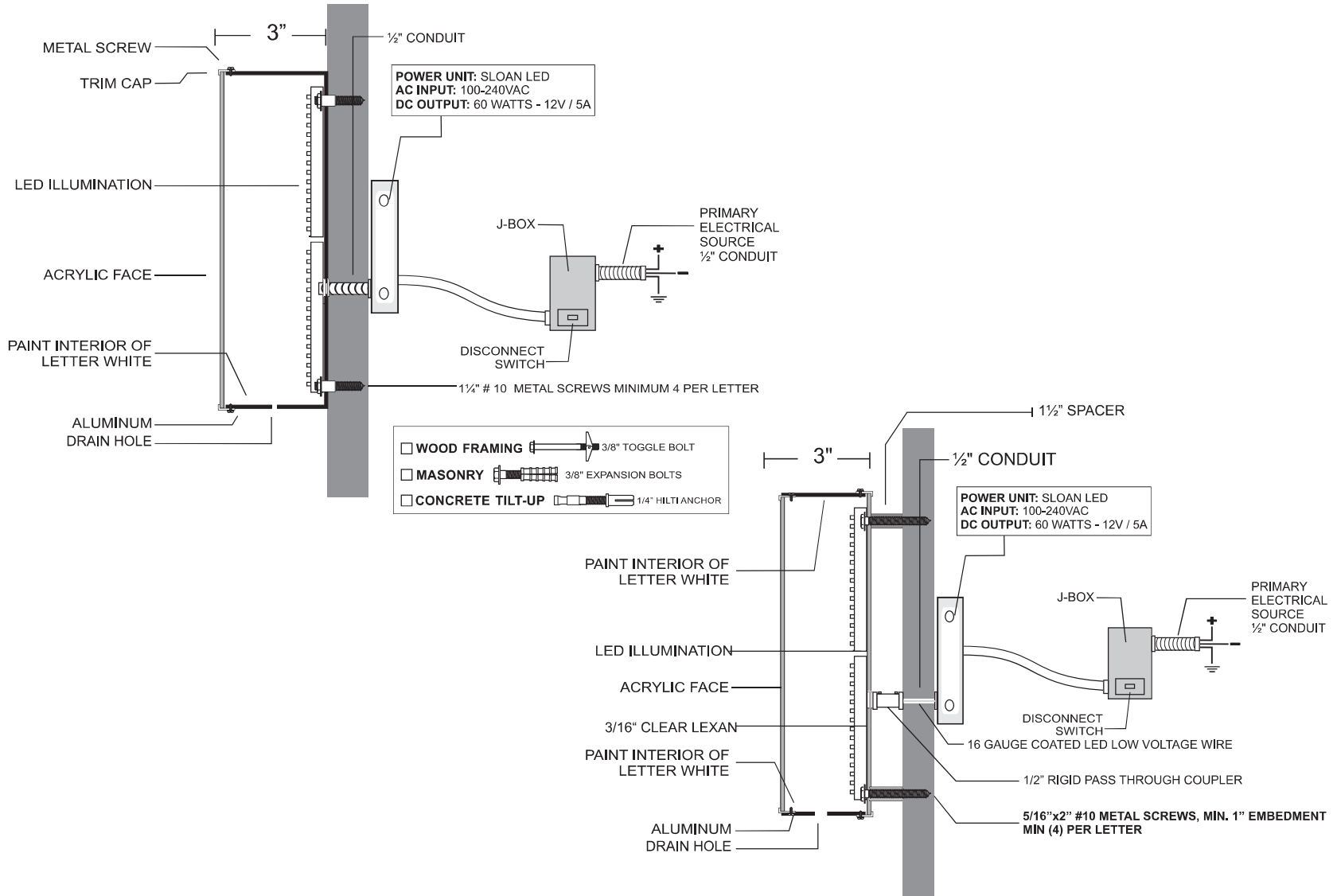
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Typical Wall Sign Section Detail



Client: _____



Address: _____

6TH ST. & MAIN ST.

City: _____

CORONA, CA.

Drawn By: Salesperson: _____

JM DC

Drawing Number: Option: _____

SP-NORTHGATE-CORONA

Drawing Date: _____

07/06/23

Revision: _____

1. 07/19/23
2. 08/10/23
3. 02/16/23
4. 07/04/24

CUSTOMER / LANDLORD APPROVAL

SIGNATURE _____

DATE _____

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**City of Corona
400 S. Vicentia Avenue
Corona, CA 92882**

July 22, 2024

**RE: 6th and Main
DPR2023-0021
Nadel Project 19010**

To whom it may concern:

As the applicant and on behalf of the Ownership of this project we are pleased to make this application for Precise Plan, Parcel Map, General Plan Amendment and Specific Plan Amendment.

The project is in the Mixed Use Downtown General Plan and is zoned D / Commercial. It is located on the North side of 6th street bounded on the West by Belle Avenue and the East by Main Street. On the North the project extends past 4th street and included two (current) residential parcels that will become part of the project. For the purpose of these applications the ownership has procured these two sites, the application for a general plan amendment and specific plan amendment for these sites is part of this process. Those applications are to be processed concurrently with the Precise Plan and Parcel Map.

The scope of work is as follows:

Proposed Land Use: The project will be comprised of a 40,000 s.f. Northgate Gonzalez Market as the anchor. The project also includes a re-design / re-purpose of the existing Citizens Bank which will become a multi-tenant retail / restaurant building. The project is in the Downtown plan area and is bounded by commercial on the West / South and East. The project is bounded on the North by residential and commercial parcels in the downtown revitalization plan area.

Site Design: The site combines a number of disparate lots / parcels into a cohesive whole with Market anchoring the project. The project proposes a pedestrian crossing light at 5th Street crossing Main – this will provide a pedestrian link between the project and the Mall to the East.

Architecture and Materials: Both the new Market building and the re-positioned Bank building utilize a Spanish colonial design approach using clean, white stucco with arched features as well as tower elements with clay tile roofs.

Parking and Access: The parking for the project is on-grade and convenient for the users of both the market and the bank building. Access is off of Main, 6th and Belle with Service access at the rear of the site off of Belle. Parking meets code requirements based upon use.

Landscaping: The landscaping is compliant with all local codes and includes an attractive fountain / seating area at the South East corner of the market fronting main

street. This creates an attractive landscape buffer between the busy roadway and the Market building.

Fencing: As required by code we are proposing a solid masonry screen wall along the North side of the project at the interface of the adjacent properties.

We feel that this project will be a successful component of the revitalized downtown of Corona and look forward to working with the City to complete this vision.

Best regards,

NADEL STUDIO ONE, INC.

David Anderson, AIA, NCARB
Principal – Retail Development



City of Corona Planning Commission/City Council
Corona City Hall
400 S. Vicentia Avenue
Corona, CA 92882

October 10, 2025

Subject: Northgate Gonzalez Market -Business & Operations Plan Submission

Dear Members of the Planning Commission and City Council:

On behalf of Northgate Gonzalez Market, we are pleased to submit the following Business and Operations Plan for your review and consideration. As a multigenerational family business deeply rooted in Southern California since January 2, 1980, our mission has always been to serve communities through fresh ingredients, prepared food, and unique cultural experience that celebrate Mexican heritage and community connection.

We are excited about the opportunity to bring this experience to the City of Corona-providing not just a grocery store, but a destination that honors culture, family and community. We appreciate your time and thoughtful consideration of our proposal.

Sincerely,

A handwritten signature in black ink, appearing to read 'Paul Mittmann', is located below the 'Sincerely,' text.

Paul Mittmann
Senior Vice President Real Estate, Development

Business & Operations Plan

Operations

Northgate Markets is a multigenerational family business that has been serving communities in Southern California since January 2nd of 1980 through fresh ingredients, prepared foods, and unique experiences. The family is in its 4th generation and has 32 family members spanning 3 generations working within the business. The passion behind serving great products in a family environment is core to the family's values. The first store the family opened in 1980 was a small 2,000 square foot neighborhood market in Anaheim, California. Today, the company has forty-three markets with three distinct formats that serve its communities. The largest of those formats is called Mercado Gonzalez which brings to life the rich history, ingredients, regional prepared foods, art, and music of Mexican culture. This experience is a unique retail concept that does not exist anywhere else in the United States. Our first Mercado Gonzalez location opened in November of 2023 in Costa Mesa, California and has been a tremendous success with people coming from both near and far to experience a Authentic Mexican Food Experience.

The design aims to immerse communities in an authentic and traditional Mexican mercado experience. A mercado in Mexico is a traditional marketplace, the kind that eschews the trappings of a typical American supermarket with its bright lights, uniform look and packaged foods. It's a hub of different individual, specialty merchants' stalls each named and decorated to suit, with an old-world approach to visual merchandising. Displays are lush, the senses are piqued, and everything is visible. Shoppers mingle alongside diners. Northgate Gonzalez brings not only the freshest produce, meats, and seafood for a market style shop but also unique regional Mexican prepared foods such as

- Ceviches & Cocteles
 - Mexican shrimp and fish from Sea of Cortez
 - Fresh veggies
 - Unique regional sauces
- Carnitas & Chicharrones
 - Slow cooked kettles
 - Traditional authentic method
- Salsas & Guacamole
 - Mild to spice
 - Regional Mexican recipes
 - Use of many different fresh and dry chiles
- Artisan Mexican Cheeses
 - Locally sourced and produced Mexican cheeses
 - From regions of Oaxaca, Jalisco, Chihuahua, and others
- Tamales & Champurrado
 - Corn husk and banana leaf regional tamales
 - Champurrado make from fresh masa and cacao
- Churros & Hot Chocolate
 - Made from fresh warm dough and fried to order
 - Cacao from Tabasco, Mexico used for seven hot chocolate varieties
- Traditional Soups & Stews

- Stews from all over Mexico
- Protein and plant-based stews
- Natural Juices
 - Made to order
- Tacos & Tortas
 - Regional tacos varieties
 - Unique torta from different regions of Mexico



Puestos are designed as standalone stalls with their own names, finishes, materials, graphics and signage. They fill the entire floor, creating multiple “streets” or “alleys” to explore, and a flow of discovery rather than the efficiency of straight lines. Puestos each focus on an iconic Mexican specialty from different regions, whether food or cultural. Cooking puestos feature open kitchens, visible theater, its own check-out, and counter seating for that marketplace experience. Important ingredients like chiles secos (dry hot chili peppers) broke out of conventional departments by becoming standalone puestos of specialty displays that capture the vibrant and eclectic feel of a traditional Mexican mercado’s merchandising approach. Puestos range from the founder Don Miguel Gonzalez Jimenez’s own crafted family carnitas recipe, to local taco and torta sandwich trucks going brick-and-mortar for the first time, and an iconic Mexican churro shop, Churros El Moro. The puestos are enveloped with open areas of communal seating, vibrant artist murals, an indoor bar, patios, and a standalone fine-

dining restaurant. Mercado Gonzalez imports the familiar flavors, sounds, and visual palette that the local Mexican community is familiar with and nostalgic for. Whether long-time immigrants seeking out a memory, new immigrant missing home, American-born second or third generation Hispanics, or non-Hispanic food explorers who have a passion for learning about Mexican culture, the mercado format creates an amazing experience.



Site Organizational Structure & Operations

Northgate Gonzalez will provide the community fresh ingredients, a full grocery assortment, unique Mexican prepared foods, and unique experience through its best in class company culture and people first approach. The typical Northgate Gonzalez market will employ approximately 180-200 community members and deliver unmatched hospitality. Currently, Northgate employs over 7,000 associates and is proud of its commitment to education, development, and growth. This mercado will not only allow for upward mobility but also foster community development and a better future for employees. Examples of how Northgate Market develops its associates are the following

- Scholarships and scheduling flexibility for school
- Internal leadership development classes and programs
- Leadership seminars and clinics
- 70% full-time staffing

Below is the organizational structure of the mercado which oversees all operations as well as hiring, on-boarding, and training of associates.

MERCADO GONZALEZ STRUCTURE								
STORE TEAM LEADER	MERCADO TEAM LEADER							
ASSISTANT STORE TEAM LEADER	FRESH PREPARED TEAM LEADER		PERISHABLES TEAM LEADER		CENTER STORE TEAM LEADER		CUSTOMER EXPERIENCE TEAM LEADER	
DEPARTMENT TEAM LEADER	PASTELERIA AND PANADERIA GROUP TEAM LEADER	PANADERIA LEAD POSTRES LEAD	CARNICERIA & CHORIZERIA GROUP TEAM LEADER	MEAT LEAD (1) MEAT LEAD (2)	GROCERY & BEVERAGE GROUP TEAM LEADER	CENTER STORE TEAM LEAD	CUSTOMER SERVICE GROUP TEAM LEADER	CUSTOMER SERVICE LEADERS
MARKETING SPECIALIST	GUISADOS, BIRRERIA & AZADOS GROUP TEAM LEADER	PF GUIADOS LEAD PF AZADOS LEAD PF BIRRERIA LEAD	CREMERIA GROUP TEAM LEADER	CREMERIA LEAD	DELI & FROZEN GROUP TEAM LEADER		OPERATIONS GROUP TEAM LEADER	OPERATIONS TEAM LEAD
STORE OPERATIONS COORDINATOR	CAZOS AND TAMALERIA GROUP TEAM LEADER	PF CAZOS LEAD PF TAMALERIA LEAD	PRODUCE GROUP TEAM LEADER	PRODUCE LEAD	DULCERIA & GM GROUP TEAM LEADER			
	SALSA, AGUA FRESCA & FRUTERIA TEAM LEAD	PF AGUAS/FRUTA LEAD PF SALSAS LEAD	FLORERIA GROUP TEAM LEAD		LICORTERIA GROUP TEAM LEADER			
	TORTILLERIA GROUP TEAM LEADER	TORTILLERIA CORN LEAD TORTILLERIA FLOUR LEAD TORTILLERIA FRIED LEAD	SPICES, CHILLI, SECO, NUTS & GRAINS TEAM LEAD					
	PESCADERIA & CEVICHERIA GROUP TEAM LEADER	SEAFOOD MEAT LEAD CEVICHE LEAD						

Our team’s priority is the safety and enjoyment of our customers through the care of our associates. The Mercado will be open seven days a week from 7am-10pm and offer solutions for unique day parts such as breakfast, lunch, and dinner with menus changing by day part. Our team will also be responsible for overseeing the events in the mercado such as cooking classes, live entertainment, and pop-ups. Day to day the mercado leadership team ensures that the hospitality, product quality, cleanliness, and safety of the mercado are consistent and maintain first-class operating standards.

Network & Existing Client Relationships

Our customers, vendor partners, service providers, and associates make up a integral part of our ecosystem and allow us to serve our community the best products and experiences. Below is a list of our network and existing client relationships that will contribute to the success of the business and operations plan

- Harris Ranch Beef Company: provider of the highest quality beef from Northern California
- Rancho Janitorial: provider of janitorial and sanitation supplies and services

- Securitas: provider of on-site security
- NCR: provider of point-of-sale systems and technology
- Bake Mark: provider of flour and ingredients for scratch baked goods and pastries
- Honeyville: provider of quality corn for tortilla making
- UKG: provider of human resource software
- Melissa's Produce: provider of fresh produce
- Churros El Moro: Iriarte family from Mexico City who makes churros and hot chocolate

In addition to this list, we use many local suppliers and servicers. Many of these are small family-owned businesses of which we were their first customer. Since the founding of Northgate González Market in 1980, the family patriarch and matriarchs, Don Miguel and Doña Teresa Gonzalez understood the importance of giving back to the community to be a successful business. More than 40 years later, their legacy continues through 2nd, 3rd, and 4th generation family members and associates who continue to make it a priority not to be just a business but a valuable and caring neighbor.

The González family has led by example under three main pillars of giving – faith, education, and well-being. The current 13 owners established the Familia González Reynoso Foundation (FGRF). The foundation supports organizations involved with higher education and human services. Over the past two decades, FGRF has awarded more than \$2 million in community donations and scholarships.

As part of the company's comprehensive well-being initiative, Viva La Salud, Northgate Market has hosted a diverse range of over 600 events in 2023. These events, held at our stores, cover a wide spectrum of health-related topics. From sponsoring mammography and health screenings to providing chronic illness detection, nutrition and healthy cooking classes and demonstrations, and COVID-19 vaccines and boosters, we have strived to address the holistic health needs of our community. As a testament to our commitment, Northgate Market is the only retailer in the US that has more than 3,000 bilingual nutrition tags placed throughout the store.

In addition, the company offers back-to-school fairs and annual toy giveaways. When the COVID pandemic hit, Northgate Market committed over \$1.5 million to support and help the most vulnerable community members by supporting food insecurity programs. The company gives schools, churches, and nonprofits more than \$250,000 in holiday food vouchers and turkeys to help needy families during the holidays.

In partnership with UC San Diego and the USDA, Northgate Market is part of the Mas Fresco, More Fresh program that provides community members participating in the CalFresh program up to \$100 a month in nutrition incentives when they purchase fresh fruits and vegetables with their CalFresh benefits at any Northgate Market, including the new Mercado store in Costa Mesa

Teaching children the importance of good nutrition is also a priority for Northgate Market. We are proud to partner with and host every year the Cooking Up Change[®] program this year, we will host our 11th annual event on March 16th. In partnership with the Healthy Schools Campaign and Kid Healthy, the program challenges high school culinary students from underserved communities to create healthy, great-tasting meals that meet the real-life requirements of the national school meal program.

As Northgate Market opens new stores in Southern California, it plans to make substantial investments in the community, including scholarships and donations to local schools and nonprofit organizations, and begin serving the community by providing nutrition education and health care outreach.

Company Org Composition

Northgate Markets is an organization which prides itself on long-term thinking and investment in people and the community. This culture permeates throughout our team and our focus is on providing the best experience for our customers and associates. The business is led by 13 brothers and sisters which make up the second generation of ownership as well as a well-rounded executive team. The Office of the President are two of the siblings Oscar and Miguel Gonzalez Reynoso. The executive team oversees all areas of the business including human resources, distribution, procurement, legal, marketing, information technology, maintenance, operations, enterprise protection, risk management, and finance. Below you will find information about key leaders of the organization.

Northgate Markets self distributes 60%+ of its products and delivers them at least once a day to every location. This includes fresh produce, meat, and seafood. Strong values and ethics are at the core of the organization.*

*Delivery Footnote

Safety Processes

Northgate Market cares about its Associates and works hard to provide an environment that is safe, healthy and promotes working in a safe manner to prevent accidents. Senior management considers no phase of operation or administration of greater importance than safety and believes that accidents that result in personal injury or damage to property and equipment represent unneeded pain, suffering, loss of income, waste and loss of quality of life. It is the policy of Northgate Market to conduct all operations with the utmost regard to safety. The Company strongly believes in the philosophy "Safety Starts with Me" and encourages personal responsibility regarding safety, not only for oneself, but for all co-workers and customers as well. The Company expects each Associate to work safely, since each Associate's family depends on them to return home safely. Working safely at all times is required for continued employment. Northgate has adopted a "Zero Accident" goal as a method of avoiding injuries. ALL accidents are considered preventable in the workplace. To accomplish this goal, planning for safety will be incorporated into all phases of the Company's work. All reasonable steps shall be taken by management to create and maintain safe and healthy places of work by: Providing information and training to Associates about health and safety hazards; Identifying and correcting health and safety hazards and encouraging Associates to report hazards; Providing proper tools and equipment, including protective equipment, and the training in how to use these correctly; Working in compliance with all applicable Local, State, and Federal environmental, health, and safety laws and regulations.

Northgate's security strategy focuses on creating a safe environment for everyone. Our efforts are directed at protecting people and property through the use of various security measures that include, but are not limited to, alarms, CCTV, security, live monitoring, and extensive training for our associates.

Maintenance

Northgate Gonzalez Markets Strategic and Preventive Maintenance Group is comprised of 7 dynamic team members in Construction and 35 skilled individuals in Maintenance. Northgate Gonzalez Markets strategic asset management is a top-down equipment and property management framework for long-term maintenance and operation planning. With our focus on operating efficiencies, longevity, and sustainability, Northgate Gonzalez Markets deploys a strategy of substantial initial capital investment and asset upgrading followed by a strict maintenance and reinvestment program. The ongoing focus of Northgate Gonzalez Markets maintenance program shall be centered on preventive maintenance. Preventive Maintenance, which prevents problems before they arise, is a benefit to the physical plant and systems in the following areas:

- Prolonging the life of equipment and physical plants
- Reducing downtime
- Decreasing reactive maintenance requests
- Managing compliance and inspection needs
- Preventing costly emergency repairs
- Improving reliability and safety of equipment or systems
- Lowering maintenance costs and energy consumption
- Increasing workplace safety
- Improving asset value, lifespan, and efficiency
- Decreasing unplanned maintenance and inspections

Property and equipment shall be serviced and in some instances be replaced according to manufacturer instructions and municipal requirements. All preventive maintenance and capital projects shall adhere to first-class industry standards and according to the appropriate operations and maintenance instructions. Northgate Gonzalez Markets maintenance group is 24 hour 365 days a year operation, who's work does not go unnoticed and is mandatory for the success of operations.

Tenant, Property Standards

Our quality approach and first-class standards to tenant services and property management is important to us as we are creating a safe space and a top-tier retail experience from the moment a community member drives or steps into the property. The tenant services we offer include:

- Common area maintenance
- Hood cleaning
- IT support
- Pest control
- Security and monitoring
- POS Management

Outstanding real estate management is at the core of every successful Northgate. Our management teams are committed to fully understanding a property's strengths, weaknesses and positioning in the marketplace, while proactively engaging all available resources to achieve an outstanding customer experience and environment. With 40-plus years of experience, our teams have established efficiencies to help maximize potential, reduce operating costs and enhance value. Management services include:

- Strategic Planning

- Budgeting, Financial Reporting, Forecasting, and Expense Reconciliation
- CAM Audit and Analysis
- Tenant Administration
- Building Operations and Maintenance
- Property Accounting
- Tenant and Vendor Relationship Management

Experience

Northgate Markets has a long history since 1980 of managing and operating markets. In the 44 years Northgate has developed expertise in placemaking while managing a healthy business that puts people first and is built for longevity. The evolution of the organization has been steady, and the importance of paced growth has been key to a sustainable business. Today Northgate Market operates 43 locations across 4 counties including Los Angeles County, Orange County, San Diego County, and the Inland Empire. Northgate supports its stores with its corporate office team in Anaheim, California as well as its distribution center. As previously mentioned, Northgate has 7,000 employees and growing. Most of Northgate's stores has full bakery, tortilleria, produce department, meat department, grocery, and fresh prepared foods. Northgate serves 40,000,000 customers a year throughout its communities.

Jan2/80 & Oct 7/93 NORTHGATE #1 722 N. Anaheim Blvd Anaheim, CA 92805 Converted to Community Center	March ____ 1986 NORTHGATE #2 580 W. LA HABRA BLVD LA HABRA, CA. 90631 Closed 3/28/17	3/29/2017 NORTHGATE #2 1305 W. Whittier Blvd La Habra, CA 90631	Nov. 81 NORTHGATE #3 1150 N. East Street Anaheim, CA 92805	March 8/89 NORTHGATE #4 9101 TELEGRAPH RD PICO RIVERA, CA 90660 Re-Grandopening 3/30/2016	Nov. 10/90 NORTHGATE #5 720 W. LA PALMA AVE. ANAHEIM. CA. 92801	NORTHGATE # 6 1425 Hacienda Blvd. La Puente, CA 91744 Closed 12/18/2006	January 14/93 NORTHGATE # 6 831 Hacienda Blvd. La Puente, CA 91744	6/24/2005 NORTHGATE # 7 1623 W. 177h. St. Santa Ana, CA 92706 Closed 11/7/2022	11/9/2022 NORTHGATE # 7 2603 Westminster Ave Santa Ana, CA 92706
May 15/97 NORTHGATE #8 409 E. 4th Street Santa Ana, CA 92701	Jul 20/95 NORTHGATE #9 230 N. Harbor Blvd. Santa Ana CA 92703	May 18/2000 NORTHGATE #10 6720-6801 Atlantic Blvd Bell, CA 90201	Nov 10/99 NORTHGATE # 11 1120 S Bristol Ave Santa Ana, CA 92706	Nov. 24/2000 NORTHGATE #12 2120 Pacific Ave Long Beach, CA 90806	Aug. 28/2002 NORTHGATE #14 6991 E. Lincoln Ave Buena Park, Ca 90620	4/2/2004 NORTHGATE #15 1010 S Main Street Santa Ana, Ca 92701	3/27/2004 NORTHGATE #16 929 S Euclid Ave Anaheim, Ca 92804	11/19/2004 NORTHGATE #17 16259 Paramount Blvd Paramount, Ca 90723	10/28/2004 NORTHGATE #18 770 S Harbor Blvd Santa, Ca 92704
1/19/2005 NORTHGATE #19 2633 Santa Ana St Southgate, Ca 90280	12/3/2005 NORTHGATE #20 3828 Peck Road El Monte, Ca 91732 Re-grandopening 4/20/2016	6/22/2006 NORTHGATE #21 1410 S. 43rd St San Diego, Ca 92113	12/21/2006 NORTHGATE #22 320 W Francisquito Avh West Covina, Ca 91790	1/28/2006 NORTHGATE #23 1222 N Avalon Blvd Wilmington, Ca 90744 Closed 4/29/2009	5/1/2009 NORTHGATE #23 110 W. Pacific Coast Hh Wilmington, Ca 90744	9/27/2007 NORTHGATE #24 11660 E Firestone Blvd Norwalk, Ca 90650 Re-Grandopening 11/11/2015	3/29/2007 NORTHGATE #25 3930 W. Rosecrans Ave. Hawthorne CA 90250	3/1/2008 NORTHGATE #26 2909 Coronado Ave. San Diego, CA 92154	3/1/2008 NORTHGATE #27 1058 3rd Ave. Chula Vista, CA 91911
3/1/2008 NORTHGATE #28 606 N. Escondido Blvd. Escondido, CA 92025	3/1/2008 NORTHGATE #29 1150 E. Vista Way Vista, CA 92084	3/1/2008 NORTHGATE #30 1346 S. Mission Rd. Fallbrook, CA 92028	12/12/2013 Northgate #31 1950 Main St. San Diego CA 92113	9/1/2010 NORTHGATE #32 15107 S Atlantic Blvd Rancho Dominguez,CA 9022	6/24/2010 NORTHGATE #33 2323 W. Olympic Blvd Los Angeles, CA 90006	9/15/2010 NORTHGATE #34 710 W. Chapman Ave. Placentia, CA 92870	11/14/2012 Northgate #35 10801 Prairie Ave Inglewood, 90303	10/29/2012 Northgate #36 5403 University Ave San Diego CA 92105	10/17/2013 Northgate #37 425 S. Soto Ave Los Angeles CA 90033
4/15/2014 Northgate #38 944 E. Silauson Ave Los Angeles, 90111	10/29/2014 Northgate #39 4700 Cherry Ave Long Beach,90807	7/20/2016 Northgate #40 1150 East St Anaheim CA 92805	12/18/2013 Northgate#41 4700 Inglewood Los Angies, CA	12/18/2013 Northgate #42 2038 E. 10th St Long Beach 90804	11/20/2019 Northgate #43 10391 Magnolia Ave Riverside CA 92505	11/17/2023 Northgate #44 2300 Harbor Blvd Costa Mesa, CA 92626	6/24/2021 NORTHGATE #45 4155 Tweedy Ave South Gate, CA 90280	11/29/2023 Northgate #46 9630 Sierra Ave Fontana, CA	



Capital Improvement Experience

Northgate González Market began serving Hispanic customers over 43 years ago by building and operating grocery stores and grocery-anchored shopping centers that catered to Mexican customers. Since opening our first store in 1980, Northgate has expanded our presence across Southern California, reaching underserved communities and opening exceptional opportunities for the retailers who serve them.

Today, Northgate González Real Estate continues to grow and strengthen communities by investing in quality real estate projects that include retail, industrial, housing, medical, and mixed-use properties focused on meeting the needs of the thriving Mexican market. Northgate Gonzalez Real Estate holdings are comprised of forty-nine properties. We own and manage 1,501,942 square feet of retail, industrial, medical, commercial, and residential space. We have a 98.4% occupancy rate; 95.7% of our multi-tenant retail and industrial buildings are leased.

Northgate González Real Estate is a vertically integrated real estate firm whose platform is focused on investing, managing, and executing in assets across the risk spectrum. Our strategy will be centered on our successful, profitable, and aspirational Northgate Markets and the customers we serve by making some of the following community investments and enhancements:

- New construction.
- Redevelopment
- Strategic Acquisitions-Asset and Portfolio
- Core Plus-Remerchandising

Our in-house team of real estate professionals executes at the highest level. Our asset capabilities include sourcing, property management, asset management, design, entitlement procurement, project management, development management, maintenance, and capital markets. Our team is committed

to every project from painting and mechanical replacement to ground up shopping center construction. Northgate Markets is continuously improving and expanding the portfolio. In 2023 and 2024, we invested over \$100,000,000 in capital projects. We look forward to putting our capital and experience to work in Corona and the surrounding communities.

Staffing & Training

Through its core values Northgate strives to be an employer of choice and deliver best in class associate experience. It offers many benefits in addition to growth opportunities. At Northgate Gonzalez the company will employ approximately 200 people. Jobs will range from leadership to specialized roles. This will include our robust on-boarding and training program that covers values, safety, food safety, customer service, regulatory, and technical training. Benefits that we offer include

- Competitive pay
- Health, dental, and vision benefits for full-time associates
- 20% off discount on food purchases (some exceptions apply) for active associates and their immediate family members residing in the associate's household.
- 401k (after 6 months or 1000 hours tenure)
- Flexible schedules and self-scheduling tools for hourly associates
- Leadership Development and Advancement opportunities
- Associate Birthday Recognition with the opportunity to win a \$10, \$25, \$50, \$100 or \$250 gift card
- Education Reimbursement - \$3000 annual maximum for college tuition, books, and materials
- Full tuition to the *Retail Management Certification* program through Western Associate of Food Chains (WAFC)
- Annual scholarships through the Gonzalez-Reynoso Scholarship Foundation
- Educational Opportunities: Sponsored GED Prep courses, ESL courses
- Employee Assistance Program (EAP) to assist associates with problems that may impact their health, mental and emotional well-being
- Paid Time Off (calculated based on tenure)
- Sick days – minimum 5 days
- Additional Health & Wellbeing Programs: Fitness Sponsorships, Health Education and Fairs, Volunteer Opportunities and Health Management Programs available to all associates and dependents
- Access to Employee Loan Program via 3rd party credit union
- Gym discounts
- Yearly subsidy for purchase of safety shoes
- New Associate Referral Program bonus
- Service Recognition Awards for associates that reach milestone anniversaries of employment



Delivery Notes:

Hours and loading deliveries all occur during business operations of 7a.m. To 10 p.m.

From Northgate DC, beer & soda vendors:

48–53-foot trailer with tractor (truck weight less than 80k):

1.4 load of produce, meat and Deli per day 7 days a week. They will deliver before 10 am and will follow any local or city curfew laws.

1 Grocery load per day 5 days a week delivered in the afternoon before 10 pm.

2 Beer/Soda loads per day delivered after 7 am daily, no Sunday deliveries.

4.5 Large truck and trailers average daily

Bobtail trucks Bread, Tortillas & supplies (truck weight less than 30k lb.) all after 7 am and before 6 pm.

Bimbo 5 trucks per week

Chips 5 trucks per week

Supplies 2 trucks per week

Tortilla 3 trucks per week

2.2 bobtail's average daily



CITY OF CORONA MITIGATED NEGATIVE DECLARATION

NAME AND DESCRIPTION OF PROJECT:

GPA2024-0003: General Plan Amendment to change the General Plan land use designation of two parcels located at 323 S. Belle Avenue and 322 S. Washburn Avenue from Low Density Residential (LDR) and General Commercial (GC), respectively, to Mixed Use Downtown (MUD), and establish the MUD land use designation on approximately 0.87 acres of public right-of-way (Fourth and Fifth Streets).

SPA2024-0003: Specific Plan Amendment to the Downtown Revitalization Specific Plan to change the zoning of two parcels located at 323 S. Belle Avenue and 322 S. Washburn Avenue from Single Family (SF) and Gateway Business (GB), respectively, to Downtown (D), and to establish the Downtown zoning on approximately 0.87 acres of public right-of-way (Fourth and Fifth Streets).

PM 38981: Parcel Map to create one lot totaling 4.88 net acres for commercial purposes located at the northwest corner of S. Main Street and W. Sixth Street, within the proposed Downtown (D) zone.

PP2024-0001: Precise Plan to review the site plan, architecture, landscaping, and other features for a proposed 40,000 square foot Northgate Gonzalez market and remodel of an existing 6,930 square foot commercial building on 4.88 net acres located at the northwest corner of S. Main Street and W. Sixth Street, within the proposed Downtown (D) zone.

PROJECT LOCATION: The project site is approximately 5.68 gross acres (4.88 net acres) and is located at the northwest corner area of S. Main Street and W. Sixth Street. (Assessor's Parcel Numbers: 117-103-026 and -027, 117-142-006, -007, -008, -009, 117-142-015, -016, -017, -018, -019, -020, and 117-044-017).

ENTITY OR PERSON UNDERTAKING PROJECT:

Northgate Gonzalez Market - Marco Arzola
1201 N. Magnolia Ave.
Anaheim, CA 92801

The City Council, having reviewed the initial study of this proposed Project and the written comments received prior to the public meeting of the City Council, and having heard, at a public meeting of the Council, the comments of any and all concerned persons or entities, including the recommendation of the City's staff, does hereby find that the proposed Project may have potentially significant effects on the environment, but mitigation measures or revisions in the Project plans or proposals made by or agreed to by the applicant would avoid or mitigate the effects to a point where clearly no significant effects will occur. **Therefore, the City Council hereby finds that the Mitigated Negative Declaration reflects its independent judgment and shall be adopted.**

The Initial Study and other materials that constitute the records of proceedings are available at the office of the City Clerk, City of Corona City Hall, 400 South Vicentia Avenue, Corona, CA 92882.

Date: _____

Mayor
City of Corona

Date filed with County Clerk: _____

EXHIBIT 16

CITY OF CORONA INITIAL STUDY / ENVIRONMENTAL CHECKLIST

PROJECT TITLE: Northgate Market Development Project

- General Plan Amendment (GPA2024-0003)
- Specific Plan Amendment (SPA2024-0003)
- Parcel Map 38981 (PM 38981)
- Precise Plan (PP2024-0001)

PROJECT LOCATION: The project site is located at the northwest corner area of W. Sixth Street and S. Main Street and is comprised of 13 parcels and portions of W. Fifth Street and W. Fourth Street . The Project's location is depicted on Figure 1, *Regional Location Map*, and Figure 2, *Local Vicinity Map*.

PROJECT PROPONENT:

Northgate Markets - Marco Arzola
1201 N. Magnolia Ave.
Anaheim, CA 92801

PROJECT DESCRIPTION:

Project Overview

The proposed Project includes the development of a 40,000 square foot Northgate Gonzalez grocery market and remodel of an existing 6,930 square foot bank building into a bank and restaurant use on a combined acreage of 4.88 net acres consisting of 13 combined parcels. The development would include landscaping, parking and public right-of-way improvements such as sidewalks, curb and gutter, and utility and stormwater improvements.

Development of the proposed Project requires multiple entitlements which include a General Plan Amendment (GPA), Specific Plan Amendment (SPA), Parcel Map (PM) and Precise Plan (PP). The following describes the reason for each entitlement:

- **GPA2024-0003:** GPA2024-0003 is a proposal to amend the General Plan Land Use Map to change the existing land use designation of two parcels located at 323 S. Belle Avenue and 332 S. Washburn Avenue from Low Density Residential (LDR) and General Commercial (GC) to Mixed Use Downtown (MUD). The GPA will also establish the MUD designation on a section of Fifth Street and Fourth Street, totaling approximately 0.87 acres (36,697 square feet), that currently bisect the project site. The MUD designation proposed for the two parcels and for the aforementioned sections of Fifth and Fourth Streets would establish General Plan consistency with 11 other parcels that are already designated MUD within the project site
- **SPA2024-0003:** SPA2024-0003 is a proposal to amend the Downtown Revitalization Specific Plan to change the zoning designation of two parcels located at 323 S. Belle Avenue and 332 S. Washburn Avenue from Single Family (SF) and Gateway Business (GB) to Downtown (D). The SPA will also establish the Downtown (D) zone on a section of Fifth Street and Fourth Street, totaling 0.87 acres, that currently bisect the project site. The Downtown (D) zoning proposed for the two parcels and for the aforementioned sections of Fifth and Fourth Streets would establish zoning consistency with 11 other parcels that are already zoned Downtown within the project site.
- **PM 38981:** PM 38981 is a parcel map application to merge 13 existing parcels plus approximately 0.87 acres of existing public right-of-way within Fifth and Fourth Streets into one parcel totaling 4.88 net acres in order to accommodate the proposed commercial project. The existing public rights-of-way within Fifth and Fourth Streets will be abandoned as part of the parcel map process.

- **PP2024-0001:** PP2024-0001 is a precise plan application to review the site plan, architecture, and other features of the Project.

The Project site is comprised of 13 contiguous parcels and approximately 0.87 acres of public rights-of-way totaling 5.68 gross acres. The commercial project will be developed on 4.88 net acres, after the dedication of streets for public rights-of-way purposes. The Project is anticipated to be developed in a single phase with an anticipated opening year of 2028. Refer to Table 1 below.

Table 1: Existing and Proposed Zoning and General Plan of Project Site

APN	Address	Existing General Plan Land Use	Proposed General Plan Land Use	Existing Zoning	Proposed Zoning
117-103-026	323 S. Belle	LDR	MUD	SF	D
117-103-027	332 S. Washburn	GC	MUD	GB	D
117-142-006	413 S. Belle	MUD	MUD	D	D
117-142-007	417 S. Belle	MUD	MUD	D	D
117-142-008	421 S. Belle	MUD	MUD	D	D
117-142-009	215 W. Fifth	MUD	MUD	D	D
117-142-015	401 S. Belle	MUD	MUD	D	D
117-142-016	N/A	MUD	MUD	D	D
117-142-017	410 S. Main	MUD	MUD	D	D
117-142-018	407 S. Belle	MUD	MUD	D	D
117-142-019	411 S. Belle	MUD	MUD	D	D
117-142-020	450 S. Main	MUD	MUD	D	D
117-144-017	225 W. Sixth	MUD	MUD	D	D

Project Features

Development Summary

Two of the 13 parcels within the Project area contain existing commercial buildings which will be demolished. The two structures include an existing strip retail building and a drive-in bank related to the existing Citizens bank business. The two former single family dwelling units, located at 323 S. Belle Avenue and 332 S. Washburn Avenue, were demolished within the last two years.

The redevelopment of the project site will include the construction and operation of a 40,000 square foot grocery market with a 650 square foot outdoor patio area. Additionally, the vacant Citizens Bank building will be remodeled into a 3,297 square foot sit-down restaurant and a 3,633 square foot bank. The site plan situates the grocery market on the northern portion of the site, while the bank/restaurant building is positioned at the southwestern corner. A shared parking lot will be centrally located to serve both facilities.

Parking

The Project includes 261 parking spaces, of which 236 are on-site and the remaining 25 are on-street parking spaces located adjacent to the Project site, as permitted by the Downtown Revitalization Specific Plan.

Access and Circulation

The Project proposes a total of five vehicular access points from the surrounding roadways. The main entrance into the Project site is located at the site’s eastern perimeter and will allow for vehicles to access the site from S. Main Street. The entrance is proposed directly across from the existing entrance to the Corona Mall commercial center located on the east side of S. Main Street. This entrance will form a four-way intersection at S. Main Street and will be signalized to allow for full access. A secondary access point is located at the site’s southern perimeter and will allow vehicles to enter the site from W. Sixth Street. This entrance will be restricted to right-in and right-out turn movements and will be unsignalized. Two additional secondary access points are located at the site’s western perimeter and will allow for vehicles to enter the site from S. Belle Avenue. The fifth access point is located at the northeastern portion of the site on S. Belle Avenue that will primarily be used for truck access to the market’s rear loading area. The access points on S. Belle Avenue will allow for full access and will be unsignalized.

Additionally, the southbound lanes on Main Street adjacent to the Project Site will be restriped to include the following:

- Two (2) 12-foot wide through-lanes;
- One (1) left-turn pocket to allow for vehicles to turn left into the North Corona Mall entrance; and
- One (1) right-turn pocket to allow for vehicles to turn right into the Project Site.

Delivery trucks will access the Project site from the SR-91 Freeway off ramp at Main Street and head south, turning westbound onto W. Sixth Street and northbound onto S. Belle Avenue and into the Project site. After unloading, the trucks will head back out to S. Belle Avenue, proceed south turning left on W. Sixth Street and left on S. Main Street heading north, back onto the SR-91 freeway west/east bound on-ramps.

Architecture

The proposed height of the market and bank/restaurant building is 40 feet and 33 feet and 5 inches respectively, as measured from finish grade to the tower elements of both buildings. Project elevations include a variety of architectural elements such as articulated massing and finish material palettes and have design characteristics consistent with Spanish Mediterranean architecture.

The Project plan is shown in Figure 8, *Architectural Site Plan*, and conceptual colored elevations of the Project are shown in Figure 9, *Elevations*.

Fences and Walls

The Project would include construction of a six-foot-high split face block wall along the northern property line, a 12-foot-high screen wall at the truck loading area, a 3-foot high retaining wall with guard rail along the south side of the restaurant area, 3-foot high retaining walls along the west side of the bank/restaurant area, and 3-foot high decorative metal railing around the market's outdoor dining patio area located on the east side of the market. The Project proposes fencing and gates equipped with a Knox box located behind a portion of the market on the north side to restrict access within the partially vacated portion of W. Fourth Street from Washburn Avenue to the west side of the alley.

Lighting

Outdoor lighting would consist of parking lot lighting throughout the market and bank/restaurant parking areas as well as decorative wall-mounted lighting on the buildings. All outdoor lighting would be directed downward and shielded to minimize off-site spillover. The location of all exterior lighting would comply with lighting and glare standards established in the City of Corona Municipal Code (CMC) §17.84.070.

Landscaping

The Project would install approximately 27,642 square feet of new drought-tolerant low water use ornamental landscaping throughout the site (see Figure 10, *Landscaping Plan*). Landscaping would include a variety of trees, such as: *Jacarandas, Palo Verdes, Fern Pines, Southern Live Oaks, Ornamental Orange and Lemon, Date Palms and others*.

Infrastructure Improvements

The proposed development would construct on-site infrastructure improvements that would connect to the existing utility infrastructure in S. Main Street and in S. Belle Avenue as described below.

Gas and Electric

The Project would install underground electric lines that would connect to existing infrastructure in Main Street. Electricity for the Project would be provided by Southern California Edison (SCE).

Water and Sewer

The 40,000 square foot market building is being proposed over an existing 8-inch waterline in W. Fourth Street and part of the 2-inch waterline through the alleyway that continues north. As such, the Project proposes removing and replacing the 8-inch diameter waterline further north, where it will not be under the

proposed building. Domestic water will be supplied to the project through two points of connection (POC) to the existing 8-inch diameter waterline in S. Belle Avenue for the existing building to be remodeled, with one connection serving the restaurant, and the serving the bank. Domestic water for the proposed market building will be through a POC to the existing 8 -inch diameter waterline in S. Belle Avenue near the existing 8 -inch diameter waterline that is to be relocated.

The existing building being remodeled proposes to connect to the existing 6-inch diameter sewer line through a proposed 6-inch private sewer lateral that exits on the east side of the building and continues north parallel to the existing sewer until tying into the existing sewer in W. Fifth Street. From there, the existing sewer that continues north will be removed/abandoned and instead a proposed 6 -inch diameter public sewer line will be directed northeast and tie into the existing 10-inch diameter sewer in S. Main Street. For the proposed market building, a 6 -inch diameter private sewer lateral is proposed at the northeast corner which will tie into the existing 6 -inch diameter sewer at the norther project boundary. Connection details, alignments, and sizes of proposed sewer facilities will be validated during the plan check review process.

Stormwater Drainage

There is an existing 30-inch storm drain line within Main Street along the project frontage, which, based on the City of Corona Drainage Master Plan, the Main Street storm drain line capacity is deficient along the project frontage. Due to this deficiency, the Project will not discharge generated Project storm runoff into this line. Under the developed condition, runoff generated by the Project will be captured via catch basins and conveyed through an underground storm drain system. Collected flows from the area south of the proposed market building will be conveyed via underground storm drain to proposed MWS (Modular Wetlands System) treatment units to address water quality requirements before reaching an underground chamber system within the market parking area for increased runoff mitigation. A pump will convey flows from the underground chambers to a v-ditch along the western project boundary. The v-ditch will allow surface flows to exit the site via an under sidewalk drain to a 24-inch storm drain in S. Belle Avenue. Flows will drain north along S. Belle Avenue following the existing condition drainage pattern.

Construction

Construction was estimated for a two-year construction schedule, which includes demolition, site preparation, grading, building construction, paving, and architectural coating. Construction equipment and staging are to occur on-site, and construction vehicle access is planned along S. Main Street. Table 2 lists the anticipated construction schedule.

Table 2: Anticipated Construction Schedule

Construction Phase	Working Days
Demolition	20
Site Preparation	5
Grading	8
Building Construction	230
Paving	231
Architectural Coatings	231
Total	725

Source: Table 5.1 (Construction Schedule), Air Quality & Greenhouse Gas Emissions Assessment, March 2024

Construction activities would be limited to the hours between 7:00 a.m. and 8:00 p.m. on weekdays (Monday through Saturday) and between the hours of 10:00 a.m. and 6:00 p.m. on Sundays and federal holidays, which would be consistent with the City’s regulations (CMC §17.84.040). Figure 11 shows the Conceptual Grading Plan and Figure 12 shows the Utilities Plan for the Project.

Operation

The proposed Project would operate as a new 40,000 square foot Northgate Gonzelez market with a re-design of an existing Citizen’s Bank to a 3,297 square foot sit-down restaurant and 3,633 square foot bank.

Typical operational characteristics would include customers shopping at the market and visiting both the bank and restaurant. The market will be open seven days a week from 7:00 a.m. to 10:00 p.m. and will employ approximately 180 to 200 employees.

Grocery store deliveries will be from 7:00 a.m. to 7:00 p.m. Monday through Saturday and from 10 a.m. to 6 p.m. on Sundays (per city curfew). There will be a variety of deliveries to the market daily as follows:

- One (1) delivery of produce, meat and deli per day, seven days a week;
- One (1) delivery of groceries per day, five (5) days a week, occurring in the afternoon;
- Two (2) deliveries of beer/soda loads per day from Monday through Saturday;
- 2 to 4 deliveries of bread, chips and tortillas per day.

In total, there are an average of 4 to 5 large truck and trailer deliveries and an average of 2 daily bobtail truck deliveries per day.

It is anticipated that the future restaurant tenant and bank will have a small number of employees, and the restaurant would typically have a daily delivery of supplies from a smaller bobtail truck.

ENVIRONMENTAL SETTING:

CEQA Guidelines §15125 establishes requirements for defining the environmental setting to which the environmental effects of a proposed project must be compared. The environmental setting is defined as “...the physical environmental conditions in the vicinity of the project, as they exist at the time the Notice of Preparation is published, or if no Notice of Preparation is published, at the time the environmental analysis is commenced...” (CEQA Guidelines §15125[a]). Because a Notice of Preparation was not required, the environmental setting for the Project is October 20, 2025, which is the date that the Project’s environmental analysis commenced.

Site Surroundings

The project site is located in an urbanized, developed area within the City. The area is part of the City’s downtown. The existing on-site and adjacent land uses, General Plan land use designations, and zoning classifications are summarized in Table 3.

Table 3: Land Uses, General Plan Land Use Designations, and Zoning Classifications

Location	Current Land Use	Current General Plan Land Use/ Zoning Designations
Project Site	The site is currently partially vacant and also occupied with a strip retail building (to be demolished), vacant bank, and two (2) vacant parcels on the north side of W. Fourth Street.	Mixed Use Downtown (MUD), Low Density Residential (LDR) and General Commercial (GC) / D (Downtown), SF (Single Family) and GB (Gateway Business)
North	Residential to the north and General Commercial to the northeast	Low Density Residential (LDR) and General Commercial (GC) / SF (Single Family) and GB (Gateway Business)
South	Corona Public Library / Commercial Development	Mixed Use Downtown (MUD) / D (Downtown)
East	North Corona Mall / Commercial Development	Mixed Use Downtown (MUD) / D (Downtown)
West	Commercial Development / Residential	Mixed Use Downtown (MUD), Low Density Residential (LDR) / D (Downtown), SF (Single Family)

Source: Field inspection, City of Corona General Plan Land Use, Downtown Revitalization Specific Plan & Zoning District Maps.

Site Description

The existing conditions of the Project site and surrounding areas are depicted in Figure 3, *Aerial View*. The Project site formerly contained various commercial and residential land uses. Currently, there is a vacant Citizens bank building with drive-thru bank and a strip commercial building currently occupied by four tenants. The subject site is nearly rectangular in shape and gently slopes to the north with elevations ranging from 669 to 647 feet above mean sea level.

GENERAL PLAN \ ZONING:

The Project site includes 13 parcels and 0.87 acres of public rights-of-way (Fourth and Fifth Streets), totaling 5.68 gross acres. According to the Corona General Plan, 11 parcels are designated MUD, while the two northern parcels are LDR and GC. The proposed general plan amendment, GPA2024-0003, seeks to change these two northern parcels to MUD and apply the MUD designation to the 0.87 acres of public rights-of-way. If approved, this would ensure uniform General Plan consistency and allow the site to be developed for commercial use as proposed by Precise Plan PP2024-0001. Per Table LU-1 of the Corona General Plan, the MUD designation supports retail commercial or mixed-use developments with a maximum FAR of 3.0; the Project has an FAR limit of 0.22, thus consistent with the MUD designation.

The Downtown Revitalization Specific Plan designates 11 parcels as D, and the two northern parcels as SF and GB. The specific plan amendment, SPA2024-0003, proposes zoning changes for the two northern parcels to D and extends this zoning over 0.87 acres of public rights-of-way (Fourth and Fifth Streets). If approved, this would align the entire site's D zoning with its proposed MUD land use designation.

The D zoning allows for commercial, office, dining, cultural, entertainment uses, and promotes a pedestrian-friendly environment with efficient access and attractive streetscapes. The project, therefore, aligns with the D zoning. Thus, with the approval of the general plan and specific plan amendments, the Project would comply with the proposed General Plan land use designation and zoning of MUD and D, respectively.

OTHER PUBLIC AGENCIES WHOSE APPROVAL IS REQUIRED

Issuance of building permits and completion of structures to the current building code are required by the city prior to the establishment of the Project. Additionally, approvals from the following agencies are required:

- Santa Ana Regional Water Quality Control Board (National Pollutant Discharge Elimination System Permit and Report of Waste Discharge)
- South Coast Air Quality Management District (Authority to Construct)

NATIVE AMERICAN CONSULTATION

Pursuant to SB 18 and AB 52, the city sent out letters to 37 Native American tribal individuals that could have knowledge regarding tribal cultural resources in the Project area. As discussed in Section 17, Tribal Cultural Resources (TCR), the Rincon Band of Luiseno Indians was the only tribe that expressed interest in archaeological monitoring, with monitoring report and protocols for discovery of cultural material and human remains. The Rincon Tribe was also in agreement with the suggested mitigation measures related to Tribal Cultural Resources (TCR), which resulted in the addition of mitigation measures included in the TCR section.

STAFF RECOMMENDATION:

The city's staff, having undertaken and completed an initial study of this project in accordance with the City's "Local Guidelines for Implementing the California Environmental Quality Act (CEQA)", has concluded and recommends the following:

- ___ The proposed project could not have a significant effect on the environment. **Therefore, a NEGATIVE DECLARATION will be prepared.**
- ___ The proposed project could have a significant effect on the environment; however, the potentially significant effects have been analyzed and mitigated to below a level of significance pursuant to a previous EIR as identified in the Environmental Checklist attached. **Therefore, a NEGATIVE DECLARATION WILL BE PREPARED.**
- X The Initial Study identified potentially significant effects on the environment but revisions in the project plans or proposals made by or agreed to by the applicant would avoid or mitigate the effects to below a level of significance. **Therefore, a MITIGATED NEGATIVE DECLARATION will be prepared.**
- ___ The proposed project may have a significant effect on the environment. **Therefore, an ENVIRONMENTAL IMPACT REPORT is required.**
- ___ The proposed project may have a significant effect on the environment, however, a previous EIR has addressed only a portion of the effects identified as described in the Environmental Checklist discussion. As there are potentially significant effects that have not been mitigated to below significant levels, a **FOCUSED EIR will be prepared to evaluate only these effects.**
- ___ There is no evidence that the proposed project will have the potential for adverse effect on fish and wildlife resources, as defined in Section 711.2 of the Fish and Game Code.

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED

The following indicates the areas of concern that have been identified as "Potentially Significant Impact" or for which mitigation measures are proposed to reduce the impact to less than significant.

- | | | |
|--|--|--|
| <input type="checkbox"/> Land Use Planning | <input type="checkbox"/> Hazards / Hazardous Materials | <input checked="" type="checkbox"/> Greenhouse Gases |
| <input type="checkbox"/> Population and Housing | <input type="checkbox"/> Noise | <input checked="" type="checkbox"/> Tribal Cultural Resources |
| <input type="checkbox"/> Geologic Problems | <input type="checkbox"/> Public Services | <input checked="" type="checkbox"/> Mandatory Findings of Significance |
| <input type="checkbox"/> Hydrology and Water Quality | <input type="checkbox"/> Utilities | <input type="checkbox"/> Wildfire |
| <input checked="" type="checkbox"/> Air Quality | <input type="checkbox"/> Aesthetics | <input type="checkbox"/> Energy |
| <input type="checkbox"/> Transportation / Traffic | <input checked="" type="checkbox"/> Cultural Resources | |
| <input checked="" type="checkbox"/> Biological Resources | <input type="checkbox"/> Agricultural Resources | |
| <input type="checkbox"/> Mineral Resources | | |

Date Prepared: October 20, 2025
 Contact Person: Rocio Lopez

Prepared By: Rocio Lopez, Senior Planner
 Phone: (951) 736-2293 / Email: rocio.lopez@coronaca.gov

AGENCY DISTRIBUTION

(check all that apply)

- ___ Responsible Agencies
- ___ Trustee Agencies (CDFG, SLC, CDPR, UC)
- X State Clearinghouse (CDFG, USFWS, Redev. Projects) (local 20-day circulation)
- X AQMD
- ___ Pechanga
- ___ Soboba
- ___ WQCB
- X Other: Rincon tribal representatives

UTILITY DISTRIBUTION

___ Southern California Edison

Southern California Edison
 Adriana Mendoza-Ramos, Esq.
 Region Manager, Local Public Affairs
 1351 E. Francis St.
 Ontario, CA 91761

Southern California Edison -Karen Cadavona
 Third Party Environmental Review
 2244 Walnut Grove Ave.
 Quad 4C 472A
 Rosemead, CA 91770

Figure 1: Regional Location

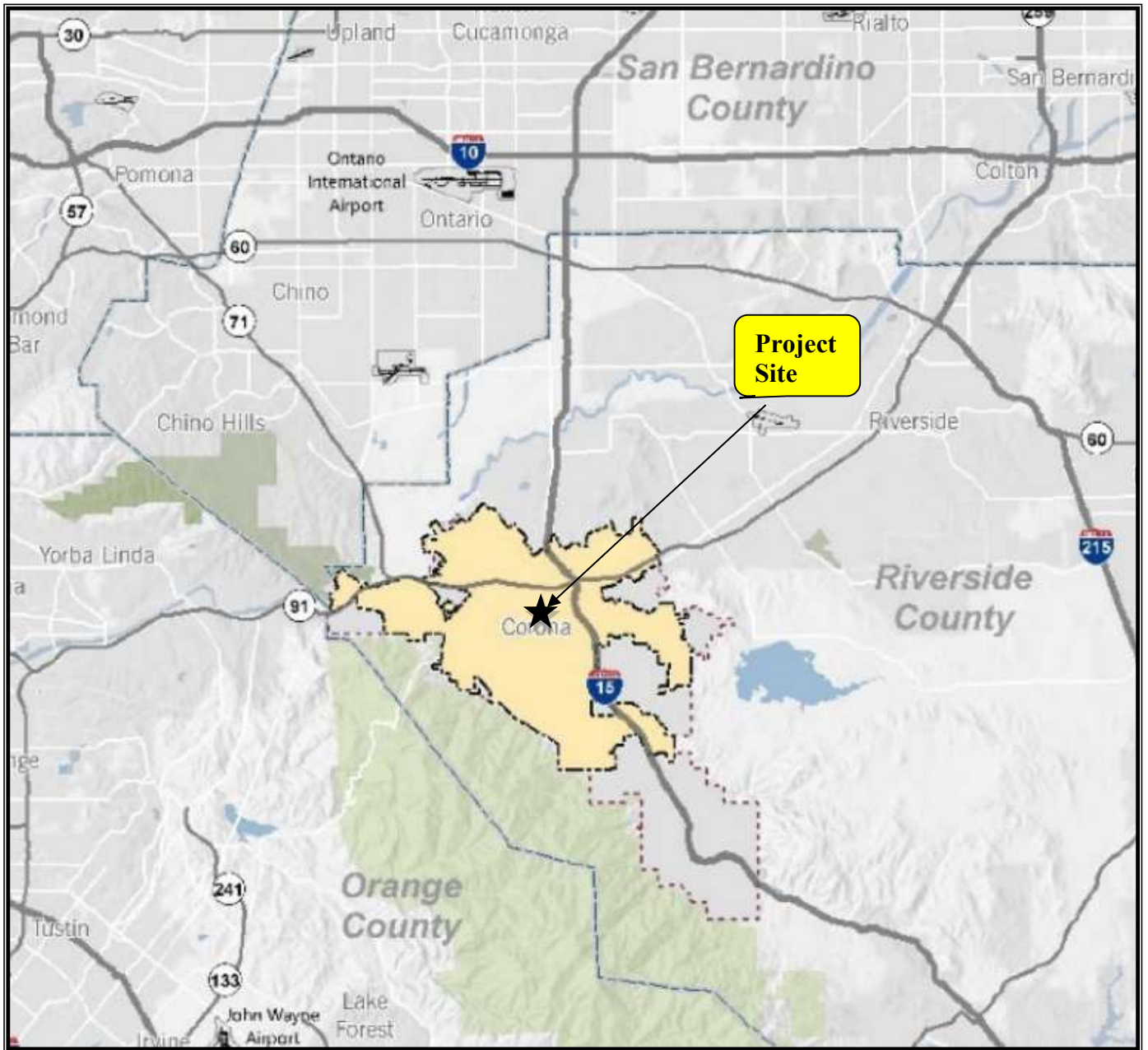
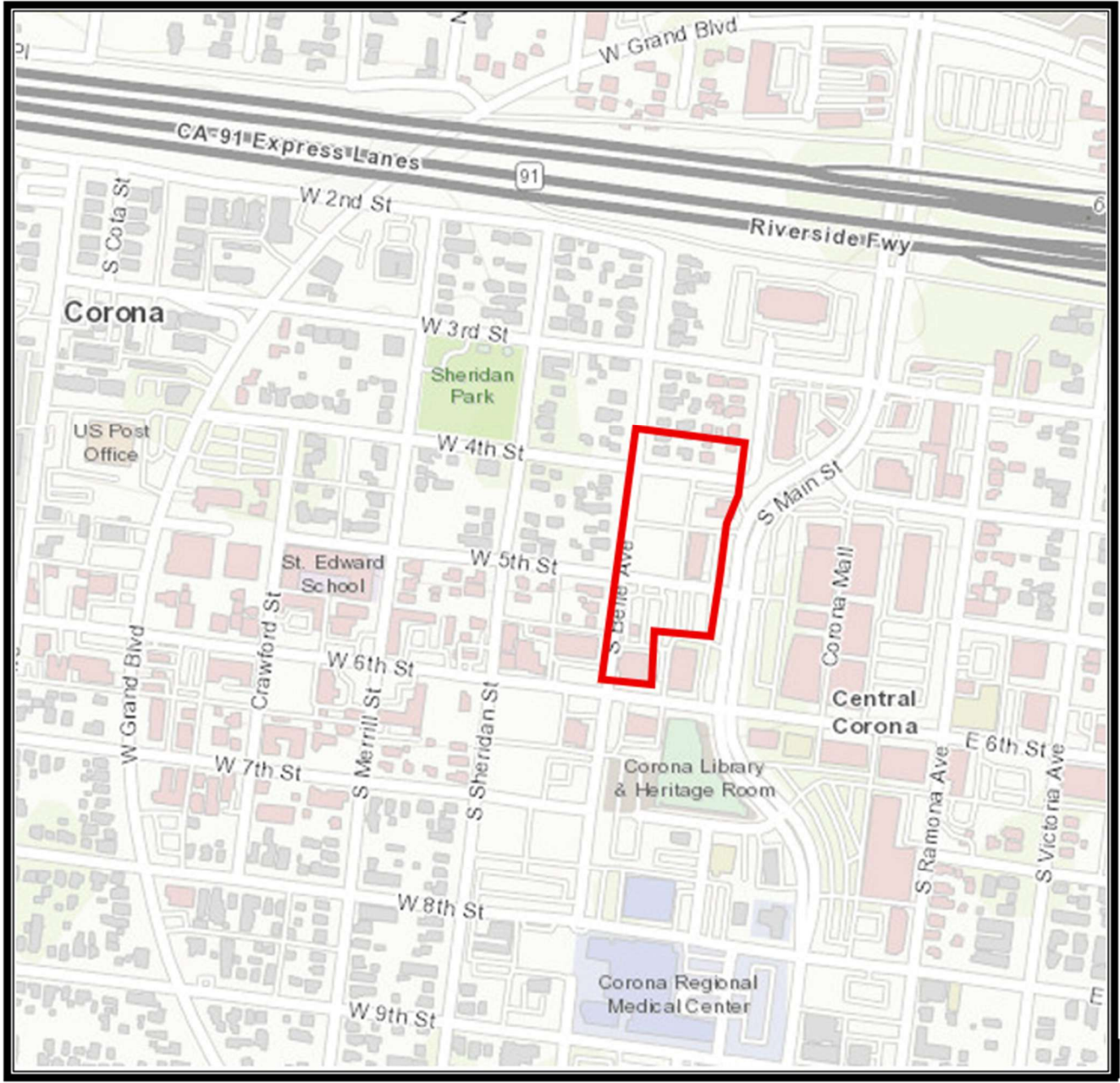


Figure 2: Local Vicinity

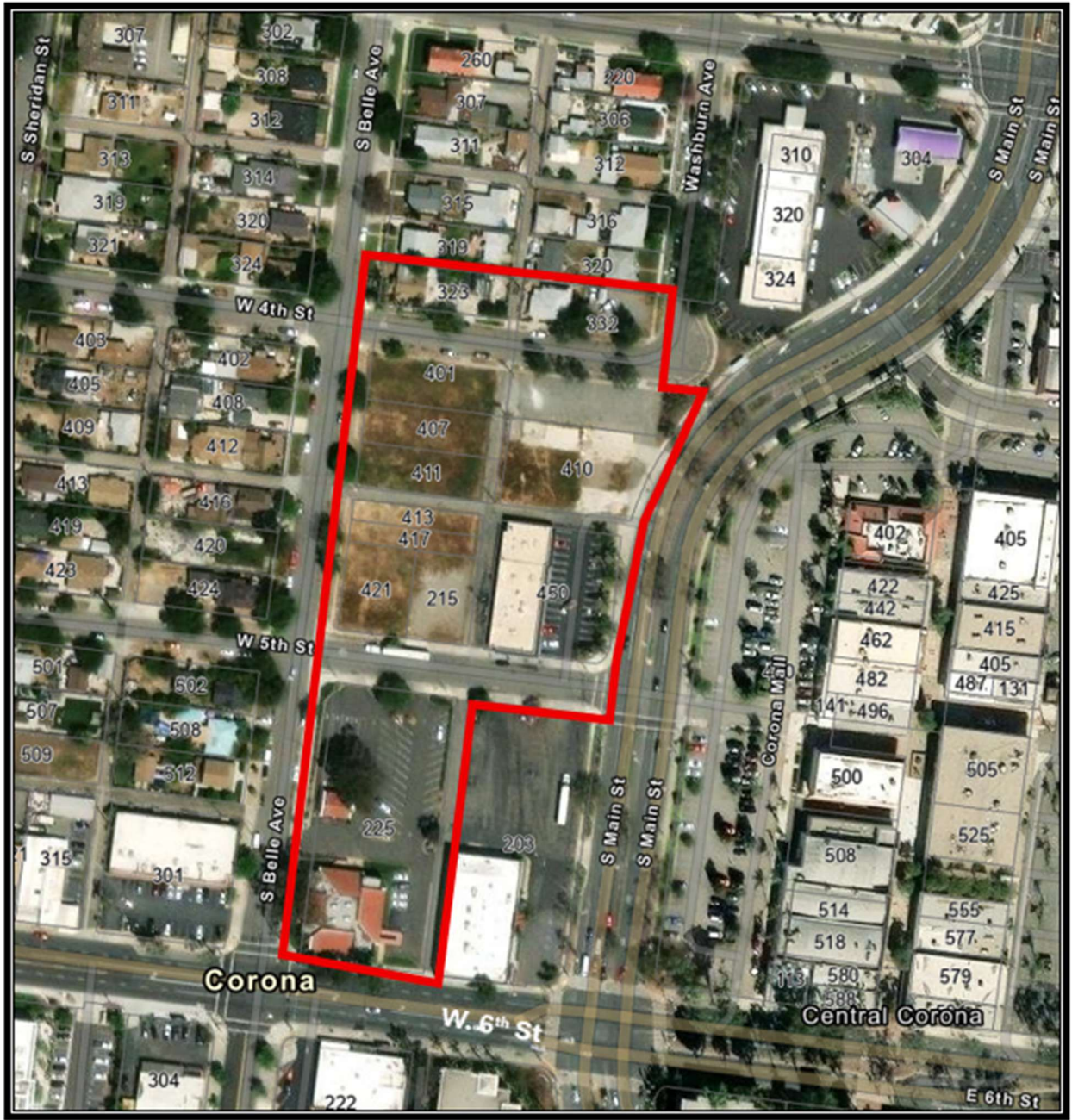


LEGEND

 Project Site



Figure 3: Aerial view

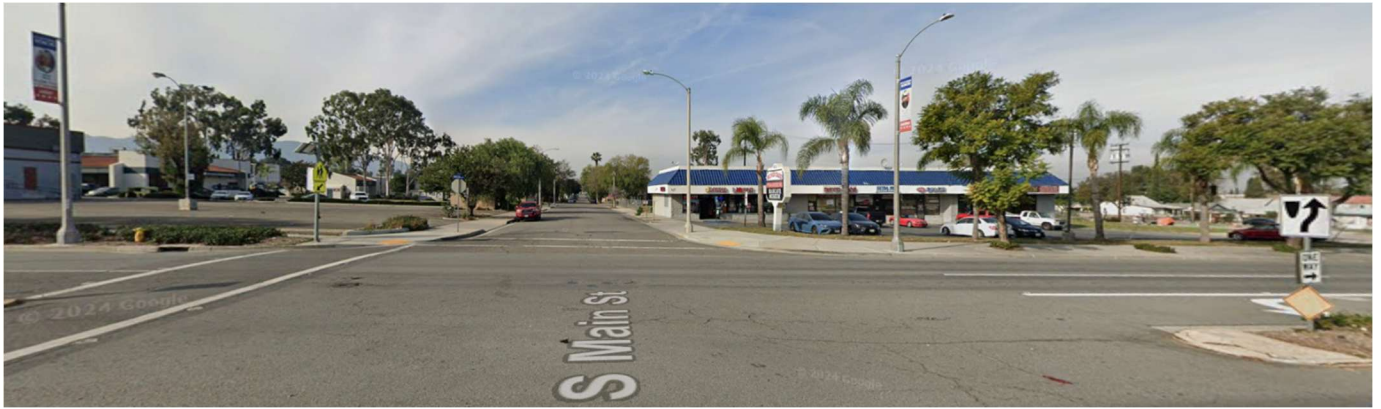


LEGEND

 Project Site



Figure 4a. Existing Site Photos



View Looking West from S. Main St. and W. 5th St.



View Looking Southwest from S. Main St.



View Looking Northeast from W. 6th St. and S. Belle Ave.



View Looking North on S. Belle Ave.

Figure 4b: Existing Site Photos



View Looking Southeast from S. Belle Ave. & W. 5th St.



View Looking Northeast from S. Belle Ave. & W. 5th St.



View Looking Southeast from S. Belle Ave. & W. 4th St.

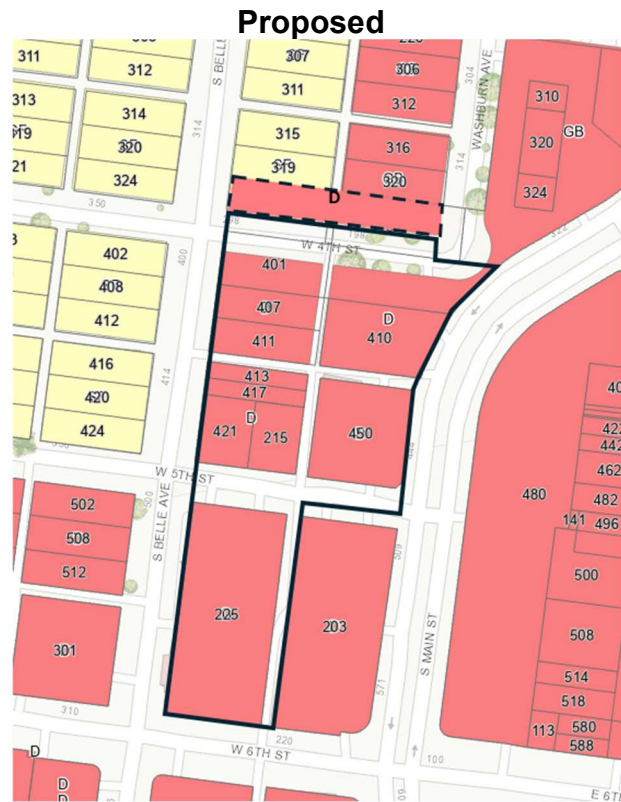


View Looking South from S. Washburn Ave.

Figure 5: Existing & Proposed General Plan Land Use Designations



Figure 6: Existing & Proposed Zoning



- D - DOWNTOWN
 - SF - SINGLE FAMILY
- GB - GATEWAY BUSINESS
 - SUBJECT SITE FOR SPA
 - PROJECT BOUNDARY

Figure 7: Parcel Map 38981

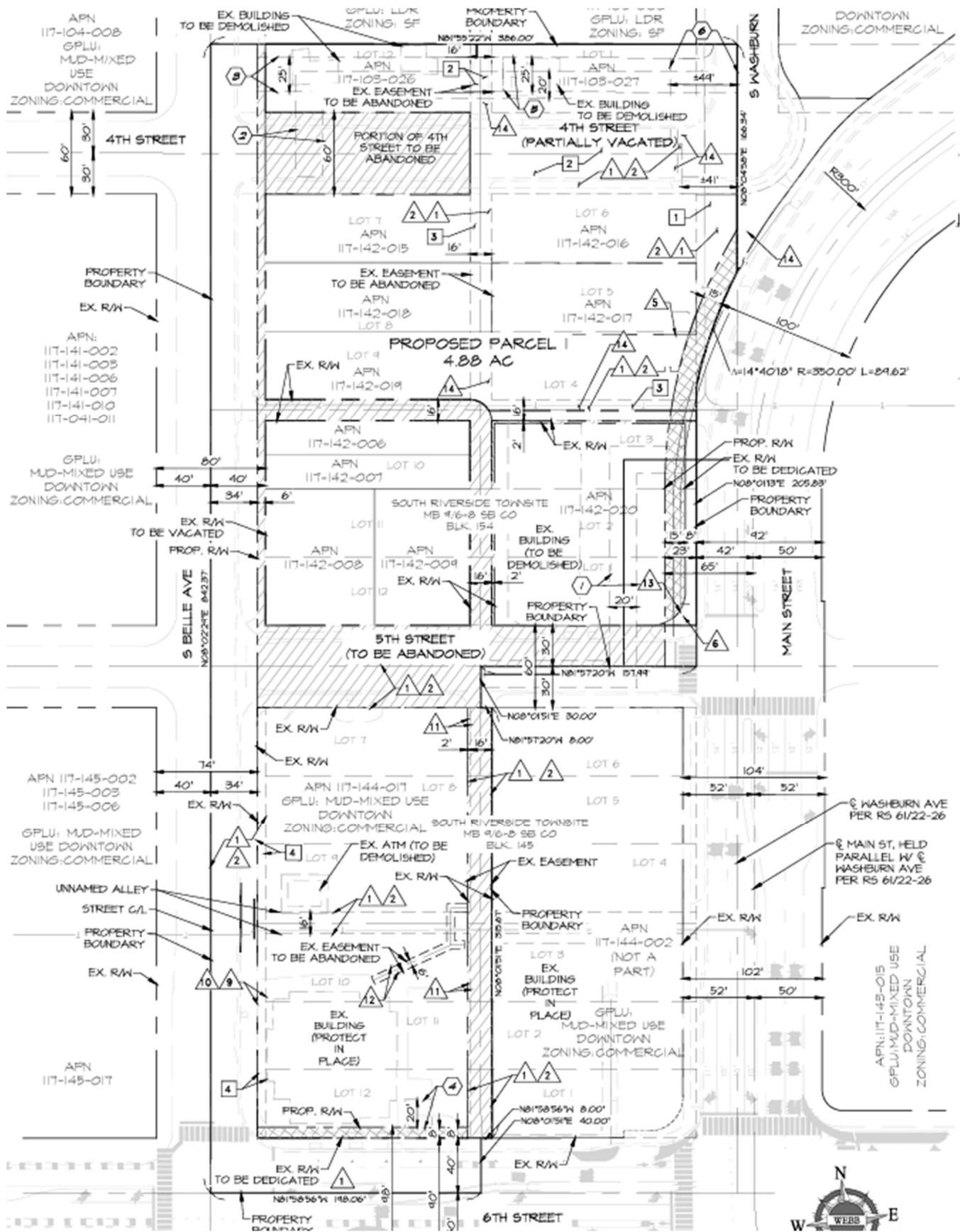


Figure 8: Architectural Site Plan

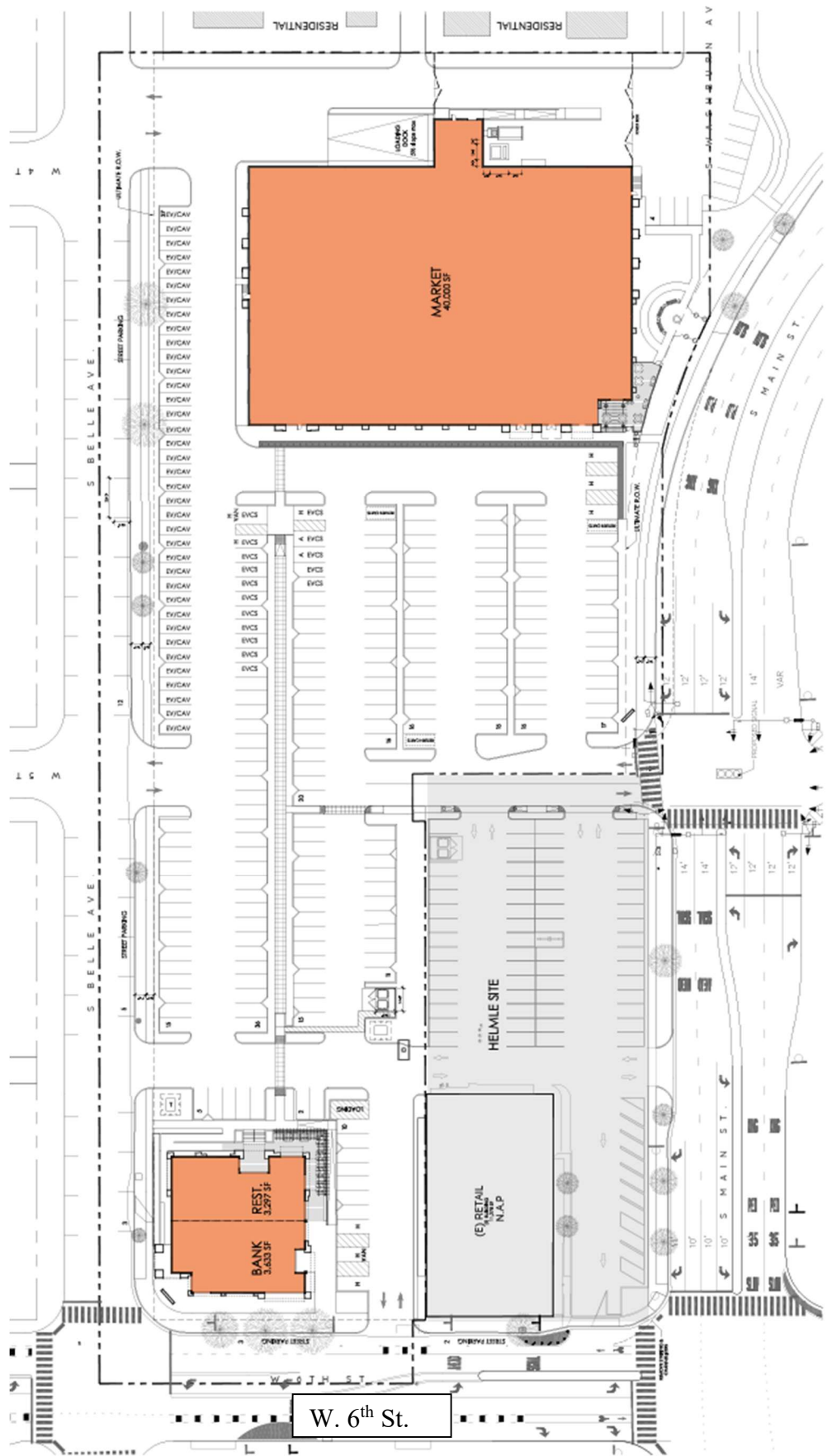
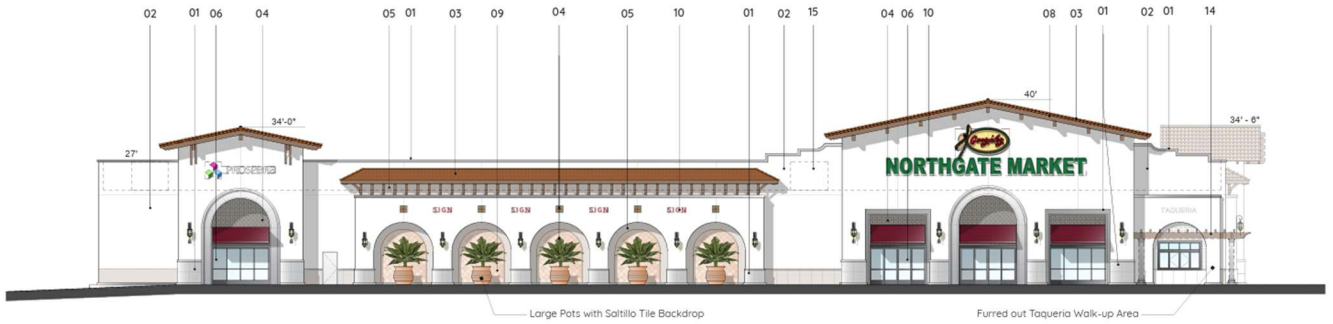


Figure 9: Elevations - Market

LEGEND

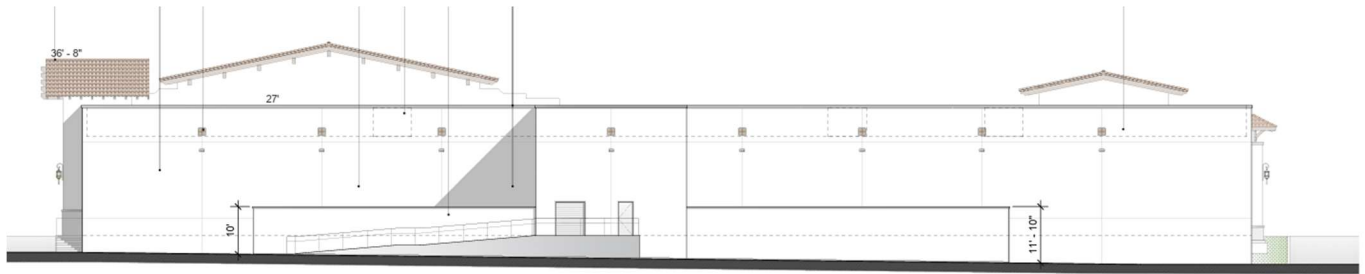
- | | | |
|-------------------------------|----------------------------|---|
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| 02 THREE COAT PLASTER - WHITE | 07 METAL FINISH | 12 LOADING DOCK ENCLOSURE |
| 03 SPANISH ROOF TILE | 08 COMPOSITE WOOD | 13 B.O.H. ENCLOSURE |
| 04 SPANISH CERAMIC TILE | 09 SALTILLO CONCRETE TILES | 14 DARK WOODEN TRELLIS |
| 05 THREE COAT PLASTER - GRAY | 10 SIGNAGE | 15 MECHANICAL EQUIPMENT |



SOUTH ELEVATION



EAST ELEVATION



NORTH ELEVATION

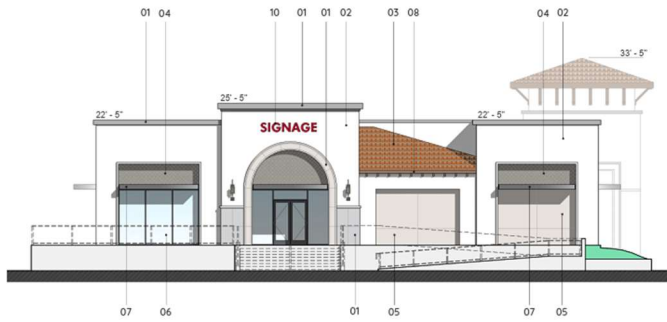


WEST ELEVATION

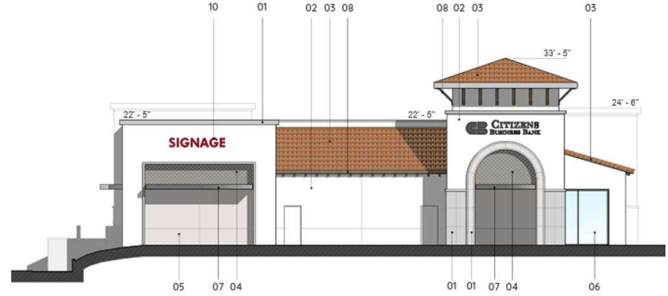
Figure 9a: Elevations – Bank/Restaurant Building

LEGEND

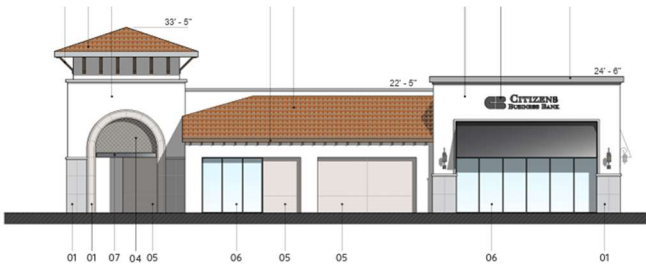
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| 01 CONCRETE FINISH | 06 STOREFRONT GLAZING | 11 WROUGHT IRON PATIO SEATING ENCLOSURE |
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| 04 SPANISH CERAMIC TILE | 09 SALTILLO CONCRETE TILES | 14 DARK WOODEN TRELLIS |
| 05 THREE COAT PLASTER - GRAY | 10 SIGNAGE | 15 MECHANICAL EQUIPMENT |



NORTH ELEVATION



WEST ELEVATION



SOUTH ELEVATION



EAST ELEVATION

Figure 10: Landscape Plan



Figure 11: Conceptual Grading

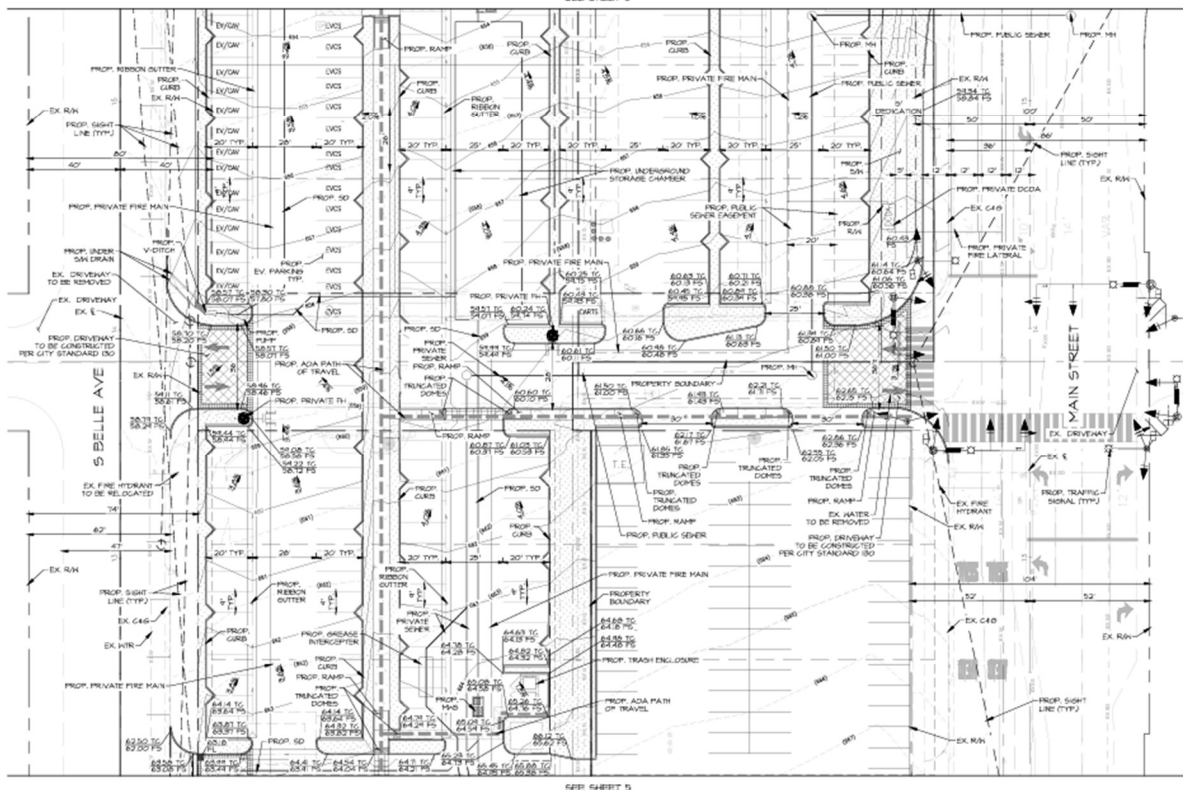
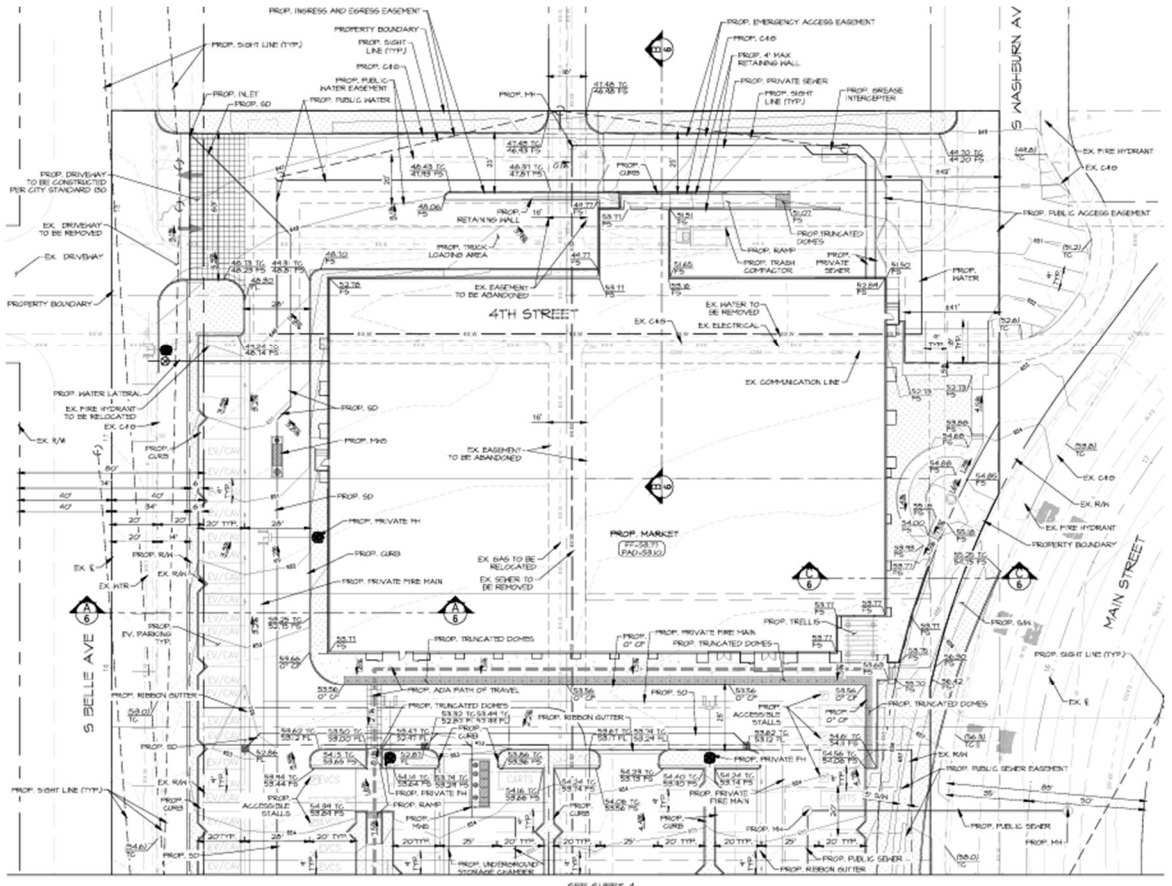


Figure 11a: Conceptual Grading

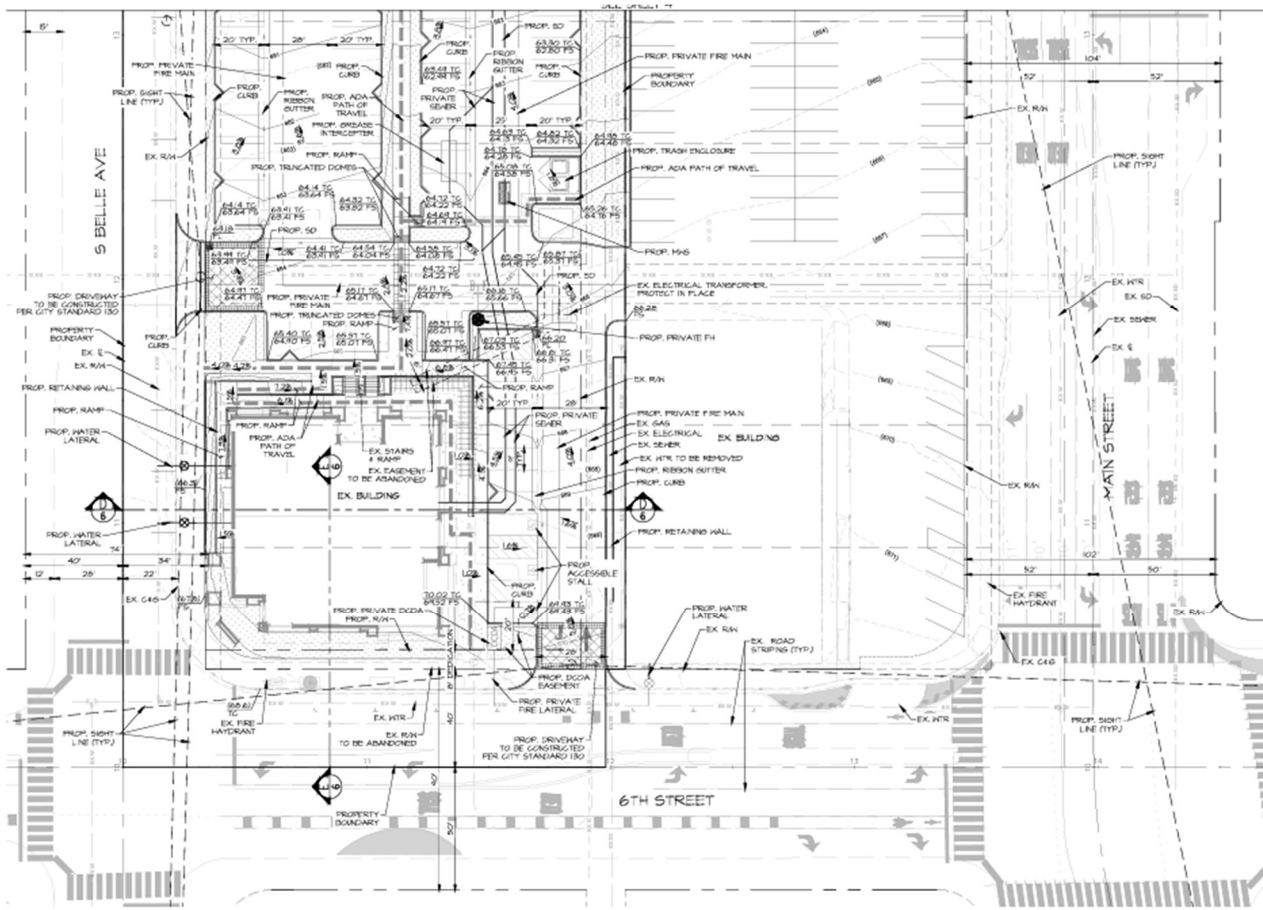
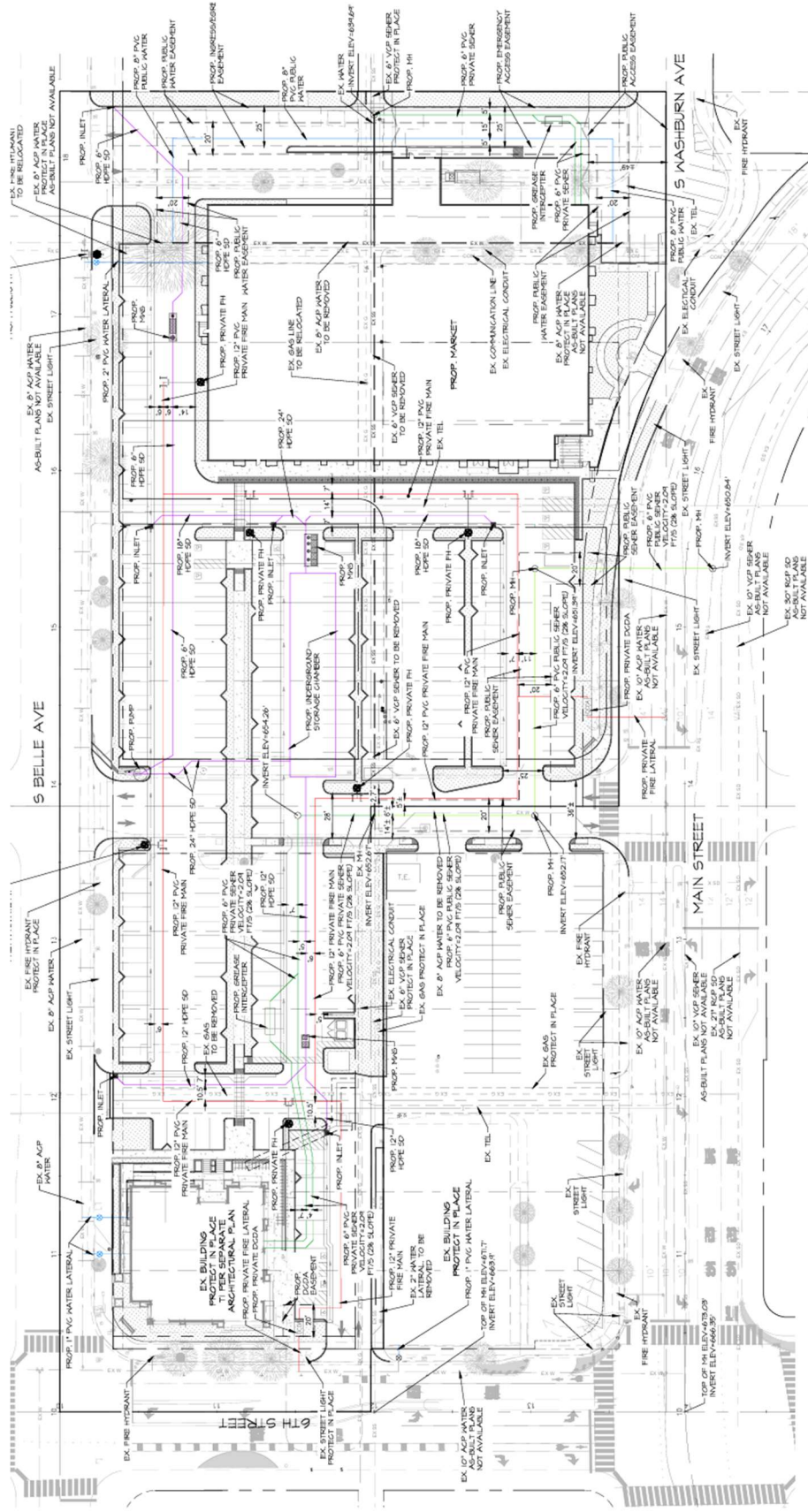


Figure 12: Utilities Plan



Note: This form represents an abbreviation of the complete Environmental Checklist found in the City of Corona CEQA Guidelines. Sources of reference information used to produce this checklist may be found in the City of Corona Planning and Development Department, 400 S. Vicentia Avenue, Corona, CA.

1. LAND USE AND PLANNING:

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
a. Conflict with any land use plan/policy or agency regulation (general plan, specific plan, zoning)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Conflict with surrounding land uses	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Physically divide established community	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

a. Conflict with any land use plan/policy, or agency regulation (general plan, specific plan, zoning)

Less Than Significant Impact. The Project site is generally located within the northwest corner area of W. Sixth Street and S. Main Street. The Project proposes to develop multiple vacant and underutilized parcels into a 40,000 square foot Northgate Gonzalez Market and remodel an existing 6,930 square foot vacant Citizens bank building into a bank and restaurant use on an overall 4.88-net acre site comprised of 13 parcels.

The Project site includes 13 parcels and 0.87 acres of public rights-of-way (Fourth and Fifth Streets), totaling 5.68 gross acres. According to the Corona General Plan, 11 parcels are designated MUD, while the two northern parcels are LDR and GC. The proposed general plan amendment, GPA2024-0003, seeks to change these two northern parcels to MUD and apply the MUD designation to the 0.87 acres of public rights-of-way. If approved, this would ensure uniform General Plan consistency and allow the site to be developed for commercial use as proposed by Precise Plan PP2024-0001.

Per Table LU-1 of the Corona General Plan, the MUD designation supports retail commercial or mixed-use developments with a maximum FAR of 3.0; the Project has an FAR limit of 0.22, thus consistent with the MUD designation.

The Downtown Revitalization Specific Plan designates 11 parcels as D, and the two northern parcels as SF and GB. The specific plan amendment, SPA2024-0003, proposes zoning changes for the two northern parcels to D and extends this zoning over 0.87 acres of public rights-of-way (Fourth and Fifth Streets). If approved, this would align the entire site's zoning with its proposed MUD designation.

The D zoning allows for commercial, office, dining, cultural, entertainment uses, and promotes a pedestrian-friendly environment with efficient access and attractive streetscapes. The project, therefore, aligns with the D zoning.

Thus, with the approval of the general plan and specific plan amendments, the project would comply with relevant land use plans, policies, and regulations, and no mitigation is required.

b. Conflict with surrounding land uses.

Less Than Significant Impact. The Project site is located in within an urbanized, dense, downtown district within the Downtown Revitalization Specific Plan. It is surrounded primarily by existing commercial uses along S. Main St. and W. Sixth St. in the heart of downtown Corona. To the southwest of the Project site is an existing strip commercial center and to the west is S. Belle Ave. with a mixture of multiple family and single family residential uses. To the north of the Project site are residential uses and to the northeast there are commercial uses. The Project site is centrally located to accommodate the proposed Project as it is within walking distance of the surrounding residential community and within a mile of the SR-91 freeway located to the north of the Project site. The proposed Project is consistent with the General Plan goals and policies for the downtown area, which include policies:

LU-17.3: Promote innovative redevelopment activities as feasible and supporting programs for business retention and proactive recruitment of new businesses to stimulate the revitalization of the Downtown; and

LU-17.6: Promote and support the redevelopment of the Corona Mall and adjacent properties into a central attraction within the Downtown Core, with a mix of supporting land uses.

The Project is consistent with previous commercial land uses formerly developed on the Project site. Furthermore, the Project site is within the appropriate General Plan land use designation of MUD and consistent with the uses permitted within the Downtown zoning of the Downtown Revitalization Specific Plan. Therefore, development of the Project, as designed and conditioned per companion application PP2024-0001, would not conflict with the surrounding land uses; therefore, mitigation is not warranted.

c. Physically divide established community.

Less Than Significant Impact. The Project site contains a vacant Citizens bank building and strip commercial center, with the remaining area largely vacant with only remnants of earlier development (concrete pads) still remaining. The Project site is located within an urban and developed area within the City of Corona and is located along two major streets, S. Main Street and W. Sixth Street, which are primarily surrounded by commercial land uses. While there are surrounding residential land uses to the west of S. Belle Avenue and to the north of the Project site, the Phase I Environmental Site Assessment (ESA) indicates that previous land uses on the Project site consisted of a machine shop, roofing and metalworking companies. More recently, there was a carpet shop and drapery and upholstery shop in addition to other general commercial land uses.

The proposed Project would introduce an anchor market with renovation of an existing bank into a bank and sit down restaurant use, which would be similar in nature to previous commercial uses and nearby commercial land uses. The project would utilize existing roadways, resulting in no change in roadway patterns. No separation of uses or disruption of access between land use types would occur as a result of the Project. The Project is designed to help mitigate any potential conflicts with adjacent residential uses by installing six-foot high screen walls along the entire northern property line and installing a 12-foot high screen wall within the truck loading area. For this reason, the Project would not physically divide an established community and would have a less than significant impact. Therefore, mitigation is not required.

2. POPULATION AND HOUSING:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
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a. Induce substantial growth	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Displace substantial numbers of existing housing or people	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

a. Induce substantial growth

Less Than Significant Impact. The Project proposes the development of a new 40,000 square foot market and remodel of an existing bank building. It does not propose construction of any residential uses, nor does it include extension of existing infrastructure. The Project would create employment opportunities (both during the construction and operational phases) and it is anticipated that employees from the local workforce would be hired during both the construction and operational phases of the project. Furthermore, the City of Corona’s 2020-2040 General Plan, accounts for an additional 13,423 employees within city limits as identified in Table LU-2, Corona Buildout Summary. Based on the Project’s 4.88 acre area, only a small fraction (1.3%) of the overall additional city employees is anticipated. Furthermore, the buildout anticipated under the General Plan is a function of the acreage of available land and the amount of development allowed on a property. In this case the Project’s proposed FAR of 0.22 is well below the maximum 3.0 FAR limit within the Downtown Revitalization Specific Plan.

Additionally, the proposed Project is located in an urbanized area of the city that is already served by existing roadways and infrastructure systems. No infrastructure would need to be extended to serve areas beyond the Project site, and indirect impacts related to growth would not occur from implementation of the proposed Project. The Project consists of development that would generate less than a 1.3 percent increase in the overall number of projected employees citywide and is well below the maximum allowable FAR. Therefore, potential impacts related to inducement of unplanned population growth, either directly or indirectly, would be less than significant. No mitigation is warranted.

b. Displace substantial numbers of existing housing or people

No Impact. The Project site is partially vacant, occupied with a strip retail building (to be demolished) and bank building that will be renovated into multitenant commercial use. Two former single family homes, located on the north side of W. Fourth Street within the boundary, were demolished within the last two years. In addition, one of the parcels (332 S. Washburn Ave.) has a Gateway Business (GB) zoning with a current land use designation of General Commercial (GC).

Therefore, the project would not displace housing or residents, and the project would not necessitate the construction of replacement housing elsewhere. Therefore, the Project would result in no impact related to displacement and replacement housing.

3. GEOLOGIC PROBLEMS:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
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a. Fault /seismic failures (Alquist-Priolo zone) /Landslide/Liquefaction	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Grading of more than 100 cubic yards	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Grading in areas over 10% slope	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Substantial erosion or loss of topsoil	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Unstable soil conditions from grading	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f. Expansive soils	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

The following section is based on the Geotechnical Engineering Investigation Report prepared by SALEM Engineering Group on December 20, 2022, and revised on April 3, 2024 (Appendix A).

a. Fault/seismic failures (Alquist-Priolo zone) /Landslide/Liquefaction

Less Than Significant Impact.

Fault/seismic failures (Alquist-Priolo zone)

The Project site is not located within a designated Alquist-Priolo Earthquake Fault Zone, and does not lie within the current State of California Earthquake Fault Zone for surface fault rupture hazards. No active faults with the potential for surface fault rupture are known to pass directly beneath the site. Therefore, the potential for surface rupture due to faulting occurring beneath the site during the design life of the proposed development is considered low. The closest known active fault to the site is the Chino Fault, located approximately 2.3 miles from the site. Other faults in the region include the Whittier-Elsinore fault zone located approximately 3.6 miles away, Puente Hills fault located 17.3 miles away, and the Cucamonga fault located approximately 19 miles away.

The historical seismicity of the site entails numerous small to medium magnitude earthquake events occurring in the region around the subject site. Any future developments at the subject site should anticipate that moderate to large seismic events could occur very near the site.

Structures built in the City of Corona are required to be built in compliance with the California Building Code (CBC), which regulates all building and construction projects within the city and implements a minimum standard for building design and construction that includes specific requirements for seismic safety, excavation, foundations, retaining walls, and site demolition. Compliance with the CBC would include the incorporation of 1) seismic safety features to minimize the potential

for significant effects as a result of earthquakes; 2) proper building footings and foundations; and 3) construction of the building structures so that it would withstand the effects of strong ground shaking. Moreover, consistent with the CBC, the Project is required to implement recommendations from the Geotechnical Engineering Investigation Report, which includes recommendations related to earthwork and the design and construction of foundations, floor slabs, pavements, and infiltration systems. Because the proposed Project would be constructed in compliance with the CBC, the proposed Project would result in a less than significant impact related to strong seismic ground shaking. Therefore, no mitigation is warranted.

Landslides

Landslides and other slope failures are secondary seismic effects that occur during or soon after earthquakes. Areas that are most susceptible to earthquakes induced landslides are steep slopes underlain by loose, weak soils, and areas on or adjacent to existing landslide deposits.

The project site is relatively flat. Furthermore, according to the City of Corona General Plan EIR Deep-seated Landslide Hazard Map, the Project area is not identified as a highly susceptible landslide hazard area. Additionally, the Geotechnical Engineering Investigation Report determined that hazards from slippage or landslide from proposed construction of the Project is unlikely (SALEM Engineering Group). Therefore, the Project would not cause potential substantial adverse effects related to slope instability or seismically induced landslides and impacts would be less than significant. As such, no mitigation is warranted.

Liquefaction

Soil liquefaction is a state of soil particles suspension caused by a complete loss of strength when the effective stress drops to zero. Liquefaction normally occurs under saturated conditions in soils such as sand in which the strength is purely frictional. Primary factors that trigger liquefaction are: moderate to strong ground shaking (seismic source), relatively clean, loose granular soils (primarily poorly graded sands and silty sands), and saturated soil conditions (shallow groundwater). Due to the increasing overburden pressure with depth, liquefaction of granular soils is generally limited to the upper 50 feet of a soil profile. However, liquefaction has occurred in soils other than clean sand.

The soils encountered within the depth of 21½ feet on the project site consisted predominately of loose to very dense silty sand with various amounts of gravel, gravelly silty sand and sandy gravel; and firm to hard sandy silt with various amounts of gravel. The historically highest groundwater is estimated to be at a depth greater than 50 feet below ground surface according to the regional groundwater data. In accordance with the Riverside County Office of Information Technology GIS, the site is located within a low liquefaction potential zone. Based on the depth to groundwater, the liquefaction potential of the site is considered to be low and mitigation measures are not warranted (SALEM Engineering Group).

With compliance with existing regulations and the Project location, impacts related to seismically related ground failure and liquefaction would be less than significant. Therefore, no mitigation is warranted.

b. Grading of more than 100 cubic yards

Less Than Significant Impact. Construction of the proposed Project would consist of a cut volume of 6,900 cubic yards (CY) and a fill volume of 1,600 CY, thus resulting in a net export volume of 5,300 CY. As such, the Project would result in grading of more than 100 CY. However, the Project would be required to be built in compliance with the California Building Code (CBC), which regulates all building and construction projects within the city and implements a minimum standard for building design and construction that includes specific requirements for seismic safety, excavation, foundations, retaining walls, and site demolition. Further, impacts associated with grading have been analyzed throughout this Mitigated Negative Declaration (MND) in Section 5, Air Quality, and Section 16, Greenhouse Gases, both of which were determined to have less than significant impacts. As such, impacts related to grading would be less than significant therefore, no mitigation is warranted.

c. Grading in areas over 10% slope

No Impact. Based on its topography, the proposed Project would not include grading of any areas with slopes over 10 percent. Project grading would be required to comply with the California Building Code (CBC), which regulates all building and construction projects within the city and implements a minimum standard for building design and construction that includes specific requirements for seismic safety, excavation, foundations, and retaining walls. Additionally, the Project would incorporate construction best management practices (BMPs) through adherence to CBC grading and site preparation recommendations included in the Geotechnical Investigation such as removal of undesirable and/or unstable soils to be recompacted to decrease the likelihood of settlement after construction. Further, impacts associated with grading have been analyzed throughout this MND in Section 5, Air Quality, and Section 16, Greenhouse Gases, both of which would result in less than significant impacts. As such, impacts related to grading would be less than significant and no mitigation is warranted.

d. Substantial erosion or loss of topsoil

Less Than Significant Impact. Construction of the proposed Project has the potential to contribute to soil erosion and the loss of topsoil. Excavations and grading activities that would be required for the Project would expose and loosen topsoil, which could be eroded by wind or water.

Chapter 15.36.290 of the City's Municipal Code implements the requirements of the Santa Ana Regional Water Quality Control Board (RWQCB) National Pollutant Discharge Elimination System (NPDES) Storm Water Permit Regional Board Order No. R8-2010-0033, as amended, (MS4 Permit) and establishes minimum stormwater management requirements and controls that are required to be implemented for construction and grading activities for the Project.

To reduce the potential for soil erosion and the loss of topsoil, a Stormwater Pollution Prevention Plan (SWPPP) is required by City and RWQCB regulations to be developed by a QSD (Qualified SWPPP Developer), which would be implemented as listed within Section 4, Hydrology and Water Quality, of this report. The SWPPP is required to address site-specific conditions related to specific grading and construction activities that could cause erosion and the loss of topsoil and provide erosion control BMPs to reduce or eliminate the erosion and loss of topsoil. Erosion control BMPs include use of silt fencing, fiber rolls, or gravel bags, stabilized construction entrance/exit, hydroseeding, etc. With compliance with the City's Municipal Code stormwater management requirements, RWQCB SWPPP requirements, and installation of BMPs, which would be implemented by the City's Project review by the City of Corona's Planning and Development Department, Development Services Division, construction impacts related to erosion and loss of topsoil would be less than significant. Therefore, no mitigation is warranted.

e. Unstable soil conditions from grading

Less Than Significant Impact. Unstable soil conditions have the potential to result in hazards such as landslides, lateral spreading, subsidence, and liquefaction or collapse. Landslides are the downhill movement of masses of earth and rock and are often associated with earthquakes; but other factors, such as the slope, moisture content of the soil, composition of the subsurface geology, heavy rains, and improper grading can influence the occurrence of landslides. As discussed previously, implementation of the Project and associated grading are unlikely to result in hazards such as landslides. Additionally, the Project site and surrounding area are fully developed and do not have natural or manufactured slopes. Accordingly, the Project would not be located on a geologic unit or soil that is unstable and that would result in on- or off-site landslides, therefore no significant impacts would occur.

Lateral spreading is a phenomenon in which large blocks of intact, non-liquefied soil move downslope on a liquefied soil layer. Lateral spreading is a regional event. For lateral spreading to occur, the liquefiable soil zone must be laterally continuous, unconstrained laterally, and free to move along the sloping ground. The Project site's potential for lateral spreading is considered low due to the site's relatively flat topography, distance from slopes, and "very low" potential for liquefaction. Thus, the Project would not be located on a geologic unit or soil that would result in lateral spreading, and no significant impacts would occur. Therefore, no mitigation is warranted.

Subsidence is a general lowering of the ground surface over a large area that is generally attributed to lowering of the ground water levels within a groundwater basin. Localized or focal subsidence or settlement of the ground can occur as a result of an earthquake motion in an area where groundwater in basin is lowered. The test boring locations were checked for the presence of groundwater during and after the drilling operation, and free groundwater was not encountered during this investigation. The historically highest groundwater within the site vicinity is estimated to be at a depth greater than 50 feet below ground surface according to regional groundwater well data.

It should be recognized that water table elevations may fluctuate with time, being dependent upon seasonal precipitation, irrigation, land use, localized pumping, and climatic conditions as well as other factors. Therefore, water level observations at the time of the field investigation may vary from those encountered during the construction phase of the project. However, recommendations of the Geotechnical Engineering Investigation Report would be implemented during grading and construction and the Project would be required to comply with the CBC and the City's Municipal Code, which would be verified through the City's plan check and permitting process.

Thus, with compliance with existing regulations and implementation of best management practices (BMPs) impacts related to unstable soil conditions from grading, including landslides, lateral spreading, subsidence, liquefaction or collapse would be less than significant. Therefore, no mitigation is warranted.

f. Expansive soils

Less Than Significant Impact. Expansive soils contain certain types of clay minerals that shrink or swell as the moisture content changes; the shrinking or swelling can shift, crack, or break structures built on such soils. Arid or semiarid areas with seasonal changes of soil moisture experience, such as southern California, have a higher potential of expansive soils than areas with higher rainfall and more constant soil moisture.

The Geotechnical Engineering Report determined that the site soil consisted of silty sand based on test borings (Salem 2022). Therefore, the Project site has low potential for expansive soil. Additionally, the Project would require compliance with the CBC requirements, as implemented by the Corona Municipal Code and verified through the City's plan check and permitting process. Thus, impacts related to expansive soils would be less than significant.

4. HYDROLOGY AND WATER QUALITY:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than significant Impact	No Impact
a. Violate water quality standards/waste discharge requirements	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Deplete groundwater supplies	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Alter existing drainage pattern	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Increase flooding hazard	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Degrade surface or ground water quality	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f. Within 100-year flood hazard area	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g. Increase exposure to flooding	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
h. Exceed capacity of storm water drainage system	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

The following section is based on the Project Specific Water Quality Management Plan (WQMP), prepared by Webb Associates, October 9, 2025 (Revised) (Appendix B); the Preliminary Drainage Study, prepared by Webb Associates, October 2025 (Revised) (Appendix C); and the Water and Sewer Study Report, prepared by Webb Associates, October 2025 (Revised) (Appendix D).

a. Violate water quality standards/waste discharge requirements

Less than Significant Impact.

Temporary Construction-Related Activities

Construction of the Project would require grading and excavation of soils, which would loosen sediment and then have the potential to mix with surface water runoff and degrade water quality. Additionally, construction would involve paving, utility installation, building construction, and landscaping activities. Construction activities would result in the generation of potential water quality pollution such as silt, debris, chemicals, paints, solvents, and other chemicals with the potential to adversely affect water quality. As such, short-term water quality impacts have the potential to occur during construction of the Project in the absence of any protective or avoidance measures.

These types of water quality impacts during construction of the Project would be prevented through implementation of a SWPPP. Construction of the Project would disturb more than one acre of soil; therefore, the proposed Project would be required to obtain coverage under the NPDES General Permit for Discharges of Storm Water Associated with Construction activity. Construction activity subject to this permit includes clearing, grading, and ground disturbances such as trenching, stockpiling, or excavation. The Construction General Permit requires implementation of a SWPPP that is required to identify all potential sources of pollution that are reasonably expected to affect the quality of storm water discharges from the

construction site. The SWPPP would generally contain a site map showing the construction perimeter, proposed buildings, stormwater collection and discharge points, general pre- and post-construction topography, drainage patterns across the site, and adjacent roadways. The SWPPP would also include construction BMPs which would reduce erosion or siltation. Typical BMPs for erosion or siltation, include use of silt fencing, fiber rolls, gravel bags, stabilized construction driveway, and stockpile management.

Adherence to the existing requirements and implementation of the appropriate BMPs, as ensured through the City's plan check and permitting process, would ensure that the Project would not violate any water quality standards or waste discharge requirements, potential water quality degradation associated with construction activities would be minimized, and impacts would be less than significant. Therefore, mitigation is not warranted.

Operation

The Project proposes a commercial development to include a 40,000 square foot market building, asphalt drive aisles and parking stalls, landscaped areas, internal concrete walkways and improvements to the existing bank building on the southwest corner of the site. The Project also includes public right-of-way improvements such as sidewalks, curb and gutter, and utility and stormwater improvements. The development would include landscaping, parking and potential pollutants associated with the proposed uses include various chemicals from bacterial indicators, metals, nutrients from fertilizer, pesticides, sediments, trash and debris, and oil and grease from vehicles. If these pollutants discharge into surface waters, it could result in degradation of water quality. However, the proposed Project would be required to incorporate a Water Quality Management Plan (WQMP) with post-construction (or permanent) Low Impact Development (LID) site design, source control, and treatment control BMPs. The LID site design would minimize impervious surfaces and provide infiltration of runoff into landscaped areas.

Section 13.27.120 of the City's Municipal Code requires implementation of a Water Quality Management Plan (WQMP) based on the anticipated pollutants that could result from new development and redevelopment projects. The Project's WQMP was created to comply with the requirements of the City of Corona, the Riverside County Water Quality Management Plan, and the NPDES Areawide Stormwater Program. The BMPs would include pollutant source control features and pollutant treatment control features. The source control BMPs would minimize the introduction of pollutants that may result in water quality impacts; and treatment control BMPs that would treat stormwater runoff. For the purposes of stormwater quality, an underground bioretention/biofiltration system is proposed. The Project site is split into several drainage management areas.

Runoff generated by the development of the Project is proposed to be treated for water quality requirements by modular wetland system (MWS) units located throughout the site before being conveyed to an underground chamber system for increased runoff mitigation. A 4'x8', 4'x17', and an 8'x20' MWS unit are proposed onsite. A pump is proposed to limit the flows to Belle Avenue, following the existing drainage pattern.

Water quality treatment of runoff produced by the northeastern 0.2 acres of the project site (northeast area) discharging to Washburn Avenue is addressed by LID principles to achieve treatment to the maximum extent practicable. The landscaped areas are self-treating within the northeast area. Therefore, the overall site meets water quality treatment requirements. With implementation of NPDES requirements and the WQMP, pursuant to the City Municipal Code, which would be verified during the plan check and permitting process for the proposed Project, the proposed Project would not violate any water quality standards or waste discharge requirements, and impacts would be less than significant. Therefore, mitigation is not warranted.

b. Deplete groundwater supplies

Less Than Significant Impact. No potable groundwater wells are proposed as part of the Project. The Project would be served with potable water by the City of Corona Department of Water and Power (DWP). The city has a diverse water supply portfolio including imported water from Western Municipal Water District (WMWD), groundwater from two local groundwater basins (Temescal Basin and Bedford-Coldwater Basin), and reclaimed water for landscape irrigation and other non-potable uses (City of Corona, Urban Water Management Plan, 2021, p. ES-2). The City's Urban Water Management Plan (UWMP) demonstrates that it has sufficient available water resources to adequately serve projected water demands within the City's service area through 2045. The water demand factors used to project future water demand within the City's service area are based in part on the land uses planned by the City of Corona General Plan. Thus, because the Project is fully consistent with the site's General Plan land use plan designation, it can be concluded that the city would have adequate water supplies, including groundwater supplies, to serve the Project in addition to past, present, and future commitments to supply water (City of Corona, Urban Water Management Plan, 2021, Chapter 7). Therefore, implementation of the Project would not substantially deplete groundwater supplies and the Project's impact on groundwater supplies would be less than significant.

c. Alter existing drainage pattern

Less Than Significant Impact. Under existing conditions, the site consists of two commercial buildings and vacant lots. Existing elevations across the site vary from 670.4 to 645.6 feet above sea level. The site currently slopes down at an approximate 3% grade to the northwest. The existing drainage pattern for the site and the general area is characterized by sheet flow across the project site towards Belle Avenue. Within Belle Avenue, flows are conveyed north via curb and gutter to an existing catch basin at the intersection with 3rd Street, approximately 350 feet north of the project site northern boundary. Based on the City of Corona Storm Water Gravity Main GIS map, the catch basin connects to an existing 24-inch storm drain line which confluences with a 72-inch storm drain line at the intersection of Grand Boulevard and Railroad Street, which discharges to an existing channel at the northwest corner of Harrison Street and Sheridan Street and ultimately to the Prado Basin Management Zone.

There is an existing 30-inch storm drain line within Main Street along the project frontage, which, based on the City of Corona Drainage Master Plan, the Main Street storm drain line capacity is deficient along the project frontage. In the developed condition however, onsite runoff will be collected via a network of inlets. Collected flows from the area south of the proposed market building will be conveyed via underground storm drain to proposed MWS treatment units to address water quality requirements before reaching an underground chamber system within the market parking area for increased runoff mitigation. A pump will convey flows from the underground chambers to a v-ditch along the western project boundary. The v-ditch will allow surface flows to exit the site via an under sidewalk drain to Belle Avenue. Flows will drain north along Belle Avenue following the existing condition drainage pattern.

Runoff north of the proposed market building will surface flow via curb and gutter. Due to grading constraints, this area will discharge to the surrounding existing streets without direct mitigation for increased runoff. The project has been analyzed to ensure the overall site is mitigating for increased runoff, as detailed in Section 4. Approximately 1.2 acres of the northwestern portion will drain to Belle Avenue. An inlet is proposed before these flows exit the site to pick up the water quality design flow rate of 0.2 cubic feet per second (cfs) which is conveyed to an MWS unit for treatment. The treated flows will be conveyed to the proposed pump and discharge to Belle Avenue with the mitigated flows from the underground chambers. The MWS unit will limit the flowrate to the pump as it will be designed to only treat 0.2 cfs and will not include a bypass to ensure the remaining pump capacity will appropriately mitigate the flows from the underground chamber system. The remaining flows from the northwest area will flow past the inlet and discharge directly to Belle Avenue at the northwestern driveway. Approximately 0.2 acres of the northeastern portion of the site will drain to Washburn Avenue. Due to grading constraints, this area will be treated to the maximum extent practicable by LID principles before draining offsite. See the separate report, "Northgate – 6th and Main Street Preliminary Water Quality Treatment Plan" for additional information regarding water quality treatment of the project site.

Based on utility research responses, as-built storm drain plans for the 24" storm drain line at 3rd Street and Belle Avenue were unavailable and the Drainage Master Plan Map does not include this storm drain line. Therefore, the invert elevation and hydraulic information of this 24" storm drain line are unknown. With the limited information available for the storm drain line to which the project drains, the project site was designed to route the proposed condition to the existing 10-year peak flow of 9.3 cfs to provide a conservative analysis as the City of Corona Drainage Master Plan study analyzed the existing storm drain facilities for the peak 10- and 100-year, 1-hour storm events. The runoff from the underground chambers will be routed by using a pump to limit flow to 4.5 cfs. A pump is required due to grading limitations to discharge flows at the street surface. The proposed pump will discharge a total flowrate of 4.7 cfs., which includes the 4.5 cfs for mitigated flows and 0.2 cfs for the water quality design flow rate of the northwest portion of the site. The pump is sized so the overall flows leaving the site are within 10% of the existing 10-year, 1-hour peak flow while accounting for the flows which discharge without mitigation.

Since the site runoff under the proposed Project would be conveyed to existing storm drain facilities, it can be concluded that the Project would not substantially alter the site's existing drainage pattern. As such, it can be concluded that the Project would not increase the rate or amount of surface runoff in a manner which would result in flooding; create or contribute to runoff water which would exceed the capacity of existing or proposed stormwater drainage systems; or impede or redirect flood flows. Therefore, Project impacts to the site's existing drainage pattern would be less than significant.

Additionally, according to FEMA's 2024 FIRM Map #06065C0689H, the Project site is zoned as Flood Zone X, area with minimal flood hazard. The city would review the Project permit applications to ensure the proposed development would not be subject to significant flood hazard and structures would be floodproofed and would not impede or redirect flood flows. As such, the Project would result in a less than significant impact on the existing drainage pattern and no mitigation is required.

d. Increase flooding hazard

Less Than Significant Impact. According to the Federal Emergency Management Agency (FEMA), the Project site is not located within a flood hazard zone. According to the California Department of Conservation, California Official Tsunami Inundation Maps, the site is not located within a tsunami inundation zone. In addition, the Project would not be at risk from seiche because there is no water body around the Project site capable of producing a seiche.

As discussed previously, the Project site is classified as Flood Zone X, areas of minimal flood hazard. In addition, the Project site does not include, and is not adjacent to, a body of water such as a natural stream or river that would increase the potential for flooding. Further, the Project site is located approximately 25 miles northeast of the Pacific Ocean. Therefore, the Project is not located within a tsunami zone. Similarly, a seiche is the sloshing of a closed body of water from earthquake shaking. Seiches are of concern relative to water storage facilities because inundation from a seiche can occur if the wave overflows a containment wall, such as the wall of a reservoir, water storage tank, dam, or other artificial body of water. The nearest body of water is Lake Matthews, approximately 6.5 miles to the west. The Project site is not within the vicinity of any impounded bodies of water; therefore, the Project is not at risk of a seiche.

Also, as discussed previously, the Project would introduce approximately 180,925 square feet of impervious surfaces to the site, which would increase stormwater runoff from the Project site. However, the proposed Project would install an on-site storm drain system that would convey runoff to biofilter units that would capture and filter runoff, then to the existing storm drain system in Belle Avenue. In addition, the Project includes 27,642 square feet of landscaping that would infiltrate stormwater on-site. The Project would comply with City and NPDES requirements as identified in the WQMP. Adherence to the existing requirements and implementation of the post-construction stormwater requirements would be confirmed during Project plan check prior to Project approval. Therefore, the Project would result in a less than significant impact on flooding hazards on-site or off-site and no mitigation is required.

e. Degrade surface or ground water quality

Less Than Significant Impact. As discussed under the analysis of threshold a., above, with mandatory compliance with the City's NPDES permit and with implementation of a SWPPP during construction and a WQMP during long-term operations, the Project would not degrade surface or ground water quality during either construction or long-term operation, and impacts would therefore be less than significant. Therefore, no mitigation is required.

f. Within 100-year flood hazard area

Less Than Significant Impact. As discussed under the analysis of threshold d., the Project site is within Zone X (Shaded), which encompasses areas with a 0.2% annual chance of flood, areas of 1% annual chance flood with average depths of less than one foot or with drainage areas less than one square mile, and areas protected by levees from the 1% annual chance flood (FEMA 2024). As such, the Project site is not subject to inundation during 100-year flood events, and impacts related to the 100-year flood hazard area are expected to be less than significant. Therefore, no mitigation is required.

g. Increase exposure to flooding

Less Than Significant Impact. As mentioned previously, the Project site does not include, and is not adjacent to, a natural stream or river. Thus, the Project would not increase exposure to flooding from proximity to a stream or river. In addition, a SWPPP would be implemented during construction to control drainage and maintain drainage patterns across the proposed Project. As discussed in the WQMP, existing drainage patterns would remain unchanged, which would result in a decrease in time of concentration due to increase in imperviousness. As discussed previously, the Project would introduce approximately 180,925 square feet of impervious surfaces to the site, which would increase stormwater runoff from the Project site. However, the proposed Project would install an onsite storm drain system that would convey runoff to a biofilter unit that would capture and filter runoff, then to the existing storm drain system in Buena Vista Avenue. In addition, the Project includes 27,642 square feet of landscaping that would infiltrate stormwater onsite. The Project would comply with City and NPDES requirements as identified in the WQMP (Appendix B). Adherence to the existing requirements and implementation of the post construction stormwater requirements would be confirmed during Project plan check prior to Project approval. Therefore, the Project would result in a less than significant impact on flooding on- or offsite, and no mitigation is required.

Based on the analyses and results of the Preliminary Drainage Report, the following conclusions were derived from the hydrology and hydraulic results:

- The proposed drainage improvements will adequately convey flows to the underground chambers and provide flood protection for the 100-year storm event.
- The proposed MWS units will provide adequate water quality treatment.
- The proposed project will not impact flooding condition to upstream or downstream properties.

h. Exceed capacity of the storm water drainage system

Less Than Significant Impact. As described in the previous responses, the proposed Project would be required to implement a SWPPP during construction that would implement BMPs, such as the use of silt fencing, fiber rolls, and gravel bags, that would ensure that runoff would not substantially increase during construction, and that pollutants would not discharge from the Project site, which would reduce potential impacts to stormwater drainage systems and water quality to

a less than significant level.

The proposed Project would introduce approximately 180,925 square feet of impervious surfaces to the Project site. Proposed flow-based biotreatment facilities would mitigate the maximum flow rate of runoff produced from a rainfall intensity of 0.2 inch of rainfall per hour. These systems would filter coarse sediment, trash, and pollutants (i.e., sediments, nutrients, heavy metals, oxygen demanding substances, oil and grease, bacteria, and pesticides). The proposed biotreatment facilities sized for the required design flowrates and the low impact development (LID) design features will meet the water quality treatment requirements for the Project site. Although the Project is anticipated to increase runoff, an underground detention and pump system is proposed to mitigate increased runoff for the 10- and 100-year storms to be within 10% of the existing 10-year, 1-hour storm event. Therefore, development of the proposed Project would not create or contribute runoff water that would exceed the capacity of existing or planned stormwater drainage systems and impacts would be less than significant. As such, no mitigation

According to the Federal Emergency Management Agency (FEMA), the Project site is not located within a flood hazard zone. According to the California Department of Conservation, California Official Tsunami Inundation Maps, the site is not located within a tsunami inundation zone. In addition, the Project would not be at risk from seiche because there is no water body around the Project site capable of producing as seiche.

5. AIR QUALITY:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
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a. Conflict with air quality plan	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Violate air quality standard	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Net increase of any criteria pollutant	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Expose sensitive receptors to pollutants	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e. Create objectionable odors	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

The following section is based on the Air Quality & Greenhouse Gas Emissions Assessment prepared by ECORP Consulting, Inc., dated October 2025 (Revised) (Appendix E).

Air Pollutants and Health Effects

Criteria air pollutants are defined as those pollutants for which the federal and state governments have established air quality standards for outdoor or ambient concentrations to protect public health with a determined margin of safety. Ozone (O3), coarse particulate matter (PM10), and fine particulate matter (PM2.5) are generally considered to be regional pollutants because they or their precursors affect air quality on a regional scale. Pollutants such as carbon monoxide (CO), nitrogen dioxide (NO2), and sulfur dioxide (SO2) are considered to be local pollutants because they tend to accumulate in the air locally. Particulate matter (PM) is also considered a local pollutant. Health effects commonly associated with criteria pollutants are summarized in Table 5-A.

Table 5-A: Criteria Air Pollutants – Summary of Common Sources and Effects

Pollutant	Major Manmade Sources	Human Health & Welfare Effects
CO	An odorless, colorless gas formed when carbon in fuel is not burned completely; a component of motor vehicle exhaust.	Reduces the ability of blood to deliver oxygen to vital tissues, affecting the cardiovascular and nervous system. Impairs vision, causes dizziness, and can lead to unconsciousness or death.
NO ₂	A reddish-brown gas formed during fuel combustion for motor vehicles, energy utilities and industrial sources.	Respiratory irritant; aggravates lung and heart problems. Precursor to ozone and acid rain. Causes brown discoloration of the atmosphere.
O ₃	Formed by a chemical reaction between reactive organic gases (ROGs) and nitrous oxides (N ₂ O) in the presence of sunlight. Common sources of these precursor pollutants include motor vehicle exhaust, industrial emissions, solvents, paints, and landfills.	Irritates and causes inflammation of the mucous membranes and lung airways; causes wheezing, coughing and pain when inhaling deeply; decreases lung capacity; aggravates lung and heart problems. Damages plants; reduces crop yield.
PM ₁₀ & PM _{2.5}	Power plants, steel mills, chemical plants, unpaved roads and parking lots, wood-burning stoves and fireplaces, automobiles, and others.	Increased respiratory symptoms, such as irritation of the airways, coughing, or difficulty breathing; aggravated asthma; development of chronic bronchitis; irregular heartbeat; nonfatal heart attacks; and premature death in people with heart or lung disease. Impairs visibility (haze).
SO ₂	A colorless, nonflammable gas formed when fuel containing sulfur is burned. Examples are refineries, cement manufacturing, and locomotives.	Respiratory irritant. Aggravates lung and heart problems. Can damage crops and natural vegetation. Impairs visibility.

Source: California Air Pollution Control Officers Association (CAPCOA 2013)

a. Conflict with air quality plan

Less than Significant Impact. The Project site is located within the South Coast Air Basin (SCAB), which is characterized by relatively poor air quality. The South Coast Air Quality Management District (SCAQMD) has jurisdiction over an approximately 10,743 square-mile area consisting of the four-county Basin and the Los Angeles County and Riverside County portions of what use to be referred to as the Southeast Desert Air Basin. In these areas, the SCAQMD is principally responsible for air pollution control, and works directly with the Southern California Association of Governments (SCAG), county transportation commissions, local governments, as well as state and federal agencies to reduce emissions from stationary, mobile, and indirect sources to meet state and federal ambient air quality standards.

Currently, these state and federal air quality standards are exceeded in most parts of the SCAB. In response, the SCAQMD has adopted a series of Air Quality Management Plans (AQMPs) to meet the state and federal ambient air quality standards. The 2022 AQMP includes aggressive new regulations and the development of incentive programs to support early deployment of advanced technologies. The AQMP focuses on available, proven, and cost-effective alternatives to traditional strategies, while seeking to achieve multiple goals in partnership with other entities promoting reductions in GHGs (Greenhouse Gas) and toxic risk, as well as efficiencies in energy use, transportation, and goods movement.

In April 2024, the SCAG Regional Council adopted the 2024-2050 Regional Transportation Plan/ Sustainable Communities Strategy (2024 RTP/SCS). The 2024 RTP/SCS charts a course for closely integrating land use and transportation – so that the region can grow smartly and sustainably. It was prepared through a collaborative, continuous, and comprehensive process with input from local governments, county transportation commissions, tribal governments, non-profit organizations, businesses and local stakeholders within various counties, including Riverside County. The 2024 RTP/SCS is a long-range visioning plan that balances future mobility and housing needs with economic, environmental and public health goals.

As described in Chapter 12, Section 12.2 and Section 12.3 of the SCAQMD’s CEQA Air Quality Handbook (1993), for purposes of analyzing consistency with the AQMP, if a proposed Project would result in growth that is substantially greater than what was anticipated, then the proposed Project would conflict with the AQMP. On the other hand, if a Project’s density is within the anticipated growth of a jurisdiction, its emissions would be consistent with the assumptions in the AQMP, and the Project would not conflict with SCAQMD’s attainment plans. In addition, the SCAQMD considers projects consistent with the 2022 AQMP if the project would not result in an increase in the frequency or severity of existing air quality violations or cause a new violation.

As shown in Tables 5-C and 5-D in Section 5.b below, the Project would not exceed SCAQMD significance thresholds for any criteria pollutant during short term construction or during long-term operation. The construction contractors are required

to comply with rules, regulations, and control measures to control fugitive dust from grading (Rule 403) and the application of architectural coatings during building construction (Rule 1113). Accordingly, the Project’s air quality emissions are less than significant and no mitigation is warranted.

The Project site is designated, and proposed, as MUD (Mixed Use Downtown) on the General Plan map and zoned D (Downtown) within the Downtown Revitalization Specific Plan. The MUD land use designation is intended for the development of retail commercial uses with a maximum Floor Area Ratio (FAR) of 3.0. The MUD land use designation was the land use designation that was used by the SCAQMD to generate the growth forecasts for the air quality plan referenced above.

Finally, emissions generated by construction and operation of the proposed Project would not exceed daily emissions thresholds established by the SCAQMD. As described in the analysis below and detailed in Appendix E, the Project would not result in an increase in the frequency or severity of existing air quality violations or cause a new violation. Therefore, impacts related to conflict with the AQMP from the proposed Project would be less than significant, and no mitigation is required.

b. Violate air quality standard

Less than Significant Impact. The South Coast Air Basin (SCAB) is in a non-attainment status for federal and State ozone standards and particulate matter standards. Any development in the SCAB, including the proposed Project, could cumulatively contribute to these pollutant violations. According to the SCAQMD, an air quality impact is considered significant if the proposed Project would violate any ambient air quality standard, contribute substantially to an existing or projected air quality violation, or expose sensitive receptors to substantial pollutant concentrations. The SCAQMD has established thresholds of significance for air quality for construction and operational activities of land use development projects such as that proposed, as shown in Table 5B. Should construction or operation of the proposed Project exceed these thresholds, a significant impact could occur; however, if estimated emissions are less than the thresholds, impacts would be considered less than significant.

TABLE 5-B: SCAQMD REGIONAL EMISSIONS THRESHOLDS (pounds/day)

Air Pollutant	Construction Activities	Operations
ROG	75	55
CO	550	550
NO _x	100	55
SO _x	150	150
PM ₁₀	150	150
PM _{2.5}	55	55

Source: South Coast Air Quality Management District 2023

Construction

Regional Construction Significance Analysis

Construction-generated emissions are temporary and short-term but have the potential to represent a significant air quality impact. The basic sources of short-term emissions that would be generated through construction of the proposed Project would be from grading activities and the from the operation of construction vehicles (i.e., trenchers, dump trucks). Construction activities such as excavation and grading operations, vehicle traffic, and wind blowing over exposed soils would generate exhaust emissions and fugitive particular matter (PM) emissions that affect local air quality at various times during construction. Construction activities would be subject to SCAQMD Rule 403, which requires taking reasonable precautions to prevent the emissions of fugitive dust, such as using water or chemicals, where possible, for control of dust during the clearing of land and other construction activities.

Construction-generated emissions associated with the Project were calculated using the CARB-approved CalEEMod computer program, which is designed to model emissions for land use development projects, based on typical construction requirements. See Attachment A of Appendix E for more information regarding the construction assumptions, including construction equipment and duration, used in this analysis. Predicted maximum daily construction-generated emissions for the Project are summarized in Table 5-C. Construction emissions generated by the Project would not exceed SCAQMD regional thresholds as identified in Table 5-C, and therefore, regional construction related air quality emissions would result in a less than significant impact and no mitigation is required.

TABLE 5-C: REGIONAL CONSTRUCTION-RELATED EMISSIONS

Construction Year	Pollutant (pounds per day)					
	ROG	NO _x	CO	SO ₂	PM ₁₀	PM _{2.5}
Construction Calander Year One	2.02	20.90	31.10	0.08	6.98	3.31
Construction Calander Year Two	1.99	17.60	28.20	0.04	0.81	0.37
<i>SCAQMD Regional Significance Threshold</i>	<i>75</i>	<i>100</i>	<i>550</i>	<i>150</i>	<i>150</i>	<i>55</i>
Exceed SCAQMD Regional Threshold?	No	No	No	No	No	No

Notes: Emissions taken of the season, summer or winter, with the highest outputs. Emission reductions for construction PM emissions are applied based on the required implementation of SCAQMD Rule 403. The specific Rule 403 measures applied in CalEEMod include the following: sweeping/cleaning adjacent roadways once per month; watering unpaved roads twice daily; watering demolition areas twice daily; watering exposed surfaces three times daily; and limiting speeds on unpaved roads to 25 miles per hour. Emissions account for the demolition of 12,677 square feet of building space and the removal of 6,091 cubic yards (7,918.3 tons) of pavement.

Source: CalEEMod version 2022.1. Refer to Attachment A for Model Data Outputs.

Operation

Regional Operational Significance Analysis

Implementation of the Project would result in long-term operational emissions of criteria air pollutants such as PM10, PM2.5, CO, and SO2 as well as O3 precursors such as ROGs and NOX. Project-generated increases in emissions would be predominantly associated with motor vehicle use. Operational air pollutant emissions are based on the area of impact identified on the Project’s site plan. Long-term operational emissions attributable to the Project are identified in Table 2-9 and compared to the operational significance thresholds promulgated by the SCAQMD.

TABLE 5-D: REGIONAL OPERATIONAL-RELATED EMISSIONS

Emission Source	Pollutant (pounds per day)					
	ROG	NO _x	CO	SO ₂	PM ₁₀	PM _{2.5}
Summer Emissions						
Mobile	13.40	11.20	103.00	0.25	21.80	5.67
Area	1.42	0.02	2.04	0.00	0.00	0.00
Energy	0.02	0.32	0.27	0.00	0.02	0.02
Total:	14.84	11.54	105.31	0.25	21.82	5.69
<i>SCAQMD Regional Significance Threshold</i>	<i>55</i>	<i>55</i>	<i>550</i>	<i>150</i>	<i>150</i>	<i>55</i>
Exceed SCAQMD Regional Threshold?	No	No	No	No	No	No
Winter Emissions						
Mobile	12.50	12.00	87.20	0.23	21.80	5.67
Area	1.09	0.00	0.00	0.00	0.00	0.00
Energy	0.02	0.32	0.27	0.00	0.02	0.02
Total:	13.61	12.32	87.47	0.23	21.82	5.69
<i>SCAQMD Regional Significance Threshold</i>	<i>55</i>	<i>55</i>	<i>550</i>	<i>150</i>	<i>150</i>	<i>55</i>
Exceed SCAQMD Regional Threshold?	No	No	No	No	No	No

Notes: Emission projections predominately based on CalEEMod model defaults for Riverside County. Mobile source emissions are based on trip rates identified in the Project’s Traffic Report (Linscott, Law, and Greenspan Engineers 2024).

Source: CalEEMod version 2022.1. Refer to Attachment A for Model Data Outputs.

As shown in Table 5-D above, operational-related emissions would not exceed South Coast Air Quality Management District thresholds. Accordingly, the Project would not emit substantial concentrations of these pollutants during operation and would not contribute to an existing or projected air quality violation on a direct or cumulative basis. As such, impacts are less than significant, and no mitigation measures are required.

c. Net increase of any criteria pollutant

Less than Significant Impact. As mentioned previously, the South Coast Air Basin (SCAB) is in a non-attainment status for federal and State ozone standards and particulate matter standards. Any development in the SCAB, including the proposed Project, could cumulatively contribute to these pollutant violations. The methodologies from the SCAQMD CEQA Air Quality Handbook are used in evaluating Project impacts. SCAQMD has established daily mass thresholds for regional pollutant emissions, which are shown above in Table 5-B.

Therefore, this analysis assumes that individual projects that do not generate operational or construction emissions that exceed the SCAQMD's recommended daily thresholds for project specific impacts would also not cause a cumulatively considerable increase in emissions for those pollutants for which SCAB is in nonattainment, and, therefore, would not be considered to have a significant, adverse air quality impact. Alternatively, individual project-related construction and operational emissions that exceed SCAQMD thresholds for project-specific impacts would be considered cumulatively considerable.

Construction Impacts

The Project-specific evaluation of emissions presented in the preceding analysis demonstrates that proposed Project construction-source air pollutant emissions would result in a temporary exceedance of regional thresholds for PM₁₀. However, proposed Mitigation Measure (AQ-1) would reduce PM₁₀ emissions to levels below the significance threshold. Therefore, the proposed Project construction-source emissions would be considered less than significant on a project-specific and cumulative basis.

Operational Impacts

The Project-specific evaluation of emissions presented in the preceding analysis demonstrates that proposed Project operational-source air pollutant emissions would not result in exceedances of regional thresholds. Therefore, the proposed Project operational-source emissions would be considered less than significant on a project-specific and cumulative basis, and no mitigation is required.

d. Expose sensitive receptors to pollutants

Less than Significant Impact. The SCAQMD has developed Localized Significance Thresholds (LSTs) that represent the maximum emissions from a Project that are not expected to cause or contribute to an exceedance of the most stringent applicable federal or state ambient air quality standards and thus would not cause or contribute to localized air quality impacts. LSTs are developed based on the ambient concentrations of CO, NO₂, PM₁₀, and PM_{2.5} pollutants for each of the 38 source receptor areas (SRAs) in the SCAB. The Project site is located in SRA 22, Norco/Corona.

The SCAQMD recommends that the nearest sensitive receptor be considered when determining the Project's potential to cause an individual or cumulatively significant impact. The nearest sensitive receptors include residences located directly adjacent to the Project's northern boundary. In order to identify localized, air toxic-related impacts to sensitive receptors, the SCAQMD recommends addressing LSTs for construction. While the Project Area would disturb up to 4.88 acres during construction, the LST threshold value for a two-acre site was employed from the LST lookup tables for these phases. This is conservative since the analysis will only account for the dispersion of air pollutants over two acres before reaching sensitive receptors, as opposed to accounting for the dispersion of pollutants over the 4.88-acre Project Area.

LST thresholds are provided for distances to sensitive receptors of 25, 50, 100, 200, and 500 meters. The nearest sensitive receptors to construction activity as a result of the Project are residences located directly adjacent to the Project Site (<25 meters). Notwithstanding, the SCAQMD Methodology explicitly states: "It is possible that a project may have receptors closer than 25 meters. Projects with boundaries located closer than 25 meters to the nearest receptor should use the LSTs for receptors located at 25 meters." Therefore, LSTs for receptors located at 25 meters were utilized in this analysis. The SCAQMD's methodology clearly states that "offsite mobile emissions from a project should not be included in the emissions compared to LSTs." Therefore, for purposes of the construction LST analysis, only emissions included in the CalEEMod "onsite" emissions outputs were considered. Table 5-E presents the results of localized emissions. The LSTs reflect a maximum disturbance of the entire site.

Localized Construction Emissions

Table 2-7 shows that the emissions of these pollutants during construction would result in significant concentrations of pollutants at nearby sensitive receptors for PM₁₀ during the site preparation phase. Therefore, **Mitigation Measure (MM) AQ-1** is required in order to reduce PM₁₀ emissions to levels below the significance threshold. **MM AQ-1** would mandate

the use of Tier 4 Certified engines for all the Project offroad construction equipment used during the Project’s construction. Tier 4 equipment has specific emission standards established by the USEPA that regulate the amount of PM and NOx emitted by diesel engines in construction equipment. Tier 4 standards require the use of advanced engine technologies such as DPM (diesel particulate matter) filters and selective catalytic reduction systems, to significantly reduce PM₁₀ emissions.

TABLE 5-E: UNMITIGATED PROJECT - LOCALIZED CONSTRUCTION IMPACTS

Activity	Onsite Pollutant (pounds per day)			
	NO _x	CO	PM ₁₀	PM _{2.5}
Demolition	24.90	21.70	5.10	2.83
Site Preparation	36.00	32.90	6.72	4.10
Grading	18.20	18.80	0.84	0.77
Building Construction, Paving, and Painting (Year One)	18.98	23.14	0.86	0.79
Building Construction, Paving, and Painting (Year Two)	17.80	22.98	0.75	0.66
<i>SCAQMD Localized Significance Threshold (2 acres of disturbance at 25 meters)</i>	<i>170</i>	<i>1,007</i>	<i>6</i>	<i>5</i>
Exceed SCAQMD Localized Thresholds?	No	No	Yes	No

Notes: Emissions taken of the season, summer or winter, with the highest outputs. Emission reductions for construction PM emissions are applied based on the required implementation of SCAQMD Rule 403. The specific Rule 403 measures applied in CalEEMod include the following: sweeping/cleaning adjacent roadways once per month; watering unpaved roads twice daily; watering demolition areas twice daily; watering exposed surfaces three times daily; and limiting speeds on unpaved roads to 25 miles per hour. Emissions account for the demolition of 12,677 square feet of building space and the removal of 6,091 cubic yards (7,918.3 tons) of pavement.

Source: CalEEMod 2021.1. Refer to Attachment A for Model Data Outputs.

Mitigation Measure

MM AQ-1: Prior to the issuance of a grading and building permit for the Corona Northgate Market Project, the Project Applicant shall add the following construction note on the grading and building plans, and demonstrate to the satisfaction of the City of Corona Planning and Development Department that the following measure is implemented during Project construction.

- All offroad equipment of greater than 50 horsepower used in the site preparation phase of Project construction shall be California Air Resources Board (CARB) Tier 4 Certified, as set forth in Section 2423 of Title 13 of the California Code of Regulations, and Part 89 of Title 40 of the Code of Federal Regulations.

Timing/Implementation: Prior to the issuance of a grading and building permit, and during construction.

Monitoring/Enforcement: City of Corona Planning and Development Department

Table 5-F shows the results of construction emissions with implementation of **MM AQ-1** for the site preparation phase, as that was the only phase with an exceedance and where **MM AQ-1** is required.

TABLE 5-F: MITIGATED PROJECT - LOCALIZED CONSTRUCTION IMPACTS

Activity	Onsite Pollutant (pounds per day)			
	NO _x	CO	PM ₁₀	PM _{2.5}
Site Preparation	14.80	28.20	5.27	2.77
<i>SCAQMD Localized Significance Threshold (2 acres of disturbance at 25 meters)</i>	<i>170</i>	<i>1,007</i>	<i>6</i>	<i>5</i>
Exceed SCAQMD Localized Thresholds?	No	No	No	No

Notes: Emissions taken of the season, summer or winter, with the highest outputs. Emission reductions for construction PM emissions are applied based on the required implementation of SCAQMD Rule 403. The specific Rule 403 measures applied in CalEEMod include the following: sweeping/cleaning adjacent roadways once per month; watering unpaved roads twice daily; watering demolition areas twice daily; watering exposed surfaces three times daily; and limiting speeds on unpaved roads to 25 miles per hour. Emissions account for the demolition of 12,677 square feet of building space and the removal of 6,091 cubic yards (7,918.3 tons) of pavement.

Source: CalEEMod version 2022.1. Refer to Attachment A for Model Data Outputs.

As shown in Table 5-F, adherence to mitigation measure AQ-1 would ensure that the proposed Project would not generate PM₁₀ emissions in excess of SCAQMD LST standards during the site preparation phase. In addition to this construction HRA (health risk assessment), the Project has been evaluated against the SCAQMD’s LSTs for construction. LSTs were developed in response to SCAQMD Governing Boards’ Environmental Justice Enhancement Initiative and can be used to assist lead agencies in analyzing localized impacts associated with Project-specific level of proposed projects. The SCAQMD Environmental Justice Enhancement Initiative program seeks to ensure that everyone has the right to equal protection from air pollution. The Environmental Justice Program is divided into three categories, with the LST protocol promulgated under Category I: Further-Reduced Health Risk. As shown in Table 2-8, the emissions of pollutants on the peak day of construction would not result in significant concentrations of pollutants at nearby sensitive receptors, with implementation of **MM AQ-1**. Thus, the fact that onsite Project construction emissions would be generated at rates below the LSTs for NO_x, CO, PM₁₀, and PM_{2.5} verifies that the Project would not adversely impact nearby sensitive receptors. In summary, Project construction would not result in a potentially significant contribution to regional concentrations of nonattainment pollutants and would not result in a significant contribution to the adverse health impacts associated with those pollutants.

Localized Operational Emissions

The Project includes the redevelopment of a 4.88-acre site with the development and operation of a 40,000 square foot supermarket, a 3,297 square foot restaurant and 3,633 square foot bank. According to the SCAQMD localized significance threshold methodology, LSTs would apply to the operational phase of a proposed project only if the project includes stationary sources (e.g., smokestacks) or attracts heavy-duty trucks that may spend long periods queuing and idling at the site (e.g., warehouse or transfer facilities). Operation of the Project would not result in the development of any substantial sources of air toxics. There are no stationary sources associated with the operations of the Project; nor would the Project attract additional heavy-duty truck sources, a major source of diesel particulate matter (DPM), that spend long periods queuing and idling at the site. Onsite Project emissions would not result in significant concentrations of pollutants at nearby sensitive receptors. Furthermore, the Project would not have a high carcinogenic or non-carcinogenic risk during operation (ECORP Air Quality Assessment, October 2025). Therefore, in the case of the proposed Project, the operational LST protocol is not applied.

Construction Health Risk Assessment

Cancer Risk

Construction cancer risk calculations for existing residential, worker and school child (Saint Edwards Catholic School) receptors were calculated using the total time that construction is proposed; two years. The school child risk was calculated using the residential exposure and health parameters. The calculated cancer risk accounts for 350 days per year of exposure to all receptors. While the average American spends 87 percent of their life indoors (USEPA 2001), neither the pollutant dispersion modeling nor the health risk calculations account for the reduced exposure structures provide. Instead, health risk calculations account for the equivalent exposure of continual outdoor living and working. The calculated carcinogenic risk at Project vicinity receptors is depicted in Table 5-G.

TABLE 5-G CANCER RISK

Table 2-10. Maximum Cancer Risk Summary			
Maximum Exposure Scenario	Total Maximum Cancer Risk	UTMx (Zone 11)	UTMy (Zone 11)
2-Year Exposure Resident	6.20	447392.7	3748617.2
2-Year Exposure Worker	0.42	447542.7	3748617.2
2-Year Exposure School Child	0.17	447142.7	3748617.2
<i>Significance Threshold</i>	<i>10</i>		
Exceed Threshold?	No	--	--

Source: ECORP Consulting 2024. See Attachment B.

As shown on Table 5-G, the existing residents, workers and school children would not experience a significant amount of cancer risk from construction of the Proposed Project. The maximumly exposed individual resident (MEIR) is west of the Project Site in the single-family homes west of S. Belle Avenue. The maximumly exposed individual worker (MEIW) is in the Corona Mall east of the Project Site.

CO Hotspots

The Project would not result in potentially adverse CO concentrations or “hot spots.” An adverse CO concentration, known as a “hot spot”, would occur if an exceedance of the state one-hour standard of 20 ppm (parts per million) or the eight-hour standard of 9 ppm were to occur. It has long been recognized that CO hotspots are caused by vehicular emissions, primarily when idling at congested intersections. In response, vehicle emissions standards have become increasingly stringent in the last twenty years. Currently, the allowable CO emissions standard in California is a maximum of 3.4 grams/mile for passenger cars (there are requirements for certain vehicles that are more stringent). With the turnover of older vehicles, introduction of cleaner fuels, and implementation of increasingly sophisticated and efficient emissions control technologies, CO concentration in the SCAB is now designated as attainment.

According to the Traffic Impact Analysis report prepared by Linscott, Law, and Greenspan Engineers, the Project is expected to generate approximately 3,377 daily vehicle trips (Linscott, Law, and Greenspan Engineers 2025). Thus, the proposed Project would not generate traffic volumes at any intersection of more than 100,000 vehicles per day (or 44,000 vehicles per day) and there is no likelihood of the Project traffic exceeding CO values. Localized air quality impacts related to mobile-source emissions would therefore be less than significant.

e. Create objectionable odors

Less Than Significant Impact. The potential for the Project to generate objectionable odors has also been considered. Land uses generally associated with odor complaints include:

- *Agricultural uses (livestock and farming)*
- *Wastewater treatment plants*
- *Food processing plants*
- *Chemical plants*
- *Composting operations*
- *Refineries*
- *Landfills*
- *Dairies*
- *Fiberglass molding facilities*

The Project does not contain land uses typically associated with emitting objectionable odors. During construction, the Proposed Project presents the potential for generation of objectionable odors in the form of diesel exhaust in the immediate vicinity of the Project Area. However, these emissions are short-term in nature and would rapidly dissipate and be diluted by the atmosphere downwind of the emission sources. Additionally, odors would be localized and generally confined to the construction area. Therefore, construction odors would not adversely affect a substantial number of people to odor emissions.

The proposed Project would also be required to comply with SCAQMD Rule 402 to prevent occurrences of public nuisances. Therefore, odors associated with the proposed Project construction and operations would be less than significant and no mitigation is required.

6. TRANSPORTATION/TRAFFIC:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
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a. Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Conflict of be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Increase the total daily vehicle miles traveled per service population (population plus employment) (VMT/SP) above the baseline level for the jurisdiction	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Cause total daily VMT within the study area to be higher than the No Project alternative under cumulative conditions (General Plan Condition)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Change in air traffic patterns	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Traffic hazards from design features	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g. Emergency access	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h. Conflict with alternative transportation policies	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

The following section is based on the Traffic Impact Analysis (TIA) prepared by Linscott, Law, and Greenspan Engineers, dated July 10, 2025 (Appendix F).

a. Conflict with an applicable plan, ordinance, or policy establishing measures of effectiveness for the performance of the circulation system

Less than Significant Impact. A significant impact would occur if the development of the Project conflicted with programs, plans, or ordinances that support transit services, bicycle lanes, sidewalks, and trails. Future street improvements that are programmed to implement the updated circulation network plan will be designed in accordance with all applicable engineering standards relating to vehicle traffic, bicycles, pedestrian safety, line of site, and other design criteria.

The Project’s TIA evaluated potential traffic impacts of the proposed Project on the area traffic circulation. The Project site is currently partially vacant and also occupied with a strip retail building (to be razed), bank (to be converted into a casual restaurant and new bank), and drive-thru ATM structure related to the existing bank. Two previous residential structures were previously demolished on the north side of W. Fourth Street.

Access to the Project site will be provided via one (1) driveway on S. Main Street, one (1) driveway on S. Belle Avenue across from W. Fifth Street, one (1) driveway on S. Belle Avenue north of W. Sixth Street, and one (1) driveway each on W. Fourth Street and W. Sixth Street. With development of the Project site, W. Fifth Street between S. Belle Avenue and S. Main Street will be abandoned and developed as parking area. Additionally, the segment of W. Fourth Street from S. Belle Avenue to the alley will be abandoned to accommodate the Northgate grocery market building. The TIA forecast is based on a 2026 forecast.

The trip generation for the Project was calculated using trip rates from the Institute of Transportation Engineers (ITE), Trip

Generation 11th Edition, 2021. As shown in Table 6-A, the Project would generate approximately 3,377 daily trips including 133 trips during the AM peak hour and 324 trips during the PM peak hour.

TABLE 6-A: PROJECT TRIP GENERATION RATES & FORECAST

ITE Land Use Code / Project Description	Daily 2-Way	AM Peak Hour			PM Peak Hour		
		Enter	Exit	Total	Enter	Exit	Total
<u>Generation Rates:</u>							
▪ 210: Single Family Detached Housing (TE/DU)	9.43	26%	74%	0.70	63%	37%	0.94
▪ 822: Strip Retail Plaza Less Than 40K (TE/TSF)	54.45	60%	40%	2.36	50%	50%	6.59
▪ 850: Supermarket (TE/TSF)	93.84	59%	41%	2.86	50%	50%	8.95
▪ 911: Walk-In Bank (TE/TSF) ⁶	100.35	58%	42%	9.95	44%	56%	12.13
▪ 912: Drive-In Bank (TE/TSF)	100.35	58%	42%	9.95	50%	50%	21.01
▪ 930: Fast Casual Restaurant (TE/TSF)	97.14	50%	50%	1.43	55%	45%	12.55
<u>Proposed Project Generation Forecast:</u>							
▪ Supermarket (40,000 SF)	3,754	67	47	114	179	179	358
Pass-By (Daily: 25%, AM: 10%, PM: 24%) ⁷	<u>-939</u>	<u>-7</u>	<u>-4</u>	<u>-11</u>	<u>-43</u>	<u>-43</u>	<u>-86</u>
<i>Supermarket Subtotal</i>	2,815	60	43	103	136	136	272
▪ Fast Casual Restaurant (3,297 SF)	320	3	2	5	23	18	41
Pass-By (Daily: 10%, AM: 10%, PM: 43%) ⁷	<u>-32</u>	<u>0</u>	<u>-1</u>	<u>-1</u>	<u>-10</u>	<u>-8</u>	<u>-18</u>
<i>Retail Subtotal</i>	288	3	1	4	13	10	23
▪ Walk-in Bank (3,633 SF)	365	21	15	36	19	25	44
Pass-By (Daily: 25%, AM: 29%, PM: 35%) ⁷	<u>-91</u>	<u>-6</u>	<u>-4</u>	<u>-10</u>	<u>-7</u>	<u>-8</u>	<u>-15</u>
<i>Retail Subtotal</i>	274	15	11	26	12	17	29
<i>Proposed Project Total [A]</i>	3,377	78	55	133	161	163	324
<u>Existing Occupied Land Use Generation Forecast:</u>							
▪ Existing Occupied Retail (6,827 SF)	372	10	6	16	23	22	45
Pass-By (Daily: 25%, AM: 10%, PM: 25%) ⁷	<u>-93</u>	<u>-1</u>	<u>-1</u>	<u>-2</u>	<u>-6</u>	<u>-5</u>	<u>-11</u>
<i>Existing Retail Subtotal</i>	279	9	5	14	17	17	34
▪ Existing Occupied Drive-In Bank (7,677 SF)	770	44	32	76	81	80	161
Pass-By (Daily: 25%, AM: 29%, PM: 30%) ⁷	<u>-193</u>	<u>-13</u>	<u>-9</u>	<u>-22</u>	<u>-28</u>	<u>-28</u>	<u>-56</u>
<i>Existing Drive-In-Bank Subtotal</i>	577	31	23	54	53	52	105
▪ Existing Occupied Single Family Detached Housing (3 DU)	28	1	1	2	2	1	3
<i>Existing Land Use Total [B]</i>	884	41	29	70	72	70	142
Total Net Proposed Project Trip Generation Forecast [A] - [B]	2,493	37	26	63	89	93	182

Notes:

- TE/DU = Trip End per Dwelling Unit
- TE/TSF = Trip End per Thousand Square Feet

Roadway Facilities

For CEQA purposes, roadway facilities are viewed in the context of how they reduce the amount of vehicle miles traveled and promote the use of other non-motorized modes of travel such as transit, bicycle, and pedestrian. Per the City of Corona's General Plan Circulation Element, the segment of S. Main Street adjacent to the Project site is designated as a major arterial 6-lane street. Major arterials are 82 to 106 feet wide curb-to-curb within a 106- to 130-foot right-of-way. Main Street is currently a four-lane divided roadway south of Fourth Street and a six-lane divided roadway located north of Fourth Street. It is located east of the Project site. Parking is restricted on both sides of the roadway within the vicinity of the Project. Main Street has a posted speed limit of 35 mph. The intersections of Main Street at SR-91 Westbound (WB) Ramps, SR-91 Eastbound (EB) Ramps, Third Street, Sixth Street, Eighth Street and Grand Boulevard are controlled by a traffic signal. The intersections of Main Street at Fifth Street and Ninth Street are stop controlled.

The segment of W. Sixth Street adjacent to the Project site is designated as a mixed use boulevard 4 lane divided/undivided street. Mixed use boulevards are streets that serve land use patterns in the city’s mixed-use land use districts. W. Sixth Street is generally a two-lane divided roadway within the Downtown traffic study area located south of the Project site. Parking is generally permitted on both sides of the roadway within the vicinity of the Project. Sixth Street has a posted speed limit of 35 mph. The intersections of Sixth Street at Main Street, Grand Boulevard East, Grand Boulevard West, and Belle Avenue are controlled by a traffic signal. It should be noted that complete street improvements were recently implemented along Sixth Street between West Grand Avenue and East Grand Avenue in August 2024, which included reducing Sixth Street from a four-lane roadway to a two-lane with Class II bike lanes in each direction and on-street parking.

The segment of S. Belle Avenue adjacent to the project site is designated as a collector street which is typically 44 feet wide curb-to-curb (2 lanes) within a 68-foot right-of-way. Belle Avenue is a two-lane undivided roadway located west of the Project site. Parking is permitted on either side of the roadway within the vicinity of the Project. The posted speed limit on Belle Avenue is 25 mph. The intersection of Belle Avenue at Sixth Street is controlled by a traffic signal. The intersections of Belle Avenue at Fifth Street, Fourth Street, and Third Street are stop controlled.

The Project requires eight (8) feet of street dedication along the project site’s W. Sixth Street frontage; however, this area is fully improved with sidewalk, curb and gutter and will not require additional public right-of-way improvements. The Project requires 15 feet of street dedication along the site’s S. Main Street frontage in addition to public right-of-way improvements consisting of parkway landscaping, sidewalk, street trees and curb and gutter. The City will vacate six (6) feet along the site’s S. Belle Avenue frontage, adding square footage to the Project’s parking area, and includes public right-of-way improvements consisting of new street trees, with existing sidewalk, curb and gutter to remain.

Additionally, as part of the Project’s development, the southbound lanes on Main Street adjacent to the Project Site will be restriped to include the following lanes:

- Two (2) 12-foot wide through-lanes;
- One (1) left-turn pocket to allow for vehicles to turn left into the North Corona Mall entrance; and
- One (1) right-turn pocket to allow for vehicles to turn right into the Project Site.

The required improvements within the public right-of-way will serve to facilitate vehicular, pedestrian, bicycle and bus travel.

Bicycle and Pedestrian Circulation

Within the vicinity of the project, Sixth Street currently has Class II bike lanes, which were installed with the complete street improvements project. Pedestrian circulation would be provided via existing public sidewalks along Main Street within the vicinity of the Project, as well as along Sixth Street and Belle Avenue. Bicycle and pedestrian activity have been captured as part of the existing data collection and are incorporated into the operations analysis.

Public Transit Service

Public transportation services within the City, including the Project area, are provided by the Riverside Transit Authority (RTA). A description of the transit services within the Project vicinity is as follows:

Riverside Transit Agency RTA)

City Route 1:

- Route 1 provides service from the University of California, Riverside (UCR) to Downtown Riverside to Corona; via Smith Avenue & Sixth Street, Corona Transit Center, Magnolia & McKinley, Galleria at Tyler, Magnolia Avenue & Adams Street, Brockton Arcade, Riverside City College, University & Lemon, Riverside – Downtown Metrolink Station, and UCR at Bannockburn.
- The route traverses the cities of Riverside and Corona.
- During the weekday and weekend AM and PM peak hours, Route 1 has approximate headways between 15 and 30 minutes in the northbound and southbound directions.

The Corona Cruiser operates within the study area. The Corona Cruiser is Corona’s fixed-route bus system that travel along two routes in the city, the Red Line and the Blue Line. A description of these routes within the Project vicinity are as follows:

Corona Cruiser

Red Line:

- The Red Line provides service within Corona; via Target at Cajalco Road, Dos Lagos at Theater, Old Navy at The Crossings Shopping Center, Walmart at California Avenue, Centennial High School, Rimpau Avenue at Magnolia

Avenue, E. Grand Boulevard at E. Sixth Street, Corona Transit Center, Corona Library, Tenth Street at Lincoln Avenue, W. Sixth Street at Harris Street, and W. Sixth Street at Smith Avenue.

- The route traverses the city of Corona.
- During the weekday and weekend AM and PM peak hours, The Red Line has approximate headways between 60 and 70 minutes.

Blue Line:

- The Blue Line provides service within Corona; via Walmart at McKinley Street, Magnolia Avenue at McKinley Street, Magnolia Avenue at Rimpau Avenue, Magnolia Avenue at Fullerton Avenue, Mountain Gate Park, Corona Library, Corona Transit Center, Circle City Center, and River Run Apartments.
- The route traverses the city of Corona.
- During the weekday and weekend AM and PM peak hours, Route 44 has approximate headways between 60 and 70 minutes.

Per the city’s Traffic Division of the Public Works Department, the Corona Cruiser will be going through changes which include re-routing their bus lines. As a result, the current bus stop located on the west side of S. Main Street, north of W. Fourth Street will be vacated once the new Corona Cruiser route is in place, which is anticipated by mid next year. As a result, the Project will not be required to relocate this bus stop. In lieu of this, the City of Corona Public Works Department has conditioned the Project to provide a designated space for a micro transit stop.

The preceding information demonstrates the Project would not conflict with a program, plan, ordinance, or policy addressing the circulation system, including transit, roadway, bicycle, and pedestrian facilities. Therefore, no mitigation is required.

b. Conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)

Less than Significant Impact. Senate Bill (SB) 743 was signed by Governor Brown in 2013 and required the Governor’s Office of Planning and Research (OPR) to amend the CEQA Guidelines to provide an alternative to LOS for evaluating transportation impacts. SB 743 specified that the new criteria should promote the reduction of GHG emissions, the development of multimodal transportation networks and a diversity of land uses. In response, Section 15064.3 was added to the CEQA Guidelines that became effective on July 1, 2020, and requires that Vehicle Miles Traveled (VMT) be evaluated for impacts and provides lead agencies with the discretion to choose the most appropriate methodology and thresholds for its evaluation.

VMT Screening Thresholds

The City of Corona Vehicle Miles Traveled (VMT) Analysis Guidelines lists screening thresholds to determine if land use projects would require a VMT assessment. The City’s Guidelines also provide criteria for projects that could screen out of further analysis and would be considered to have a less-than significant impact on VMT. If a Project meets one of the criteria below, it is considered to have a less than significant impact on VMT and does not require further analysis.

1. The Project serves the local community.
2. The Project is located within a Transit Priority Area (TPA).
3. The Project is located in a low VMT generating Traffic Analysis Zone (TAZ).

The City’s VMT Analysis Guidelines were used in the evaluation of the Project VMT analysis. The VMT analysis determined, and the City of Corona’s Traffic Engineer confirmed, that the Project would meet Screening Criteria 1 and 2. According to the City’s Guidelines, projects that are located within an area that serves the local community and is located within a TPA would not be required to complete a VMT assessment. Since the proposed Project consists of local-serving retail land uses with no building greater than 50,000 square feet., this Project’s VMT is expected to be less than significant per CEQA Guidelines section 15064.3, subdivision (b). Therefore, no mitigation is warranted with respect to VMT.

c. Increase the total daily VMT per service population (population plus employment) (VMT/SP) above the baseline level for the jurisdiction

Less than Significant Impact. As described previously, the City of Corona Vehicle Miles Traveled (VMT) Analysis Guidelines lists screening thresholds to determine if land use projects would require a VMT assessment. The City’s Guidelines also provide criteria for projects that could screen out of further analysis and would be considered to have a less-than significant impact on VMT. The VMT analysis determined that the Project is located within an area that serves the local community and is located within a TPA, thus the Project does not require further VMT analysis. As such, impacts related to

VMT, including total daily VMT per service population would be less than significant. No mitigation is warranted.

d. Cause total daily VMT within the study area to be higher than the No Project alternative under cumulative conditions (General Plan Condition)

Less than Significant Impact. As mentioned previously, the VMT analysis determined that the Project meets Screening Criteria 1 and 2 and therefore does not require further VMT analysis. As such, impacts related to VMT would be less than significant and no mitigation is warranted.

e. Change in air traffic patterns

No Impact. The closest airport is Corona Municipal Airport which is approximately 2.1 miles northwest of the Project site. As illustrated in the Riverside County Airport Land Use Compatibility Plan for Corona Municipal Airport, the Project site is not located within any land use compatibility zones. As such, the Project would not obstruct or change air traffic patterns and no mitigation is required.

f. Traffic hazards from design features

Less than Significant Impact. The Project would develop and operate a 40,000 square foot grocery market and remodel of an existing 6,930 square foot bank building into a bank/restaurant use. None of the proposed structures would include incompatible uses such as farm equipment. The Project would also not increase any hazards related to a design feature. The onsite drives would be developed in conformance with City design standards. The City's construction permitting process includes review of Project plans to ensure that no potentially hazardous transportation design features would be introduced by the Project. For example, the design of the onsite circulation would be reviewed to ensure fire engine accessibility is provided to the fire code standards. Also, access to the Project site would be provided by five, 28-foot-wide driveways located at the project site's east, south and west perimeters which would provide vehicular access from S. Main Street, W. Sixth Street and S. Belle Avenue, respectively. The project entrances would be designed in compliance with the City's design standards to provide for adequate turning for passenger cars, fire trucks, and any maintenance or delivery vehicles. As a result, impacts related to geometric design features would be less than significant.

As part of the Project, no public improvements are proposed to W. Sixth Street, however, the Project site's south perimeter along Sixth Street requires an additional eight (8) feet of street dedication. On the Project's easter perimeter adjacent to S. Main Street, a 15-foot street dedication is required in addition to public right-of-way improvements consisting of parkway landscaping, sidewalk, street trees and curb and gutter. The City will vacate six (6) feet along the site's west perimeter adjacent to S. Belle Avenue, adding square footage to the Project's parking area. The Project will construct public right-of-way improvements consisting of new street trees along S. Belle Avenue adjacent to the project site, The existing sidewalk, curb and gutter along Belle Avenue will remain.

During construction, the City would require the Project Applicant to implement a temporary traffic control plan that complies with the applicable requirements of the California Manual on Uniform Traffic Control Devices (CMUTCD), which would preclude potential construction-related impacts during improvements to S. Main Street and S. Belle Avenue. Under long-term conditions, main access to the Project site will be provided via one (1) driveway on S. Main Street where Fifth Street is currently located, one (1) driveway on Belle Avenue across W. Fifth Street, one (1) driveway on S. Belle Avenue north of W. Sixth Street, and one (1) driveway each on W. Fourth Street and W. Sixth Street. The driveway on S. Main Street aligns with the North Corona Mall entrance located on the east side of Main Street to create a four-way intersection which will be signalized and allow for full access. The two (2) driveways on S. Belle Avenue will be full-access and unsignalized; the driveway on W. Fourth Street will be full-access and unsignalized; and the driveway on W. Sixth Street will be unsignalized and restricted to right in/out only due to an existing raised center median on Sixth Street.

Project Specific Improvements

The following Project design features are to be implemented in conjunction with development of the proposed Project to ensure adequate access and egress to the site is provided:

Main Street at 5th Street/Project Driveway No. 1:

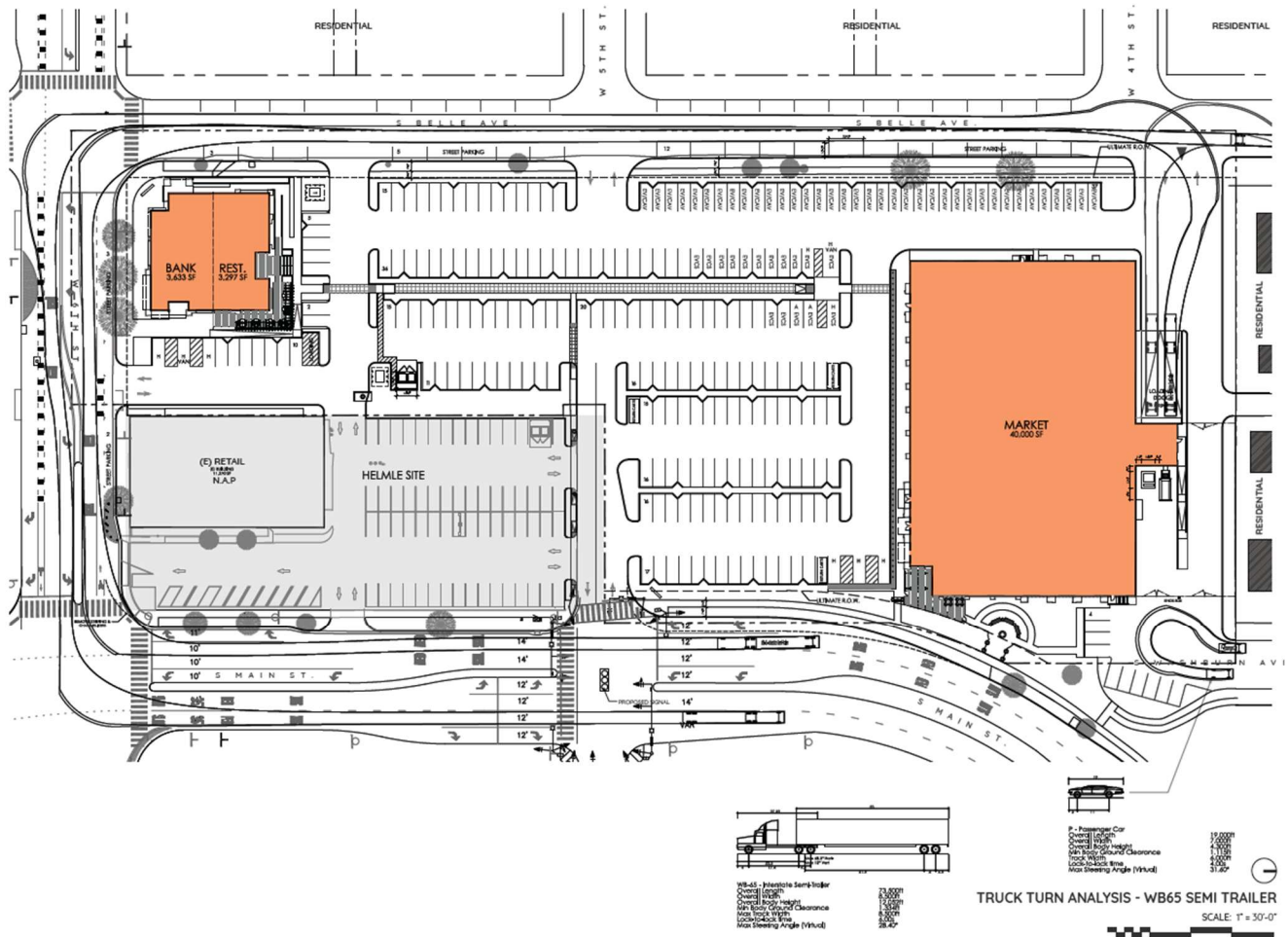
- Widen Main Street and restripe the north leg of the intersection to provide an exclusive southbound right-turn lane to provide vehicular access into the Project site.
- Install traffic signal and design for 5-phase operation with protected left-turn phasing for northbound and southbound left-turn movements.
- Stripe crosswalks on all legs, except the north leg, of the intersection.

Belle Avenue at 5th Street/Project Driveway No. 2:

- Convert the east leg of the intersection to a private driveway with abandonment of 5th Street between Belle Avenue and Main Street.

The Project’s truck turning template, see Figure 6-A demonstrates that the proposed driveways are adequately sized to accommodate truck turns into and out of the Project site. All proposed roadway improvements would be in full compliance with City of Corona Street Standards, and there are no components of the proposed Project that would result in increased hazards due to a design feature. As such, impacts would be less than significant and no mitigation is warranted.

FIGURE 6-A: PROJECT TRUCK TURNING TEMPLATE



g. Emergency access

No Impact. The proposed Project would develop and operate a grocery market, bank and restaurant that would be permitted and approved in compliance with existing safety regulations, such as the California Building Code and Fire Code (as integrated into the City’s Municipal Code) to ensure that it would not result in inadequate emergency access.

The proposed construction activities, including equipment and supply staging and storage, would occur within the Project site and would not restrict access of emergency vehicles to the Project site or adjacent areas. During construction, S. Main Street, W. Sixth Street and S. Belle Avenue would remain open to ensure adequate emergency access to the Project area and vicinity. Thus, impacts related to inadequate emergency access during construction activities would not occur.

As described above, operation of the proposed Project would also not result in inadequate emergency access. Direct access to the Project site would be provided from S. Main Street, W. Sixth Street and S. Belle Avenue. The driveways and on-site circulation constructed by the Project would be evaluated through the City’s permitting procedures to meet the City’s design standards that provide adequate turning space for passenger cars, fire trucks, and delivery trucks. The Project is also required to provide fire suppression facilities (e.g., hydrants and sprinklers). The Corona Fire Department (CFD) would review the development plans as part of the plan check and permitting procedures to ensure adequate emergency access pursuant to the requirements in Section 503 of the California Fire Code (Title 24, California Code of Regulations, Part 9). As a result, impacts related to inadequate emergency access would not occur.

h. Alternative transportation policies

No Impact. As described in Section 1, Land Use and Planning, the proposed development would be consistent with the policies and intent of the General Plan and would not conflict with alternative transportation policies. As evaluated in Section 6.a, Transportation/Traffic, the Project will provide connecting sidewalks and would not conflict with public transit or bicycle travel within the City. There would be no impact, and no mitigation is required.

7. BIOLOGICAL RESOURCES:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
a. Endangered or threatened species/habitat	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Riparian habitat or sensitive natural community	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Adversely affects federally protected wetlands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Interferes with wildlife corridors or migratory species	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e. Conflicts with local biological resource policies or ordinances	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f. Conflicts with any habitat conservation plan	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Discussion:

The following section is based on the Biological Technical Report (BTR) and Western Riverside County MSHCP (Multiple Species Habitat Conservation Plan) Consistency Analysis prepared by ECORP Consulting, Inc. dated February 2024, (Appendix G). The Biological Report documents the existing conditions at the project site and immediate vicinity and evaluates the potential for federally protected biological resources to occur on or immediately adjacent to the project site, including any federally listed species, federally protected waters and wetlands, and applicable federal laws and policies (e.g., Federal Endangered Species Act and the Migratory Bird Treaty Act) that apply to the proposed Project.

The purpose of the MSHCP Consistency Analysis is to document the proposed Project's consistency with the goals and objectives of the MSHCP. As the project site is located within the MSHCP area, the project must demonstrate consistency with the MSHCP requirements, including Section 6.1.2 (Protection of Species Associated with Riparian/Riverine Areas and Vernal Pools), Section 6.1.3 (Protection of Narrow Endemic Plant Species), Section 6.1.4 (Guidelines Pertaining to the Urban/Wildlands Interface), and Section 6.3.2 (Additional Survey Needs and Procedures), as applicable. It should be noted that the project site is not located within any MSHCP Criteria Cells; therefore, the project is not subject to the Joint Project Review process, nor Reserve Assembly requirements.

a. Endangered or threatened species/habitat

Potentially Significant Unless Mitigation Incorporated. Biological resources on the Project site were evaluated in the Biological Technical Report and MSHCP Consistency Analysis to ensure the proposed Project is consistent with the MSHCP and to analyze potential impacts to candidate, sensitive, and special-status species and associated habitat. Additionally, the BTR included a field survey conducted on November 21, 2023. The BTRM describes the Project site as consisting of disturbed, vacant land characterized by disturbed/developed areas.

The Project site is located within the boundaries of the Western Riverside County Multiple Species Conservation Plan (MSHCP). Therefore, the Project is required to demonstrate consistency with the MSHCP. The MSHCP consistency analysis identified that the Project site is not located within a MSHCP Criteria Cell or Cell Group. Further, the Project site is not located within plan-defined areas requiring surveys for criteria area species, narrow endemic species, amphibian species, or mammalian species, including burrowing owl. As part of the survey, the Project site was evaluated for the presence of native habitats that may support populations of sensitive wildlife and plant species. The property was also evaluated for the presence of sensitive habitats including wetlands, vernal pools, riparian habitats, and jurisdictional areas.

No special status plant or wildlife species, nor wetlands, vernal pools, riparian habitats or jurisdictional areas were observed during the biological reconnaissance (BTR/MSHCP Analysis, 2024).

The Project site is surrounded by commercial development to the east and south, and residential development to the north and west. Disturbances observed on the Project site include previous mechanical disturbances (e.g., ground disturbance via machinery), remnants of former development including broken cement and asphalt parking lots, trash, and nonnative plant species. Non-native vegetation present on the site was either planted for landscaping purposes or likely became established through human movement (e.g., Russian thistle [*Salsola australis*]). The northernmost part of the Project Site consists of a former residence that was either removed or fell into disrepair and currently consists of a dirt lot with a dilapidated shed. (BTR/MSHCP Analysis, 2024).

Special Status Species

Of the 42 special-status plant species identified in the literature search, all are presumed absent from the Project site. Therefore, no additional surveys or mitigation were required. Furthermore, the Project site is neither located in a Criteria Area nor a MSHCP-designated Narrow Endemic Plant Species Survey Area (NEPSSA). No impacts to special-status plant or Criteria/Narrow Endemic plant species are expected to occur as a result of the proposed Project.

Of the 54 special-status wildlife species identified in the literature search, one was determined to have a high potential to occur, one has a moderate potential to occur, one has a low to moderate potential to occur, and the remaining 51 species are presumed absent from the Project site. Cooper's hawk was found to have a high potential for occurrence and is considered an adequately conserved species under the MSHCP. The large gum trees and other ornamental trees located within the Project Site could provide suitable foraging and nesting habitat for this species, in addition to other nesting birds and raptors protected by the MBTA and California Fish and Game Code. Ground-disturbing construction activities could directly affect nesting birds and other birds protected by the MBTA and their nests through habitat removal on the Project Site, and indirectly through increased noise, vibrations, and increased human activity if any tree or vegetation removal needs to occur during the bird breeding season (January 15-September 15). Potential impacts to sensitive bird species and/or nesting birds would be less than significant with the implementation of **Mitigation Measure (MM) BIO-1**.

The project site is not in a designated burrowing owl survey area of the MSHCP. However, burrowing owl was found to have a low to moderate potential for occurrence. There is limited foraging and burrowing habitat on the project site although the vacant grass lots present within the project site could be suitable for the species. There were no California ground squirrels or burrows observed during the field survey conducted by the ECORP personnel; however, the vacant grass lots provide potential for small mammals, including ground squirrels to burrow. . Also, there are multiple records of burrowing owl occurring within 5 miles of the project site, with the closest occurrence from 2007 occurring less than a mile away. Therefore, due to the highly mobile nature of the species and the multiple recent occurrences within 5 miles of the Project Site, a preconstruction burrowing owl survey is recommended (**MM BIO-2**). With implementation of MM BIO-2, impacts to the burrowing owl would be less than significant.

Western yellow bat was found to have a moderate potential for occurrence. Any activities resulting in bat mortality (i.e., the destruction of an occupied bat roost that results in the death of bats), disturbance that causes the loss of a maternity colony of bats (resulting in the death of young), or various modes of nonlethal pursuit or capture may be considered take as defined in Section 86 of the California Fish and Game Code. Impacts to maternity roosting sites of any native bat species, regardless of status, may be considered a significant impact to a native wildlife nursery site under CEQA. In order to avoid potentially significant impacts to bats classified as SSC or to maternity colonies of non-SSC bats, a bat survey is recommended (**MM BIO-3**). With implementation of MM BIO-3, impacts to bats would be less than significant.

Additionally, the shrubs and trees on and immediately adjacent to the Project site could provide nesting habitat for nesting birds and raptors protected by the MBTA and California Fish and Game Code. If construction of the proposed Project occurs during the bird breeding season (January 15 - September 15), ground-disturbing construction activities could directly affect birds protected by the MBTA and their nests through the removal of habitat on the Project Site, and indirectly through increased noise, vibrations, and increased human activity. Impacts to nesting birds would be less than significant with the implementation of **MM BIO-1**.

b. Riparian habitat or other sensitive natural community

No Impact. Section 6.1.2 of the MSHCP defines Riparian/Riverine areas as "lands which contain habitat dominated by trees, shrubs, persistent emergents, or emergent mosses and lichens, which occur close to or which depend upon soil moisture from a nearby fresh water source; or areas with freshwater flow during all or a portion of the year." Riparian/Riverine areas as defined by the MSHCP were not present within the survey area, as identified in the BTR and the MSHCP Consistency Analysis, and therefore no impacts were identified.

c. Adversely affects federally protected wetlands

No Impact. Wetlands are defined under the federal Clean Water Act as land that is flooded or saturated by surface water or groundwater at a frequency and duration sufficient to support, and that normally does support, a prevalence of vegetation adapted to life in saturated soils. Wetlands include areas such as swamps, marshes, and bogs. No surface waters, wetlands, or riparian habitats were observed during the biological reconnaissance, and based on a review of the National Wetlands Inventory (NWI), there are no mapped wetlands on the study area (USFWS 2024c).

The Project site and adjacent areas are located within a developed urban area and do not contain natural wetlands as identified in the Biological Technical Report. Therefore, the Project would not result in impacts to wetlands.

d. Interferes with wildlife corridors or migratory species

Potentially Significant Unless Mitigation Incorporated. Wildlife corridors are areas where wildlife movement is concentrated due to natural or anthropogenic constraints and corridors provide access to resources such as food, water, and shelter. Animals use these corridors to move between different habitats and provide avenues for wildlife dispersal, migration, and contact between other populations. As mentioned previously, the Project site is disturbed and is surrounded by developed land uses. Further, no wildlife movement corridors were found to be present on the Project site nor does the Project site support conditions for migratory wildlife corridors or linkages (BTR, 2024). There are no rivers, creeks, or open drainages near the site that could function as a wildlife corridor. Thus, implementation of the Project would not result in impacts related to wildlife movement or wildlife corridors.

However, the Project site contains suitable roosting habitat and some ornamental trees that could provide nesting habitat for nesting bird species that are protected by the federal Migratory Bird Treaty Act (MBTA) and the California Fish and Game Code Sections 3503.5, 3511, and 3515 during the avian nesting and breeding season that occurs between January 15 and September 15. The provisions of the MBTA prohibit disturbing or destroying active nests. Therefore, **MM BIO-1** has been included. With implementation of **MM BIO-1**, potential impacts to nesting birds would be less than significant.

e. Conflict with local biological resource policies or ordinances

Less Than Significant Impact. The proposed Project would not conflict with any City of Corona ordinances or policies protecting biological resources. The Project would be subject to City of Corona Municipal Code Chapter 16.33 (Multiple Species Habitat Conservation Plan (MSHCP) Mitigation Fee), which requires a payment of a fee that is used for the acquisition and preservation of vegetation communities and natural areas known to support plant and wildlife species covered by the MSHCP. The Project also would not conflict with Section 12.22.080 (Heritage Trees) of the City's Municipal Code, as none of the existing trees on site comprise "Heritage" trees. Accordingly, no impact would occur.

f. Conflict with any habitat conservation plan

Potentially Significant Unless Mitigation Incorporated. The Project site is located within the boundaries of the MSHCP; therefore, it is subject to applicable provisions of the MSHCP as specified in response (a) above. The MSHCP provides for the assembly of a Conservation Area consisting of Core Areas and Linkages for the conservation of covered species. The Conservation Area is to be assembled from portions of the MSHCP Criteria Area, which consist of quarter-section (i.e., approximately 160-acre) Criteria Cells, each with specific criteria for the species conservation within that Cell. The Project site is not within the MSHCP Criteria Area; therefore, no Cell or Criteria analysis is required. No sensitive plant or sensitive/protected animal species were identified on-site during the field survey, and no on-site riparian or riverine areas were detected on the Project site. However, it is possible nesting birds may utilize the site at various times since ornamental trees and non-native grassland on the study area provide suitable foraging and nesting habitat for a number of resident native and migratory bird species protected under the MBTA.

Therefore, to reduce the potential project-related effects to nesting birds, **MM BIO-1** has been included to ensure any impacts to nesting birds are reduced to less than significant levels. In summary, implementation of the proposed Project would not conflict with the MSHCP; as such, impacts would be less than significant.

Mitigation Measures

MM BIO-1: Pre-Construction Nesting Bird Survey. If grading activities occur within the active breeding season for birds (January 15-September 15), the applicant shall retain a qualified biologist that is familiar with local birds and their nesting behaviors to conduct a nesting bird survey no more than 3 days prior to commencement of construction activities. The nesting bird survey shall be submitted to the City of Corona Planning and Development Department, Planning Division prior to issuance of a grading permit. The nesting bird survey shall include the Project Site and areas immediately adjacent to the site that could potentially be affected by Project-related construction activities, such as noise, human activity, and dust, etc. If active nesting of birds is observed within 100 feet of the designated construction area prior to construction, the qualified biologist shall establish an appropriate buffer around the active nests (e.g. 200 feet and/or subject to the

recommendations of the qualified biologist), and a biological monitor shall visit the site once a week during ground disturbing activities to ensure all fencing is in place and no nesting birds are being impacted.

MM BIO-2: Burrowing Owl Survey. Prior to the issuance of a grading permit, the applicant shall submit a 30-day preconstruction survey to the City of Corona Planning & Development Department, Planning Division to ensure that no burrowing owls have colonized the site in the days or weeks preceding Project activities. If burrowing owls are found to have colonized the Project Site prior to the initiation of construction, the Project applicant shall immediately inform the City of Corona Planning & Development Department, Western Riverside County Regional Conservation Authority (RCA) and the Wildlife Agencies prior to initiating ground disturbance. If ground-disturbing activities occur but the site is left undisturbed for more than 30 days, a preconstruction survey shall again be necessary to ensure burrowing owl has not colonized the site since it was last disturbed. If burrowing owl is found, the same coordination with the City of Corona, RCA and/or Wildlife Agencies shall be necessary.

MM BIO-3: Tree Avoidance and Removal Process. If trees are scheduled to be removed (e.g., relocating) and/or modified (i.e., trimming), the applicant shall retain a qualified bat biologist to determine if the trees are suitable for bat roosting. If the trees scheduled for removal are determined to be suitable for bat roosting, these activities shall be scheduled during seasonal periods of bat activity - September 1 to October 15 - or when evening temperatures are above 45 degrees Fahrenheit, and rain is less than 1/2 inch in 24 hours; or between March 1 to April 1 with the same parameters.

1. If tree removal/modification occurs during the maternity season (generally April 15 to August 31), a qualified bat biologist shall conduct a focused emergence survey(s) of the tree(s) within 48 hours of scheduled work. If a maternity roost is located, whether solitary or colonial, that roost shall remain undisturbed until after the maternity season or until a qualified biological monitor has determined the roost is no longer active.
2. If work is expected to occur outside of the bat maternity season, work adjacent to trees suitable as bat habitat can continue without additional surveying efforts. If trees with suitable bat roosting habitat are scheduled for removal or relocation, tree removal during the weather parameters described above using the two-step method shall be conducted:
 - As much as feasible, vegetation and trees within the area that are not suitable for roosting bats shall be removed first to provide a disturbance that might reduce the likelihood of bats using the habitat. Two-step tree removal shall occur over two consecutive days under the supervision of a qualified bat biologist. On Day 1, small branches and small limbs containing no cavity, crevice, or exfoliating bark habitat on habitat trees (or outer fronds in the case of palm trees), as identified by a qualified bat biologist are removed first, using chainsaws only (i.e., no dozers, backhoes). The following day (Day 2), the remainder of the tree is to be felled/removed. The intention of this method is to disturb the tree with noise and vibration on Day 1 during branch removal. This should cause any potentially present day-roosting bats to abandon the roost tree after they emerge for nighttime foraging. Removing the tree quickly the next consecutive day should avoid reoccupation of the tree by bats.

8. MINERAL RESOURCES:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
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a. Loss of mineral resource or recovery site	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
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Discussion:

a. Loss of mineral resource or recovery site

No Impact. According to the California Department of Conservation (CDOC), the Project site is in an area generally classified as Sand and Gravel Resource Area and Gravel Resource Areas. Although the region is classified for these resources, the Project site is not currently or planned for mineral extraction. Additionally, according to the City of Corona's General Plan 2020-2040, mineral extraction has been a part of Corona's history since 1888, when the Temescal Rock Quarry was opened to furnish rock for streets in Los Angeles and other nearby towns. Mineral resources found in the City of Corona have included crushed rock, sand, and gravel and small amounts of silver, lead, zinc, coal, and gypsum. The

Project site is in an area classified as Mineral Resource Zone 4 (MRZ-4) which includes areas where available information is inadequate for assignment to any other zone. Therefore, minerals may be present, but information is not available to make a determination. However, the Project site is not currently used or planned for mineral extraction. As such, the Project would result in no impact.

9. HAZARDS AND HAZARDOUS MATERIALS:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
a. Transport, use or disposal of hazardous materials	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Risk of accidental release of hazardous materials	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Hazardous materials/emissions within ¼ mile of existing or proposed school	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Located on hazardous materials site	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Conflict with Airport land use plan	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Impair emergency response plans	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g. Increase risk of wildland fires	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

The following section is based on the Phase I Environmental Site Assessments (ESAs) prepared by EEC Environmental, dated October 18, 2021 and November 29, 2021, and the corresponding Limited Subsurface Assessments also prepared by EEC Environmental dated December 9, 2021 and January 24, 2022, respectively; additionally, this section is based on the Phase 1 ESA prepared by Ramboll, dated May 2025. All documents are included as Appendix H.

a. Transport, use, or disposal of hazardous materials

Less than Significant Impact. A hazardous material is defined as any material that, due to its quantity, concentration, or physical or chemical characteristics, poses a significant present or potential hazard to human health and safety or to the environment if released into the workplace or environment. Hazardous materials include, but are not limited to, hazardous substances, hazardous wastes, and any material that a business or the local implementing agency has a reasonable basis for believing would be injurious to the health and safety of persons or harmful to the environment if released into the workplace or the environment. Hazardous wastes require special handling and disposal because of their potential to damage public health and the environment.

Construction

The proposed project includes the construction of a 40,000 square foot Northgate Gonzalez Market and remodel of an existing 6,930 square foot vacant Citizens bank building into a bank/restaurant. Construction activities would be temporary and could involve transport, storage, and use of chemical agents, solvents, paints, and other hazardous materials commonly associated with construction activities. These types of materials are not acutely hazardous, and all storage, handling, use, and disposal of these materials are regulated by federal and state requirements that are implemented by the city during building permitting for construction activities. These regulations include: the federal Occupational Safety and Health Act and

Hazardous Materials Transportation Act; Title 8 of the California Code of Regulations (CalOSHA), and the state Unified Hazardous Waste and Hazardous Materials Management Regulatory Program. As a result, routine transport and use of hazardous materials during construction would be less than significant.

Operation

The Project would involve the operation of a 40,000 square foot Northgate Gonzalez Market and 6,930 square foot commercial use. The Project would be subject to compliance with applicable federal, state, and local laws (including Title 49 of the CFR) and regulations pertaining to the transport, use, disposal, handling, and storage of hazardous waste. Future tenants of the proposed Project would be required to comply with existing regulations, standards, and guidelines established by the US Environmental Protection Agency, State of California, County of Riverside, Corona Fire Department and City of Corona related to storage, use, and disposal of hazardous materials, which would reduce the potential risk of hazardous materials exposure. Therefore, operation of the Project would not result in a significant hazard to the public or to the environment through the routine transport, use, or disposal of hazardous waste, and impacts would be less than significant.

b. Risk of accidental release of hazardous materials

Less than Significant Impact. The Phase I Environmental Site Assessments (ESAs) prepared by EEC Environmental, dated October 18, 2021 and November 29, 2021, recommended limited subsurface assessments to address recognized environmental conditions (RECs). The Limited Subsurface Assessments (LSAs) prepared by EEC Environmental dated December 9, 2021, and January 24, 2022, respectively, concluded that the identified RECs do not appear to have impacts on the subsurface that would result in regulatory action or will affect the future use or redevelopment of the site, based on the site remaining as a commercial/industrial property. Based on the findings of both LSAs, EEC recommended no further investigation be performed at this time. A separate Phase I Environmental Site Assessment (ESA) was conducted for the Project site by Ramboll, dated May 2025. This Phase I ESA did not identify any recognized environmental conditions (RECs), controlled RECs, historic RECs or De Minimis Conditions.

The Phase I ESA dated October 18, 2021, for 225 W. Sixth Street, identified that the Project site was developed with a single family home in 1895. From 1911 through the 1970s the site was developed with a lumber mill company, auto repair shop, machine shop, gas station and various commercial uses. By 1981, the subject property was redeveloped with the existing bank building, drive-up ATM, and parking lot.

The Phase 1 ESA dated November 29, 2021, for four parcels (410 S. Main Street, 323 S. Belle Avenue, 332 S. Washburn Avenue and a vacant lot) identified that the Project site was developed was previously developed with industrial buildings that were occupied by a machine shop and other metalworking companies from the 1930s to early 1990s. From the mid-1990s to 2000s, the buildings on the south portion of the subject property were occupied by a sign and lighting company, carpet shop, and drapery and upholstery shop. The buildings on the south portion of the subject property were demolished in 2013 and only building pads remain. The north portion of the subject property is now vacant as two single family homes were recently demolished.

The Phase I ESA dated May 2025, for eight parcels (six on South Belle Ave., one at 450 S. Main St. and one at 215 W. Fifth St.). The site was used for residential and agricultural purposes since at least the 1890s through the 1980s and 2000s, when the residences were demolished. A strip mall was constructed on the eastern half of the site in the mid-to late-1980s. The current site building has been used for commercial (retail/office/restaurant) operations since its construction. The site building is currently occupied by four tenants including a collectibles store (Mancave Museum), a beauty salon (Carolina's Beauty Salon), a restaurant (Super Taco), and a liquor store (Jashua Liquor), as well as a vacant space.

Construction

Accidental Releases. While the routine use, storage, transport, and disposal of hazardous materials in accordance with applicable regulations during construction activities would not pose health risks or result in significant impacts; improper use, storage, transportation and disposal of hazardous materials and wastes could result in accidental spills or releases, posing health risks to workers, the public, and the environment. To avoid an impact related to an accidental release, the use of BMPs during construction are implemented as part of a SWPPP as required by the National Pollution Discharge Elimination System General Construction Permit. Implementation of an SWPPP would minimize potential adverse effects to workers, the public, and the environment. Construction contract specifications would include strict on-site handling rules and BMPs that include, but are not limited to:

- Establishing a dedicated area for fuel storage and refueling and construction dewatering activities that includes secondary containment protection measures and spill control supplies;
- Following manufacturers' recommendations on the use, storage, and disposal of chemical products used in construction;
- Avoiding overtopping construction equipment fuel tanks;

- Properly containing and removing grease and oils during routine maintenance of equipment; and
- Properly disposing of discarded containers of fuels and other chemicals.

Operation

As described previously, the Project would involve the operation of a 40,000 square foot Northgate Gonzalez Market and 6,930 square foot commercial use. The Project would be subject to compliance with applicable federal, state, and local laws (including Title 49 of the CFR) and regulations pertaining to the transport, use, disposal, handling, and storage of hazardous waste. Future tenants of the proposed Project would be required to comply with existing regulations, standards, and guidelines established by the US Environmental Protection Agency, State of California, County of Riverside, Corona Fire Department and City of Corona related to storage, use, and disposal of hazardous materials, which would reduce the potential risk of hazardous materials exposure. Therefore, operation of the Project would not result in a significant hazard to the public or to the environment through the routine transport, use, or disposal of hazardous waste, and impacts would be less than significant.

c. Hazardous materials/ emissions within one-quarter mile of an existing or proposed school

Less than Significant Impact. The nearest school to the Project site is St. Edward's Catholic Church located within .16 miles to the west of the Project site. However, as described previously, construction and operation of the Project would involve the use, storage, and disposal of small amounts of hazardous materials on the Project site. These hazardous materials would be limited and used and disposed of in compliance with federal, state, and local regulations, which would reduce the potential for accidental release into the environment near a school. The emissions that would be generated from construction and operation of the Project were evaluated in the air quality analysis discussed above, and the emissions generated from the Project would not cause or contribute to an exceedance of the federal or state air quality standards. Thus, the Project would not emit hazardous or handle acutely hazardous materials, substances, or waste near a school, and impacts would be less than significant.

d. Located on a hazardous materials site

Less Than Significant Impact. Government Code § 65962.5 requires the Department of Toxic Substances Control (DTSC) to compile and update these lists, at least annually. These lists are collectively referred to as the "Cortese List" and contain the following:

- Hazardous waste and substances sites from the DTSC EnviroStor database.
- Leaking Underground Storage Tank (LUST) sites by county and fiscal year in the State Water Resources Control Board (SWRCB) GeoTracker database.
- Solid waste disposal sites identified by SWRCB with waste constituents above hazardous waste levels outside waste management units.
- SWRCB Cease and Desist Orders (CDOs) and Cleanup and Abatement Orders (CAOs).
- Hazardous waste facilities are subject to corrective action pursuant to § 25187.5 of the Health and Safety Code, identified by DTSC.

EEC contracted EDR to perform a regulatory database review for the subject property and surrounding properties up to 1.0 mile from the subject property. The purpose of the database review is to identify sites of potential environmental concern, such as leaking underground storage tank (LUST) sites; spills, leaks, investigation, and cleanup (SLIC) sites; historical gasoline service stations and dry cleaners; hazardous waste sites; landfills; and sites that are currently under investigation for environmental violations. Within the Phase I ESA dated October 18, 2021, the historical subject property addresses of 217-219 W. 6th Street were identified on the EDR Hist Auto database. According to the database, the following auto repair companies were identified on the subject property: Star Garage & Machine Works in 1924, Fink S. Garage and Machine Shop in 1927, and Smith J. C. in 1939. No other information was provided in the listings. EEC evaluated all adjoining or nearby sites listed on the database for their potential to impact the subject property.

The Phase 1 ESA for 225 W. Sixth Street revealed the following RECs in connection with the subject property.

- Potential for undocumented releases as a result of the former use of the subject property as a lumber and milling company, gasoline service station, auto repair garage, machine shop, and laundromat.
- Potential impacts to the subject property from former adjacent gasoline service stations and former nearby dry cleaner.

Based on the results of this assessment, the Phase 1 ESA recommended a limited subsurface assessment to address the onsite and offsite RECs. The Limited Subsurface Assessment (LSA) prepared by EEC Environmental dated December 9, 2021, concluded that the identified RECs do not appear to have impacts on the subsurface that would result in regulatory action or would affect the future use or redevelopment of the site, based on the site remaining as a commercial/industrial property. Therefore, based on the findings of the LSA, EEC recommended no further investigation be performed at this time.

Within the Phase 1 ESA dated November 29, 2021, the subject four parcels were not identified within the Cortese list and the addresses were not in any of the databases searched. EEC evaluated all adjoining or nearby sites listed on the database for their potential to impact the subject property. With the exception of a former dry cleaner located at 310 S. Main St., the previous surrounding uses are not expected to represent an environmental concern. This assessment revealed the following REC in connection with the subject property.

- Potential for undocumented releases as a result of the former use of the subject property as a machine shop and metalworking facility.

Additionally, this assessment identified the following offsite REC:

- Potential impacts to the subject property from a nearby dry cleaner.

Based on the results of this assessment, EEC recommended a limited subsurface assessment to address the onsite and offsite RECs. The Limited Subsurface Assessment (LSA) prepared by EEC Environmental dated January 24, 2022, concluded that the identified REC does not appear to have impacts on the subsurface that would result in regulatory action or would affect the future use or redevelopment of the site, based on the site remaining as a commercial/industrial property. Therefore, based on the findings of the LSA, EEC recommended no further investigation be performed at this time.

As a result, impacts related to hazards from being located on or adjacent to a hazardous materials site would be less than significant. Therefore, no mitigation is required.

e. Conflict with an airport land use plan

No Impact. The closest airport is the Corona Municipal Airport, which is approximately 2.10 miles southeast of the Project site. The Project site is not located within any land use compatibility zone for the nearest airport, nor is it within an airport safety zone. Therefore, the Project would not result in a safety hazard for people residing or working in the Project areas, and no impacts would occur. No mitigation is required.

f. Impair emergency response plans

Less than Significant Impact.

Construction

The proposed construction activities, including equipment and supply staging and storage, would occur within the Project site and would not restrict access of emergency vehicles to the Project site or adjacent areas. During construction of the Project driveway, S. Main Street, W. Sixth Street and S. Belle Avenue would remain open to ensure adequate emergency access to the Project area and vicinity. Impacts related to interference with an adopted emergency response or evacuation plan during construction activities would be less than significant. Therefore, no mitigation is required.

Operation

Operation of the proposed Project would not result in a physical interference with an emergency response evacuation. Direct access to the Project site would be provided from S. Main Street and W. Sixth Street, both are 4-lane roadways, and from S. Belle Avenue, a 2-lane roadway, that is adjacent to the Project site. The interior roadway would be designed to accommodate fire department access in coordination with the City fire authorities and would be a minimum of 28 feet wide. The Project is also required to design and construct internal access and provide fire suppression facilities (e.g., hydrants and sprinklers) in conformance with the City Municipal Code and the Fire Department prior to approval to ensure adequate emergency access pursuant to the requirements in Section 503 of the California Fire Code (Title 24, California Code of Regulations, Part 9) included as Chapter 15.12 in the City's Municipal Code. As a result, the proposed Project would not impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan, and impacts would be less than significant. Therefore, no mitigation is required.

g. Increase risk of wildland fires

No Impact. According to the California Fire Hazard Severity Zones mapping, the Project site is not within a Very High Fire Hazard Severity Zone. Additionally, the Project site is located within an urbanized area, with development surrounding the

project site on all four sides, and development of the site with residential uses would not result in impacts related to the exposure of people or structures to loss, injury, or death involving wildland fires. Therefore, no impacts would occur. Refer to additional wildfire analysis under Section 19, Wildfire.

10. NOISE:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
a. Exceed noise level standards	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Exposure to excessive noise levels/vibrations	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Permanent increase in ambient noise levels	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Temporary increase in ambient noise levels	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Conflict with Airport Land Use Plan noise contours	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

The discussion below is based on the Noise Impact Assessment prepared by ECORP Consulting, Inc., dated November 2024 (Appendix I). The following noise regulatory setting includes local, state, and federal standards applicable to the Project site.

Existing Ambient Noise Levels

As detailed in the Noise Impact Assessment, in order to quantify existing ambient noise levels in the Project Area, ECORP Consulting, Inc. conducted five short-term noise measurements (15-minutes) on the morning of March 20, 2024. These short-term noise measurements are representative of typical existing noise exposure within and immediately adjacent to the Project Site during the daytime (see Figure 13 for a visual representation of the measurement locations). The average noise levels at each location are listed in Table 10-A.

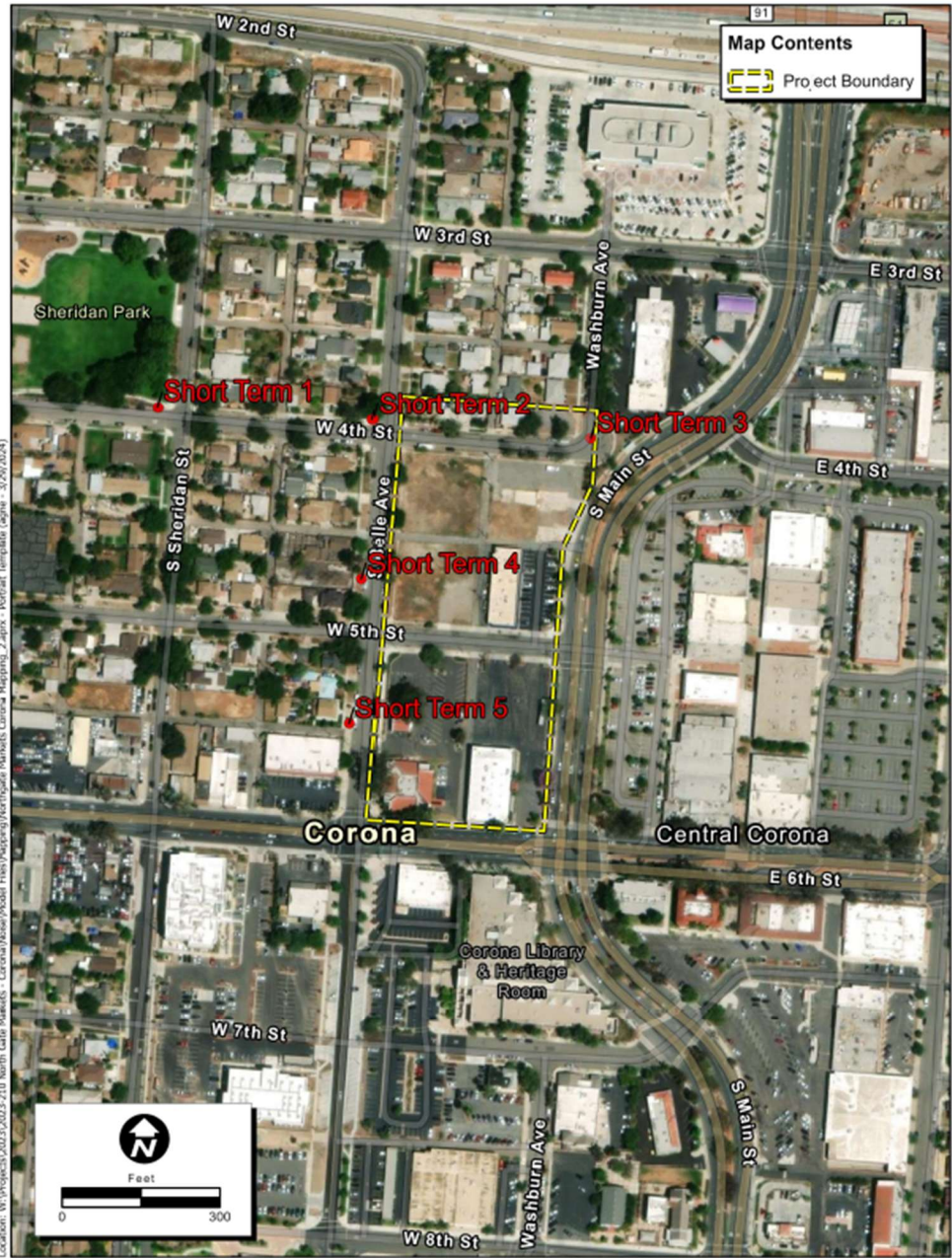
TABLE 10-A: EXISTING AMBIENT NOISE MEASUREMENTS

Location Number	Location	Leq dBA	Lmin dBA	Lmax dBA	Time
1	Northwest corner of 4 th Street and Sheridan Street	56.2	52.7	71.9	10:24 a.m. – 10:39 a.m.
2	Northwest intersection of 4 th Street and Belle Avenue	58.5	53.8	72.4	10:44 a.m. – 10:59 a.m.
3	Northwest intersection of 4 th Street and Washburn Avenue	60.8	57.5	75.7	11:01 a.m. – 11:16 a.m.
4	On sidewalk west of Belle Avenue, 50 feet north of 5 th Street	67.9	54.3	81.8	11:20 a.m. – 11:35 a.m.
5	Parkway west of Belle Avenue, approximately 200 feet north of 6 th Street	58.1	53.7	68.8	11:37 a.m. – 11:53 a.m.

Source: Measurements were taken by ECORP with a Larson Davis SoundExpert LxT precision sound level meter, which satisfies the American National Standards Institute for general environmental noise measurement instrumentation. Prior to the measurements, the SoundExpert LxT sound level meter was calibrated according to manufacturer specifications with a Larson Davis CAL200 Class I Calibrator. See Attachment A for noise measurement outputs.

Notes: Leq is the average acoustic energy content of noise for a stated period of time. Thus, the Leq of a time-varying noise and that of a steady noise are the same if they deliver the same acoustic energy to the ear during exposure. Lmin is the minimum noise level during the measurement period and Lmax is the maximum noise level during the measurement period.

FIGURE 13: NOISE MEASUREMENT LOCATIONS



Baseline Noise Measurement Locations



2023-210 Northgate Markets Project

As shown in Table 10-A, the ambient recorded noise levels range from 56.2 dBA to 67.9 dBA Leq over the course of the five short-term noise measurements taken in the Project vicinity in March of 2024. The most common noise in the Project vicinity is produced by automotive vehicles (e.g., cars, trucks, buses, motorcycles) on the local surrounding roadways such as Main Street and Belle Avenue.

City of Corona General Plan Noise Element

The Noise Element of the General Plan provides policy direction for minimizing excessive noise within the community and aims to protect residents, visitors, and noise-sensitive land uses from health impacts. By identifying noise-sensitive land uses and establishing noise level compatibility guidelines for land uses, noise considerations will influence the general distribution, location, and intensity of future land uses. The result is that effective land use planning and mitigation can alleviate the majority of noise problems. The following goals and policies have been identified to be applicable to the Proposed Project:

Goal N-1: Protect residents, visitors, and noise-sensitive land uses from the adverse human health and environmental impacts created by excessive noise levels from transportation sources by requiring proactive mitigation.

Policy N-1.1: Reduce noise impacts from transportation noise sources through the design and daily operation of arterial road improvements, enforcement of state motor vehicle noise standards, and other measures consistent with funding capabilities.

- Require site design features and structural building enhancements in the development of residential and other “noise sensitive” land uses that are to be located adjacent to major roads or railroads.

Policy N-1.5: Require new nonresidential development that attracts noise-generating vehicles (e.g., high volumes of traffic, trucking) to design and configure onsite ingress and egress points to divert traffic away from “noise sensitive” land uses, to the greatest extent practicable.

Goal N-2: Prevent and mitigate the adverse impacts of excessive ambient noise exposure, including vibration on residents, employees, visitors, and “noise sensitive” land uses.

Policy N-2.1: Consider noise and vibration levels in land use planning decisions to prevent future noise and vibration and land use incompatibilities. Considerations may include, but not necessarily be limited to, standards that specify acceptable noise limits for various land uses noise reduction features, acoustical design in new construction, and enforcement of the California Standards Building Code provisions for indoor and outdoor noise levels.

Policy N-2.3: Require new industrial and new commercial land uses or the major expansion of such uses to demonstrate that ambient noise levels will not exceed an exterior noise level of 65 dBA CNEL on areas containing “noise sensitive” land uses as depicted in [Table 4-1].

Policy N-2.6: Require development that generates increased traffic and substantial increases in ambient noise levels adjacent to noise sensitive land uses to provide appropriate mitigation measures in accordance with the acceptable limits of the City Noise Ordinance.

Policy N-2.7: Require construction activities that occur in close proximity to existing “noise sensitive” uses, including schools, libraries, health care facilities, and residential uses, to limit the hours and days of operation in accordance with the City Noise Ordinance.

Goal N-3: Discourage the spillover or encroachment of unacceptable noise levels from mixed use, commercial, and industrial land uses on to noise sensitive land uses.

Policy N-3.1: Provide for the reduction in noise impacts from commercial and industrial operations as controlled and enforced through the City Noise Ordinance.

Policy N-3.2: Incorporate noise reducing designs into new or remodeled commercial and industrial projects. Measures should include, but not be limited, to:

- Sound barriers in front of HVAC units and other similar outdoor mechanical equipment.
- Increase setbacks and buffering of parking areas and primary on-site access drives from adjacent residential areas and other sensitive uses to the maximum extent feasible with walls, fences, berms, and/or adequate landscaping.
- Require vehicle access to commercial or industrial land uses abutting existing or planned residential areas be located at the maximum practical distance from residential areas.
- Orient loading and unloading ramps and drop off zones away from noise sensitive land uses.

Policy N-3.4: Require that restaurants/bars implement operational measures to control the activities of their patrons on-site and within a reasonable distance from the establishment in order to minimize potential noise-related impacts on adjacent residential neighborhoods.

Policy N-3.5: Require mixed-use structures incorporating commercial or institutional and residential uses, or industrial uses adjacent to noise and vibration sensitive uses minimize, through design and construction technology, the transfer or transmission of noise and vibration from the commercial, institutional, or industrial use to the residential land use.

The Noise Element contains standards for interior and exterior noise for a variety of land uses, shown in Table 10-B below.

TABLE 10-B: CITY OF CORONA INTERIOR AND EXTERIOR NOISE STANDARDS

Land Use Categories		Average CNEL	
Categories	Uses	Interior ¹	Exterior ²
Residential	Single Family, Duplex, Multiple Family	45 ³	65
	Mobile Home	NA	65 ⁴
Commercial; Industrial; and Institutional	Hotel, Motel, Transient Lodging	45	65 ⁵
	Commercial Retail, Bank, Restaurant; Sports Club	55	NA
	Office Building, Research and Develop., Professional Offices, City Offices	50	NA
	Amphitheatre, Concert Hall Auditorium, Meeting Hall	45	NA
	Gymnasium (Multipurposed)	50	NA
	Manufacturing, Warehouse, Wholesale, Utilities	65	NA
	Movie Theatres	45	NA
Institutional	Hospital, Schools' classroom	45	65
	Church, Library	45	NA
	Parks	NA	65

Source: City of Corona General Plan 2020.

Notes:

¹ Indoor environment excluding bathrooms, toilets, closets, corridors.

² Outdoor environment limited to: private yard of single family, multi-family private patio or balcony that is served by a means of exit from inside, mobile home park, hospital patio, park's picnic area, school's playground, and hotel and motel recreation area.

³ Noise level requirement with closed windows. Mechanical ventilating system or other means of natural ventilation shall be provided as of Chapter 12, Section 1205 of the Uniform Building Code (UBC).

⁴ Exterior noise level should be such that interior noise level will not exceed 45 CNEL.

⁵ Except those areas affected by aircraft noise.

City of Corona Municipal Code

Noise Standards

The City's standards for noise impacts in neighboring residential areas are found in Chapter 17.84.040 of the City's Municipal Code, which sets forth exterior and interior noise limits of 65 dBA CNEL and 45 dBA CNEL, respectively, for transportation noise sources, such as roadway and airport, at residential and other sensitive land uses. Performance standards for stationary noise sources are summarized in Table 10-C.

TABLE 10-C: STATIONARY NOISE STANDARDS

Types of Land Use	Maximum Allowable Noise Levels			
	Exterior Noise Level (L)		Interior Noise Level (L)	
	7:00 a.m. to 10:00 p.m.	10:00 p.m. to 7:00 a.m.	7:00 a.m. to 10:00 p.m.	10:00 p.m. to 7:00 a.m.
Single-, Double- and Multi-Family Residential	55 dBA	50 dBA	45 dBA	35 dBA
Other Sensitive Land Uses	55 dBA	50 dBA	45 dBA	35 dBA
Commercial Uses	65 dBA	60 dBA	-	-
Industrial, Manufacturing, Agricultural	75 dBA	70 dBA	-	-

Construction Noise Standards

The City has set restrictions to control noise impacts associated with the construction of the proposed Project. According to Section 17.84.040(D)(2), Construction noise, construction noise is prohibited between the hours of 8:00 p.m. to 7:00 a.m., Monday through Saturday, and 6:00 p.m. to 10:00 a.m. on Sundays and federal holidays. Construction noise is defined as noise, which is disturbing, excessive or offensive and constitutes a nuisance involving discomfort or annoyance to persons

of normal sensitivity residing in the area, which is generated by the use of any tools, machinery or equipment used in connection with construction operations.

Operational Noise Standards

The City of Corona Municipal Code, Section 17.84.040, *Noise*, provides noise control guidelines for determining and mitigating non-transportation or stationary-source noise impacts from operations at private properties. The City of Corona Municipal Code defines *Stationary Noise Source Standards* in Section 17.84.040(C)(2), Table 1, for different land uses. For noise-sensitive residential properties, the Municipal Code identifies operational noise level limits for the daytime hours (7:00 a.m. to 10:00 p.m.) and for the nighttime hours (10:00 p.m. to 7:00 a.m.). Refer to Table 4-2 (Stationary Noise Standards) above.

The noise levels, as shown in Table 10-C, when measured on any adjacent property, shall not exceed:

- a. The noise standard for a cumulative period of more than 30 minutes in any hour;
- b. The noise standard for plus 5 dB for a cumulative period of more than 15 minutes in any hour;
- c. The noise standard for plus 10 dB for a cumulative period of more than 5 minutes in any hour;
- d. The noise standard for plus 15 dB for a cumulative period of more than 1 minute in any hour;
- e. The noise standard plus 20 dB for any period of time.

Refer to Table 10-D.

TABLE 10-D: OPERATIONAL NOISE STANDARDS

Jurisdiction	Land Use	Time Period	Exterior Noise Level Standards (dBA Leq) ²				
			L ₅₀ (30 mins)	L ₂₅ (15 mins)	L ₈ (5 mins)	L ₂ (1 min)	L _{max} (Anytime)
City of Corona ¹	Residential	Daytime	55	60	65	70	75
		Nighttime	50	55	60	65	70
	Commercial	Daytime	65	70	75	80	85
		Nighttime	60	65	70	75	80
	Industrial	Daytime	75	80	85	90	95
		Nighttime	70	75	80	85	90

¹ City of Corona Municipal Code, Section 17.84.040 Noise (Appendix 3.1).

² The percent noise level is the level exceeded "n" percent of the time during the measurement period. L50 is the noise level exceeded 50% of the time.

"Daytime" = 7:00 a.m. to 10:00 p.m.; "Nighttime" = 10:00 p.m. to 7:00 a.m.

Construction Vibration Standards

To analyze the vibration impacts originating from the construction of the Project, vibration from construction activities is typically evaluated against standards established under a City’s Municipal Code. The City of Corona Municipal Code, Section 17.84.050, identifies a vibration velocity standard of 0.05 in/sec root-mean-square (RMS) for sensitive land uses which is used in this analysis as the basis for determining the relative significance of potential Project related vibration impacts. Typically, the human response at the perception threshold for vibration includes annoyance in residential areas as previously shown on Exhibit 2-B, when vibration levels expressed in vibration decibels (VdB) approach 75 VdB. The City of Corona, however, identifies a vibration perception threshold of 0.05 in/sec at any point on the affected property. For vibration levels expressed in velocity, the human body responds to the average vibration amplitude often described as the root-mean-square (RMS). Therefore, the City of Corona vibration standard of 0.05 in/sec in RMS velocity levels is used in this analysis to assess the human perception of vibration levels due to Project-related construction activities.

a. Exceed noise level standards

Less than Significant Impact. As described above, City of Corona Municipal Code Section 17.84.040 prohibits construction noise between the hours of 8:00 p.m. and 7:00 a.m., Monday through Saturday, and 6:00 p.m. to 10:00 a.m. on Sundays and City observed federal holidays. The Project would comply with the City’s construction hours regulations, as required by standard City Conditions of Approval. Construction activities are anticipated to last approximately 24 months.

Onsite Construction Noise

Construction noise associated with the Proposed Project would be temporary and would vary depending on the specific nature of the activities being performed. Noise generated would primarily be associated with the operation of off-road equipment for onsite construction activities as well as construction vehicle traffic on area roadways. Construction noise typically occurs intermittently and varies depending on the nature or phase of construction (e.g., site preparation, excavation,

paving). Noise generated by construction equipment, including earth movers, pile drivers, and portable generators, can reach high levels. During construction, exterior noise levels could negatively affect sensitive land uses in the vicinity of the construction site.

The nearest sensitive receptors to the Project Area include residences directly adjacent to the north of the Project Site. There are also sensitive residential receptors located directly across S. Belle Avenue to the west. As previously mentioned, the Municipal Code’s Section 17.84.040 prohibits construction noise between the hours of 8:00 p.m. to 7:00 a.m., Monday through Saturday and 6:00 p.m. to 10:00 a.m. on Sundays and federal holidays. In order to remain compliant with the City’s regulations, the Proposed Project would be required to follow these construction guidelines. The city does not promulgate a numeric threshold pertaining to the noise associated with construction. This is due to the fact that construction noise is temporary, short term, intermittent in nature, and would cease on completion of the Project.

To estimate the worst-case onsite construction noise levels that may occur at the nearest noise-sensitive receptor and in order to evaluate the potential health-related effects (physical damage to the ear) from construction noise, the construction equipment noise levels were calculated using the Roadway Noise Construction Model and compared against the construction-related noise level threshold established in the Criteria for a Recommended Standard: Occupational Noise Exposure prepared in 1998 by National Institute for Occupational Safety and Health (NIOSH). A division of the US Department of Health and Human Services, NIOSH identifies a noise level threshold based on the duration of exposure to the source. The NIOSH construction-related noise level threshold starts at 85 dBA for more than 8 hours per day; for every 3-dBA increase, the exposure time is cut in half. This reduction results in noise level thresholds of 88 dBA for more than 4 hours per day, 92 dBA for more than 1 hour per day, 96 dBA for more than 30 minutes per day, and up to 100 dBA for more than 15 minutes per day. For the purposes of this analysis, the lowest, more conservative threshold of 85 dBA Leq is used as an acceptable threshold for construction noise at the nearby sensitive receptors.

It is acknowledged that the majority of construction equipment is not situated at any one location during construction activities but rather spread throughout the Project Site and at various distances from sensitive receptors. Therefore, this analysis employs FTA guidance for calculating construction noise, which recommends measuring construction noise produced by all construction equipment simultaneously from the center of the Project Site (FTA 2018), which in this case is approximately 213 feet from the closest single family home west of the Project Site, along S. Belle Avenue. The anticipated short-term construction noise levels generated for the necessary equipment for each phase of construction are presented in Table 10-E.

TABLE 10-E: CONSTRUCTION NOISE LEVELS AT THE NEAREST RECEPTORS

Construction Phase	Average Ambient Noise Level* (dBA Leq)	Exterior Construction Noise Level @ Closest Noise Sensitive Receptor (dBA Leq)	Existing Ambient Noise + Exterior Construction Noise Levels (dBA Leq)	Construction Noise Standard (dBA Leq)	Exceeds Standards?
Demolition	60.3	73.9	73.9	85	No
Site Preparation		75.0	75.0	85	No
Grading		74.7	74.7	85	No
Building Construction, Paving, and Painting		78.4	78.4	85	No

Source: Construction noise levels were calculated by ECORP Consulting using the FHWA Roadway Noise Construction Model (FHWA 2006). Refer to Attachment C for Model Data Outputs.

Notes: *Average ambient noise levels of the Project Area were estimated using the average Leq of the five short term noise measurement taken on March 20, 2024, and identified in Table 3-1.

Construction equipment used during construction derived from the California Emissions Estimator Model (CalEEMod). CalEEMod is designed to calculate air pollutant emissions from construction activity and contains default construction equipment and usage parameters for typical construction projects based on several construction surveys conducted in order to identify such parameters. Consistent with FTA recommendations for calculating construction noise, construction noise was measured from the center of the Project Site (FTA 2018), which is 213 feet from the nearest sensitive receptor.

Leq = The equivalent energy noise level, is the average acoustic energy content of noise for a stated period of time. Thus, the Leq of a time-varying noise and that of a steady noise are the same if they deliver the same acoustic energy to the ear during exposure. For evaluating community impacts, this rating scale does not vary, regardless of whether the noise occurs during the day or the night.

As shown in Table 10-E, the Project’s contribution of construction noise combined with the ambient noise environment would not exceed the 85 dBA NIOSH construction noise threshold during any phase of construction at the nearby noise-

sensitive receptors. It is noted that construction noise was modeled on a worst-case basis and is considered in addition to ambient noise levels currently experienced in the Project Area. It is very unlikely that all pieces of construction equipment would be operating at the same time for the various phases of Project construction as well as at the point closest to residences.

Offsite Construction Worker Trips

Project construction would result in temporary additional traffic on adjacent roadways over the period that construction occurs. According to the California Emissions Estimator Model, which is used to predict the number of construction-related automotive trips, the maximum number of Project construction trips traveling to and from the Project Site during a single construction phase would not be expected to exceed 94 daily trips in total. According to Caltrans Technical Noise Supplement to the Traffic Noise Analysis Protocol (2013), a doubling of traffic on a roadway is required to result in an increase of 3 dB (outside of the laboratory, a 3-dBA change is considered a just-perceivable difference). The Project Site would be accessible via Main Street during construction. According to the City's General Plan's Circulation Element, the portion of Main Street adjacent to the Project Site is classified as a major arterial roadway (6 lanes). Furthermore, according to the Traffic Impact Analysis Report prepared by Linscott, Law, and Greenspan Engineers (2024), Main Street's existing daily traffic volume at the Project Site is approximately 26,227 vehicles. Therefore, the Project's construction trips would not result in a doubling of traffic on the local transportation network, and therefore its contribution to existing traffic noise would not be perceptible. Additionally, it is noted that construction is temporary, and these trips would cease upon completion of the Project.

Operational Offsite Traffic Noise

Future traffic noise levels throughout the Project vicinity for the proposed Project were modeled based on the traffic volumes identified by Linscott, Law, and Greenspan Engineers (2024) to determine the noise levels along Project vicinity roadways. Table 10-F shows the calculated offsite roadway noise levels with traffic levels during existing levels and the projected levels of project buildout. The City of Corona does not regulate noise from transportation sources for commercial projects and does not have noise standards for such sources. As such, the thresholds recommended by FICON will be used in this analysis.

- If the existing ambient noise levels at existing and future noise-sensitive land uses (e.g. residential, etc.) are less than 60 dBA CNEL and the project creates a readily perceptible 5 dBA CNEL or greater noise level increase and the resulting noise level would exceed acceptable exterior noise standards, or
- If the existing noise levels range from 60 to 65 dBA CNEL and the project creates a barely perceptible 3 dBA CNEL or greater noise level increase and the resulting noise level would exceed acceptable exterior noise standards, or
- If the existing noise levels already exceed 65 dBA CNEL and the project creates a community noise level increase of greater than 1.5 dBA CNEL.

As shown in Table 5-2, none of the Project vicinity roadway segments would experience both an incremental increase of traffic noise in excess of the FICON standards and a resultant noise level over the applicable exterior noise limit.

TABLE 10-F: PROJECT PREDICTED TRAFFIC NOISE LEVELS

Roadway Segment	Surrounding Uses	CNEL at 100 feet from Centerline of Roadway		Change	FICON Standard	Applicable Exterior Noise Limit	Exceed Both Standards?
		Existing	Existing + Project				
Main Street							
Between SR-91 EB Ramps and 3rd Street	Commercial	68.2	68.4	+0.2	> 1.5	NA	No
Between 3rd Street and 5th Street	Commercial	67.4	67.5	+0.1	> 1.5	NA	No
Between 6th Street and 8th Street	Commercial and Public Library Use	66.1	66.2	+0.1	> 1.5	NA	No
Between 9th Street and Grand Boulevard	Commercial and Residential	65.5	65.7	+0.2	> 1.5	65 dBA	No
Belle Avenue							
North of 3rd Street	Residential	46.6	46.6	+0.0	>5	65 dBA	No
Between 3rd Street and 4th Street	Residential	47.9	50.5	+2.6	>5	65 dBA	No
Between 4th Street and 5th Street	Residential	49.6	51.4	+1.8	>5	65 dBA	No
Between 5th Street and 6th Street	Residential	49.1	49.8	+0.7	>5	65 dBA	No
3rd Street							
West of Belle Avenue	Residential and School	53.2	53.2	+0.0	>5	65 dBA	No
Between Belle Avenue and	Residential	54.8	55.1	+0.3	>5	65 dBA	No
Washburn Avenue							
Between Washburn Avenue and Main Street	Commercial	55.2	55.8	+0.6	>5	NA	No
East of Main Street	Commercial and Residential	54.4	54.4	+0.0	>5	65 dBA	No
4th Street							
West of Belle Avenue	Residential	44.5	44.5	+0.0	>5	65 dBA	No
East of Belle Avenue	Residential	41.0	42.8	+1.8	>5	65 dBA	No
5th Street							
West of Belle Avenue	Residential	45.8	50.2	+4.4	>5	65 dBA	No

TABLE 10-F: PROJECT PREDICTED TRAFFIC NOISE LEVELS

6th Street							
Between Belle Avenue and Crawford Street	Commercial	63.3	63.4	+0.0	>3	NA	No
Between Main Street and Belle Avenue	Commercial and Public Library Use	61.9	62.2	+0.3	>3	NA	No
Between Main Street and Grand Boulevard East	Commercial	63.0	63.1	+0.1	>3	NA	No
8th Street							
East of Main Street	Commercial	54.2	54.2	+0.0	>5	NA	No
West of Main Street	Residential	50.6	50.6	+0.0	>5	65 dBA	No
9th Street							
East of Main Street	Commercial	45.7	45.7	+0.0	>5	NA	No
West of Main Street	Residential	48.8	48.8	+0.0	>5	65 dBA	No

Operational Onsite Noise

The Project is proposing the redevelopment of the Project Site that would involve the construction and operation of a 40,000 square foot grocery market, a 3,297 square foot restaurant, 3,633 square foot bank, and 261 parking spaces. On-site noise associated with the Proposed Project has been calculated using the SoundPLAN 3D noise model using Project Site Plans provided by the Project proponent. SoundPLAN 3D noise model generates computer simulations of noise situations based on the site’s features. Further, SoundPLAN creates noise contour maps using reference noise levels, topography, point and area noise source, mobile noise sources, and intervening structures. The Proposed Project’s modeling scenario includes features such as the parking lots, a truck loading spot, and the proposed buildings. The Proposed Project’s land uses would primarily operate during daytime hours (7:00 a.m. – 10:00 p.m.). As such, the modeling has accounted for parking lot activity noise during the hours of 7:00 a.m. – 10:00 p.m. The Proposed Project site plans indicate a truck loading spot adjacent to the grocery store building on the site. Because grocery stores may restock at any hour of the day, the modeling conservatively assumes that the truck spot would be generating noise at all times of the day. A reference noise measurement representing the parking lot previously taken by ECORP Consulting Inc. A reference noise level for a truck unloading general cargo was accessed from the SoundPLAN Noise Library. Additionally, mechanical equipment, such as heating, ventilation and air conditioning (HVAC) units would be required to be in compliance with the city’s daytime (7:00 a.m. to 11:00 p.m.) and nighttime (11:00 p.m. to 7:00 a.m.) noise standards of 55 dBA Leq and 50 dBA Leq. Such units would receive greater noise reduction due to additional rooftop parapet shielding and therefore would likely not contribute to the combined noise level.

The City has established exterior noise thresholds for onsite stationary sources, shown in Table 10-B above. The city limits daytime noise to 65 dBA and nighttime noise to 45 dBA for residential receptors. Table 5-2 shows the predicted Project noise levels at fifteen noise-sensitive locations in the Project vicinity, including nearby residences to the north and west of the Project Site and the commercial property to the west of the Project Site, as predicted by SoundPLAN. Additionally, a noise contour graphic for each scenario (see Figure 5-1) has been prepared to provide a visual depiction of the predicted noise levels in the Project vicinity from Project operations.

TABLE 10-G: MODELED OPERATIONAL NOISE LEVELS

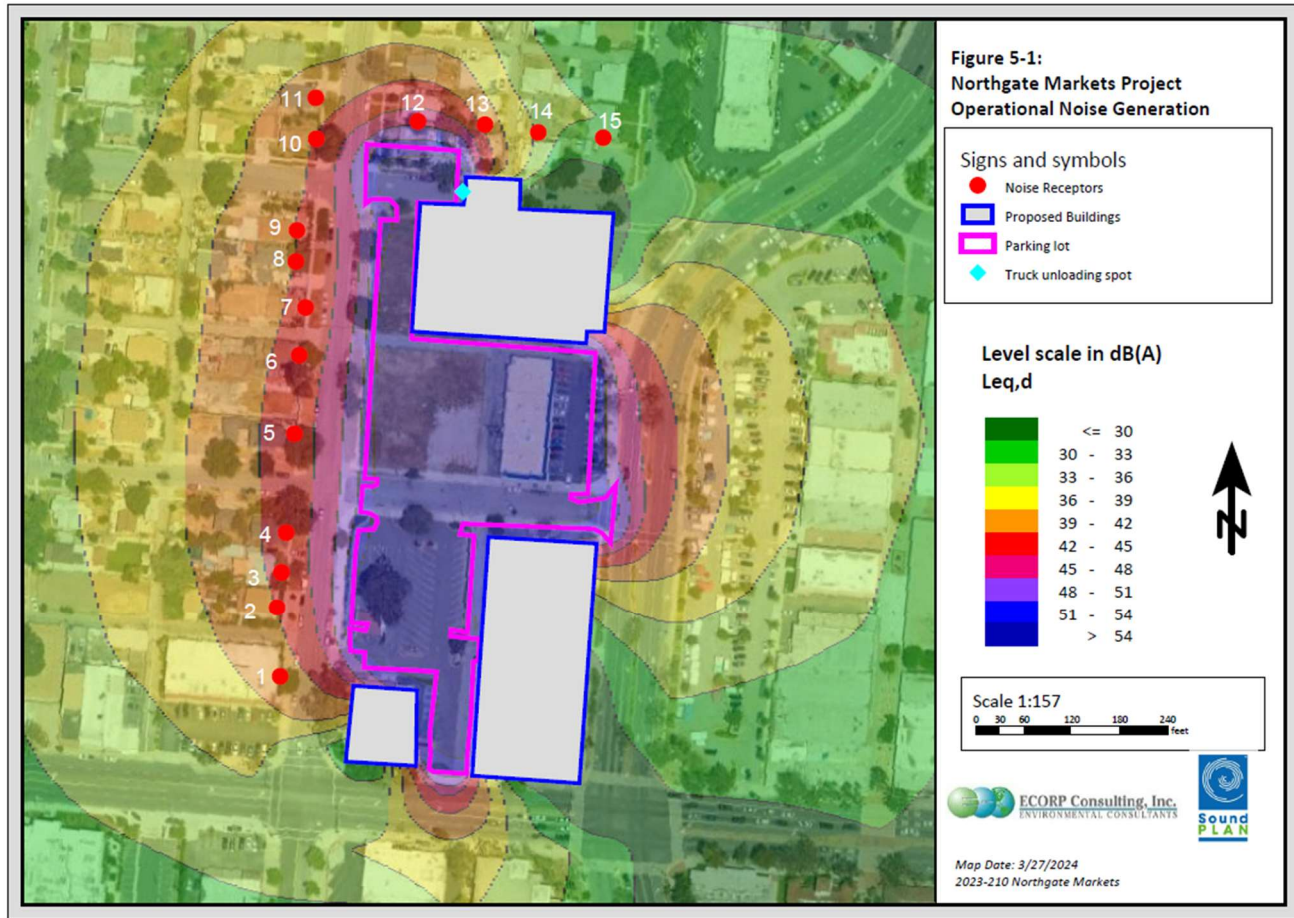
#	Location	Daytime/Nighttime Noise Attributed to the Project (dBA Leq)	Daytime/Nighttime Exterior Noise Standards (dBA Leq)	Exceed Daytime or Nighttime Exterior Standard?
1	Commercial Building West of Project Site	39.7 / 2.2	65/NA	No
2	Residence on Belle Avenue	40.0 / 35.3	65/45	No
3	Residence on Belle Avenue	41.5 / 36.4	65/45	No
4	Residence on Belle Avenue	41.6 / 33.6	65/45	No
5	Residence on Belle Avenue	41.4 / 17.2	65/45	No
6	Residence on Belle Avenue	42.4 / 14.2	65/45	No
7	Residence on Belle Avenue	42.7 / 14.0	65/45	No
8	Residence on Belle Avenue	43.1 / 9.8	65/45	No
9	Residence on Belle Avenue	42.6 / 8.3	65/45	No
10	Residence on Belle Avenue	42.1 / 6.2	65/45	No
11	Residence on Belle Avenue	40.8 / 5.5	65/45	No
12	Residence North of Project Site	34.3 / 15.7	65/45	No
13	Residence North of Project Site	31.1 / 11.7	65/45	No
14	Residence North of Project Site	38.9 / 21.5	65/45	No
15	Residence North of Project Site	44.6 / 39.8	65/45	No

Source: SoundPLAN v 9.0. Refer to Attachment D for Model Data Outputs.

As shown in Table 10-G, Project operational noise would not exceed the city’s exterior noise standards for daytime or nighttime at any location.

As previously discussed, the average daily ambient noise of the area surrounding the Project Site is approximately 60.3 dBA. This noise level is based on the average of the ambient noise measurements taken in the area and is generally representative of the existing noise environment. As shown above in Table 5-2, the Proposed Project would contribute noise levels of approximately 37.4 to 48.5 dBA Leq to the existing ambient environment during the daytime. When accounting for existing ambient noise levels combined with Proposed Project noise contribution, the noise levels attributed to the Project’s activities would remain unchanged, ranging between 37.4 to 48.5 dBA. It is likely that the existing ambient noise level of 60.3 dBA would remain, and the Project on-site activities would not influence the ambient noise levels of the area. As previously described, the dB scale is logarithmic, not linear, and therefore sound levels cannot be added or subtracted through ordinary arithmetic. For example, a 65-dB source of sound, such as a truck, when joined by another 65 dB source results in a sound amplitude of 68 dB, not 130 dB (i.e., doubling the source strength increases the sound pressure by three dB). These noise levels are still within the city’s daytime exterior noise thresholds for onsite stationary sources, shown in Table 10-B above.

FIGURE 10-H: PROJECT OPERATIONAL NOISE GENERATION



Because Project noise levels would not generate a noise level that exceeds the City’s thresholds, impacts would be less than significant and no mitigation with respect to noise would be required.

b. Exposure to excessive noise levels/vibrations

Less than Significant Impact.

Construction

Excessive groundborne vibration impacts result from continuously occurring vibration levels. Increases in groundborne vibration levels attributable to the Project would be primarily associated with short-term construction-related activities. Construction on the Project Site would have the potential to result in varying degrees of temporary groundborne vibration, depending on the specific construction equipment used and the operations involved. Ground vibration generated by construction equipment spreads through the ground and diminishes in magnitude with increases in distance.

Construction-related ground vibration is normally associated with impact equipment such as pile drivers, jackhammers, and the operation of some heavy-duty construction equipment, such as dozers and trucks. Vibration decreases rapidly with distance, and it is acknowledged that construction activities would occur throughout the Project Site and would not be concentrated at the point closest to sensitive receptors. Groundborne vibration levels associated with construction equipment are summarized in Table 10-I.

TABLE 10-I: VIBRATION SOURCE LEVELS FOR CONSTRUCTION EQUIPMENT

Equipment Type	VdB at 25 feet
Large Bulldozer	87
Caisson Drilling	87
Pile Driver	93
Loaded Trucks	86
Hoe Ram	87
Jackhammer	79
Small Bulldozer/Tractor	58
Vibratory Roller	94

Source: FTA 2018

The City of Corona provides a threshold for vibration annoyance. In the City’s Municipal Code Section 17.84.050, Vibration, it states that the vibration annoyance threshold for all projects is 0.05 inches per second RMS. This is also the level at which vibrations may begin to annoy people in buildings. Vibration annoyance (measured in VdB) focuses on human perception, as discussed in Section 2.2.1, Vibration Sources and Characteristics. The VdB equivalent of 0.05 inches per second RMS is 94 VdB (Federal Transit Administration (FTA) 2018).

The nearest structure of concern to the construction site are single-family homes north of the Project Site and is approximately 213 feet from the center of the Site. Based on the representative vibration levels presented for various construction equipment types in Table 10-I and the construction vibration assessment methodology published by the FTA (2018), it is possible to estimate the potential Project construction vibration levels. Table 10-J presents the expected Project-related vibration annoyance levels at a distance of 213 feet.

TABLE 10-J: CONSTRUCTION VIBRATION ANNOYANCE LEVELS AT 213 FEET

Receiver VdB Levels					Peak Vibration	Threshold	Exceed Threshold?
Large Bulldozer, Caisson Drilling, & Hoe Ram	Loaded Trucks	Jackhammer	Pile Driver	Vibratory Roller			
59	58	51	65	66	66	94	No

Notes: FTA 2018. Distance to the nearest structure of concern is approximately 213 feet measured from Project Site center.

As shown in Table 10-J, vibration as a result of onsite construction annoyance activities on the Project Site would not exceed 94 Vdb (or 0.05 inches per second RMS) at the nearest structure. Thus, onsite Project construction would not exceed the recommended threshold.

Operations

Project operations would not include the use of any stationary equipment that would result in excessive vibration levels. The Project would not attract large numbers of heavy-duty trucks or include heavy-duty stationary equipment. Therefore, the Project would result in negligible groundborne vibration impacts during operations.

c. Permanent increase in ambient noise levels

Less than Significant Impact. Please refer to the analysis of Section 10.a. As previously shown in Table 10-G, long-term operation of the proposed Project would result in the generation of noise levels that are below the City’s significance criteria at the nearest sensitive receptors. Accordingly, Project impacts due to a permanent increase in ambient noise levels would be less than significant.

d. Temporary increase in ambient noise levels

Less than Significant Impact. Please refer to the analysis of Section 10.a. As previously shown in Table 10-E, short-term construction activities would result in the generation of noise levels that are below the City’s significance criteria at the nearest sensitive receptors. Accordingly, Project impacts due to a temporary increase in ambient noise levels would be less than significant.

e. Would the Project conflict with airport land use plan noise contours?

No Impact. The Project Site is located approximately 2.10 miles southeast of the Corona Municipal Airport. According to the General Plan Noise Element, the Project Site is not located within any noise contours or the Corona Municipal Airport Influence Area Boundary. Therefore, the Proposed Project would not expose those visiting or working on the Project Site to excessive airport noise.

11. PUBLIC SERVICES:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
a. Fire protection	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Police protection	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Schools	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Parks & recreation facilities	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Other public facilities or services	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

a. Fire Protection

Less than Significant Impact. Fire prevention services are provided by the Corona Fire Department (CFD). The closest fire station to the Project area is CFD Fire Station No. 2, located at 225 E. Harrison Street or approximately 0.5 roadway miles northeast of the Project area (Google Earth, 2025). As part of the Project, the existing buildings and improvements on site would be demolished and replaced with a 40,000 square foot supermarket and remodel of an existing back building into a 3,297 square foot restaurant and 3,633 square foot bank and associated site improvements. The CFD currently provides fire protection service to the existing uses at the Project site, and it is not expected that redevelopment of the Project site as proposed would result in a substantial increase in the site’s demand for fire protection services or facilities. Furthermore, the Project Applicant would be required to contribute Development Impact Fees (DIF) pursuant to Chapter 16.23 of the City’s Municipal Code. The amount of the required fee will be based on the proposed increase in building area as compared to the existing buildings on site. Payment of the DIF fee would assist the CFD in providing fire protection services within the city and would ensure that funds are available for capital improvements, such as land/equipment purchases and fire station construction. Accordingly, Project-related impacts to fire protection services are evaluated as less than significant and no mitigation beyond payment of DIF fees would be required.

b. Police Protection

Less than Significant Impact. Police protection services are provided by the Corona Police Department (CPD). The CPD Police station is located at 730 Public Safety Way, Corona, CA 92880, approximately 1.1 miles northwest of the Project site. As part of the Project, the existing buildings and improvements on site would be demolished and replaced with a 40,000 square foot market and remodel of an existing back building into a 3,297 square foot restaurant and 3,633 square foot bank and associated site improvements. CPD currently provides police protection service to the existing uses at the Project site, and it is not expected that redevelopment of the Project site as proposed would result in an increase in the site’s demand for police protection services or facilities. Furthermore, the Project Applicant would be required to contribute Development Impact Fees (DIF) pursuant to Chapter 16.23 of the City’s Municipal Code. The amount of the required fee will be based on the proposed increase in building area as compared to the existing buildings on site. Payment of the DIF fee would assist the CPD in providing police protection services within the city and would ensure that funds are available to ensure that the Project does not adversely affect CPD response times or services. Accordingly, Project-related impacts to police protection services are evaluated as less than significant and no mitigation beyond payment of DIF fees would be required.

c. Schools

Less than Significant Impact. Corona is served by the Corona-Norco Unified School District (CNUSD). As part of the Project, the existing buildings and improvements on site would be demolished and replaced with a 40,000 square foot market and remodel of an existing back building into a 3,297 square foot restaurant and 3,633 square foot bank and associated site improvements. Since the Project does not include any residential uses, the Project would not directly result in an increase in demand for school services, and only would result in a nominal increase in the site's potential indirect demand for school services associated with the projected increase in the number of employees on site. In addition, the Project would not directly cause or contribute to the need for new or expanded school facilities, and it is not possible to identify environmental impacts that may be associated with the construction of new or expanded school facilities until a specific proposal and design for the facility is prepared by the applicable school district, and an analysis of potential physical environmental impacts resulting from the construction and operation of new or expanded school facilities would be speculative in nature (see State CEQA Guidelines § 15145). Although it is not possible to identify physical environmental effects that may result from new or expanded school facilities, the Project Applicant would be required to contribute fees to the CNUSD in accordance with Chapter 16.26 of the City's Municipal Code. Pursuant to the Leroy F. Greene School Facilities Act of 1998, payment of school impact fees constitutes full and complete mitigation for project-related impacts to school services. Although the Project would not result in a direct increase in demand for school services, mandatory payment of school impact fees still would be required and would ensure that the Project's impacts to school facilities and services would be less than significant. Accordingly, impacts would be less than significant and no mitigation beyond payment of fees would be required.

d. Parks and Recreation Facilities

Less than Significant Impact. As part of the Project, the existing buildings and improvements on site would be demolished and replaced with a 40,000 square foot market and remodel of an existing back building into a 3,297 square foot restaurant and 3,633 square foot bank and associated site improvements. As the proposed Project would not include any residential uses, the Project would not create a direct demand for new or expanded park or recreational facilities. Although the Project is anticipated to result in an increase in the number of employees on site as compared to the existing uses on site, the Project Applicant would be required to contribute Development Impact Fees (DIF) pursuant to Chapter 16.23 of the City's Municipal Code. The amount of the required fee will be based on the proposed increase in building area as compared to the existing buildings on site. Payment of the DIF fee would assist the city in acquiring and improving parkland within the city to meet the demands of city residents. Accordingly, Project-related impacts to parks and recreational facilities are evaluated as less than significant and no mitigation beyond payment of DIF fees would be required.

e. Other Public Facilities and Services

Less than Significant Impact.

As part of the Project, the existing buildings and improvements on site would be demolished and replaced with a 40,000 square foot market and remodel of an existing back building into a 3,297 square foot restaurant and 3,633 square foot bank and associated site improvements. As the proposed Project would not include any residential uses, the Project would not create a direct demand for new or expanded library services or facilities. Although the Project is anticipated to result in an increase in the number of employees on site as compared to the existing uses on site, the Project Applicant would be required to contribute Development Impact Fees (DIF) pursuant to Chapter 16.23 of the City's Municipal Code. The amount of the required fee will be based on the proposed increase in building area as compared to the existing buildings on site. Payment of the DIF fee would assist the city in providing library services and facilities for city residents. Accordingly, Project-related impacts to libraries are evaluated as less than significant and no mitigation beyond payment of DIF fees would be required.

12. UTILITIES:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
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a. Exceed wastewater treatment requirements	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Involve construction/expansion of water or wastewater treatment facilities	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Involve construction/expansion of storm drains	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Sufficient water supplies/compliance with Urban Water Management Plan.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
e. Adequate wastewater treatment capacity	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
f. Adequate landfill capacity	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
g. Comply with solid waste regulations	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:

a. Exceed wastewater treatment requirements

Less than Significant Impact.

The City of Corona Utilities Department is the primary provider of sewer and sanitation services to Corona, and no septic systems or alternative wastewater treatment systems are proposed as part of the Project. Pursuant to Section 402 of the CWA, the city is subject to the NPDES permit program. The Santa Ana RWQCB is responsible for enforcing the City's Waste Discharge Requirements as established under the applicable NPDES Permit. The NPDES permit sets forth discharge prohibitions including effluent limitation, receiving water limitations, monitoring mechanisms, and penalties for non-compliance with the provisions of the permit. Accordingly, the city is required to comply with all applicable waste discharge requirements. The Project's contribution of wastewater to the City's treatment facilities would be consistent with all applicable waste discharge requirements. Therefore, the Project would have no potential to result in exceedances of the applicable wastewater treatment requirements established by the RWQCB. Impacts would be less than significant and no mitigation is warranted.

b. Involve construction/expansion of water or wastewater treatment facilities

The proposed Project is within an urbanized, developed area of Corona. As identified in Section 4 of this MND, water services would be provided by the City's Utilities Department. The new on-site water system would convey water supplies to the market, commercial uses and to the landscaping through plumbing/landscaping fixtures that are compliant with the CALGreen Plumbing Code and the City's Municipal Code §17.70.070, Landscaping, and Chapter 13.14, Water and Sewer Regulations and would be reviewed for compliance by the City during Project plan check. If, during plan check, it is determined that the project may require upsizing either water or sewer lines, the Project will be required to perform such upgrades prior to the issuance of any building permit. This requirement is ensured by the Conditions of Approval for PP2024-0001. Therefore, impacts would be less than significant and no mitigation is warranted.

The construction activities related to the on-site water infrastructure that would be needed to serve the proposed units are included as part of the proposed Project and would not result in any physical environmental effects beyond those identified

throughout this MND. For example, construction emissions for excavation and installation of the water infrastructure are included in Section 5, Air Quality and Section 16, Greenhouse Gas, and noise volumes from these activities are evaluated in Section 10, Noise. In addition, Project implementation would not require off-site improvements. Therefore, the proposed Project would not result in the construction of new water facilities or expansion of existing facilities, the construction of which could cause significant environmental effects, and impacts would be less than significant and no mitigation is warranted.

c. Involve construction/expansion of storm drains

Less than Significant Impact.

As previously discussed in Section 14 of this MND, the Project would increase runoff volumes above existing conditions. However, the stormwater capture and MWS features installed as part of the Project are sized to handle the increased on-site volumes to ensure no increase in runoff beyond the site. The construction activities related to installation of the onsite storm water infrastructure that would serve the proposed Project, is included as part of the proposed Project, and would not result in any physical environmental effects beyond those identified throughout this MND. As the proposed Project includes facilities to serve the proposed development, it would not result in the need for construction of other new stormwater facilities or expansions, the construction of which could cause significant environmental effects. Therefore, impacts would be less than significant and no mitigation is warranted.

d. Sufficient water supplies/compliance with Urban Water Management Plan

Less than Significant Impact. The City provides water services to the Project site. The City has adopted an Urban Water Management Plan (UWMP) that assesses water supply reliability and demonstrates that the City would have sufficient water supplies during normal years, single dry years, and five consecutive dry years projected through 2045 (Corona, 2021, p. ES-2). The UWMP bases its growth projections in part on the City's General Plan land use plan, and projects that are consistent with the City's General Plan land use plan are inherently consistent with the growth assumptions of the UWMP. The proposed Project is fully consistent with the site's adopted Mixed Use Downtown (MUD) land use designation.

The Project would also limit water use by inclusion of low-flow plumbing and irrigation fixtures, pursuant to the California Title 24 requirements and would comply with City permits and fees as necessary. Therefore, the proposed Project would have sufficient water supplies available to serve the Project, and reasonably foreseeable future development during normal, dry, and multiple dry years, and impacts would be less than significant.

e. Adequate wastewater treatment capacity

Less than Significant Impact. See discussion under Section 12a.

f. Adequate landfill capacity

Less than Significant Impact.

Waste Management (WM) is contracted by the City of Corona as the sole hauler of solid waste and provider of recycling services. WM provides refuse collection to residential, commercial, and industrial customers. Solid waste from the Project would be transported to the El Sobrante landfill located at 10910 Dawson Canyon in Corona. The El Sobrante landfill accepts a maximum of 16,054 tons of waste per day and has a remaining capacity of 143,977,170 tons and an estimated closure date of 2051. Per the General Plan Technical Update EIR, the County of Riverside is required to maintain 15 years identified disposal capacity or have a plan to transform or divert its waste, pursuant to AB 939. Thus, while General Plan buildout could occur after 2051, the County would be required to have 15 years identified disposal capacity after that date. There is adequate landfill capacity in the region for solid waste that would be generated by the 2020-2040 General Plan buildout. Furthermore, new developments approved by the city would be required to contain storage areas for recyclable materials in conformance with California Public Resources Code Sections 42900 et seq., and the City of Corona Municipal Code Chapter 8.20 (Collection of Refuse and Recyclable Materials). Solid waste diversion program would continue operating and would have adequate capacity to accept all future wastes and recyclables to reduce landfilled waste. Therefore, impacts would be less than significant, and no mitigation is required.

g. Comply with solid waste regulations

Less Than Significant Impact. The proposed Project would result in new development that would generate an increased amount of solid waste. All solid waste-generating activities within the City are subject to the requirements set forth in Section 5.408.1 of the 2022 California Green Building Standards Code that requires demolition and construction activities to recycle or reuse a minimum of 65 percent of the nonhazardous construction and demolition waste, and AB 341 that requires diversion of a minimum of 75 percent of operational solid waste.

In addition, the proposed Project would be required to comply with all federal, State, and local regulations related to solid waste. Furthermore, the proposed Project would comply with all standards related to solid waste diversion, reduction, and

recycling during Project construction and operation. Therefore, the proposed Project is anticipated to result in less than significant impacts related to potential conflicts with federal, State, and local management and reduction statutes and regulations pertaining to solid waste.

13 AESTHETICS:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
a. Scenic vista or highway	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
b. Degrade visual character of site & surroundings	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
c. Light or glare	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Scenic resources (forest land, historic buildings within state scenic highway)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discussion:

a. Scenic vista or highway

Less than Significant Impact. Scenic vistas consist of expansive, panoramic views of important, unique, or highly valued visual features that are seen from public viewing areas. This definition combines visual quality with information about view exposure to describe the level of interest or concern that viewers may have for the quality of a particular view of visual setting.

According to the City’s General Plan, Figure CD-1, the SR-91 freeway is a state eligible scenic corridor because it runs through the Santa Ana Canyon, and its viewshed near the western portion of the City of Corona is bounded by the Chino Hills on the foothills of the Santa Ana Mountains to the south. Per Figure CD-1, S. Main Street along the Project frontage is identified as a city designated scenic corridor. Conversely, W. Sixth Street and S. Belle Avenue are not identified as eligible scenic corridors nor are they identified as city designated scenic corridors.

The Project would be developed with a 40,000 square foot grocery market and remodel of an existing back building into a 3,297 square foot restaurant and 3,633 square foot bank and associated site improvements, with a maximum building height of approximately 40 feet. The Project site is located within a developed urban area with commercial structures located to the north, east, south and west, including single family residential to a portion of the west and north. The Project site is located approximately 1.6 miles to the south of the SR-91 freeway, and the market building is set back from S. Main Street. Therefore, the Project would not encroach into views along the S. Main Street scenic corridor any more than existing structures, SR-91 freeway, and freeway walls currently do. Thus, development of the Project site would not obstruct, interrupt, or diminish a scenic vista and impacts would be less than significant.

b. Degrade visual character of site and surroundings

Less than Significant Impact. The Project site is located within the Downtown (D) District of the Downtown Revitalization Specific Plan in an urbanized area of the city. The Downtown (D) District provides opportunities for commercial retail, service commercial, business offices, restaurants and sidewalk dining, cultural and entertainment uses, mixed-use (including residential), as well as prominent buildings for governmental uses. The uses within the district are intended to encourage and support a pedestrian-friendly environment. Parking structures of a human-scale and pedestrian character, including efficient internal access, ingress and egress, plazas, courtyards, and attractive streetscapes are also intended for the district. The Project will be consistent with the D district, including revitalizing 13 underutilized and blighted parcels, and is also consistent with the goals and policies of the MUD (Mixed Use Downtown) land use designation.

The Project’s proposed buildings have a Spanish Mediterranean architectural design. The Project is consistent with the existing surrounding developments and would be developed per the Downtown Revitalization Specific Plan’s development standards and architectural design guidelines, and the applicable standards in the Corona Municipal Code. Therefore, the

development of the proposed Project would not degrade the visual character of the site or surroundings and no mitigation is required.

c. Light or glare

Less than Significant Impact. The Project site is located within an urbanized area with ambient lighting from existing lighting sources, including street lighting from the surrounding streets, security and parking lot lighting from the surrounding commercial and residential developments, and vehicular lighting from the surrounding roadways

Construction

Although construction activities would occur primarily during daylight hours, construction activities could extend into the evening hours. However, construction lighting would be temporary and would only occur during the allowed hours of 7:00 a.m. and 8:00 p.m. on weekdays (Monday through Saturday) and between the hours of 10:00 a.m. and 6:00 p.m. on Sundays and federal holidays, per Section 17.84.040 of the City’s Municipal Code. Therefore, construction of the Project would not create a new source of substantial light that would adversely affect day or nighttime views in the area, and light impacts associated with construction would be less than significant.

Operation

The Project would implement new permanent lighting fixtures on the site. Proposed fixtures include streetlights, building entry light fixtures, and light posts throughout the parking lot areas. Thus, the Project would contribute additional sources to the overall ambient nighttime lighting conditions. However, the site is located within a formerly developed commercial area that included various sources of nighttime lighting, including street lighting. All parking lot and building mounted lighting would be hooded, shielded and appropriately angled away from adjacent land uses and would comply with Municipal Code Section 17.84.070 which requires that all exterior lighting to be designed to direct light downward with minimal spillover onto adjacent residences, sensitive land uses and open space. Because the Project area is within an already developed area with various sources of existing nighttime lighting, and because the Project would be required to comply with the City’s lighting regulations that would be verified by the City during the plan check and permitting process, any increase in lighting that would be generated by the Project would not adversely affect day or nighttime views in the area. Overall, lighting impacts associated with the operation of the Project would be less than significant.

d. Scenic resources (forest land, historic buildings within state scenic highway)

No Impact. The Project includes the demolition of two non-historic structures on the 4.88-acre site, which consists of an existing strip retail building and a drive-in bank related to the existing Citizens bank business. The two former single family dwelling units, located at 323 S. Belle Ave. and 332 S. Washburn Ave., were demolished within the last two years. The site is not near scenic resources such as forest land, nor is it visible from or located on any state scenic highways. While the project is located approximately 1.6 miles to the south of the SR-91 freeway, at a distance greater than 100 feet between W. Third Street and W. Sixth Street, west of S. Main Street, it is not located near the I-15/SR-91 freeway interchange which is considered by the City’s General Plan as an Officially Designated State Scenic Highway. The Project development will be set back from S. Main Street and will therefore not encroach into views along the S. Main Street scenic corridor any more than existing structures. As such, implementation of the Project would not impact scenic resources within a state scenic highway such as forest land and historic buildings. Therefore, no mitigation is warranted.

14. CULTURAL RESOURCES:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
a. Historical resource	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Archaeological resource	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Paleontological resource or unique geologic feature	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
e. Disturb human remains	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Discussion:

The following section is based on the Cultural Resources Records Search Results completed by ECORP Consulting, Inc., dated December 13, 2023 (Appendix J) and the Paleontological Assessment completed by ECORP Consulting, Inc., dated December 5, 2023 (Appendix K).

a. Historical resource

Potentially Significant Unless Mitigation Incorporated. The California Register of Historical Resources defines a “historical resource” as a resource that meets one or more of the following criteria: (1) associated with events that have made a significant contribution to the broad patterns or local or regional history of the cultural heritage of California or the United States; (2) associated with the lives of persons important to local, California, or national history; (3) embodies the distinctive characteristics of a type, period, region, or method of construction or represents the work of a master or possesses high artistic values; or (4) has yielded, or has the potential to yield, information important to the prehistory or history of the local area, California, or the nation.

As part of the Cultural Resources Records Search Results Inventory, a records search for the Project site and surrounding area was conducted through the Eastern Information Center (EIC) at the University of California Riverside on November 13, 2023. The records search determined that two previous cultural resource investigations have been conducted within the Project Area, covering approximately 40 percent of the Project Area (Table 1). The previous studies were both conducted in 2013. This indicates that the entire project area has not been fully surveyed for cultural resources in the past, and that additional, previously unrecorded cultural resources could be present within the Project Area.

In addition to the record search, literature reviewed included survey reports, archaeological site records, and listings of resources on the National Register of Historic Places, California Register of Historical Resources, California Points of Historical Interest, California Historical Landmarks, and National Historic Landmarks. ECORP also contacted the California Native American Heritage Commission (NAHC) to request a search of its Sacred Lands File for the presence of traditional cultural properties or sacred, religious, or otherwise important Native American resources.

Although no known significant cultural resources could be impacted by the Project, the current status of the property may have affected the potential to discover any surface artifacts. Given that the previous development within the Project site might have masked archaeological deposits, there is a potential that buried historical resource deposits may be present within the Project boundaries. Therefore, it is recommended that the Project be allowed to proceed with the implementation of a cultural resources monitoring program conducted by an archaeologist and Native American representative(s) during grading of the property. With implementation of **Mitigation Measures MM CUL-1 and MM-CUL 2**, generally requiring a cultural resource monitoring program during grading activities, impacts to historical resources would be reduced to less-than-significant levels.

b. Archaeological resource

Potentially Significant Unless Mitigation Incorporated. The proposed Project includes the development of a 40,000 square foot Northgate Gonzalez Market and remodel of an existing 6,930 square foot bank building into a bank/restaurant use on an overall 4.88 acre site consisting of 13 combined parcels. The development would include landscaping, parking and public right-of-way improvements such as sidewalks, curb and gutter, and utility and stormwater improvements. Further, the Project area has been disturbed by previous grading associated with the development of various land uses including commercial, machine shop, residential and agricultural purposes since at least the 1890s through the 1980s and 2000s, when the residences were demolished. A strip mall was constructed on the eastern half of the site in the mid-to late-1980s. The 4.88-acre site contains a strip retail building occupied by four tenants, a vacant bank and a bank drive-thru ATM.

As such, the potential to encounter archaeological resources was determined to be low. However, after receiving a comment letter from the Rincon Band of Luiseño Indians and consulting with the Rincon Band of Luiseño Indians, during the AB 52 Tribal Consultation period, **Mitigation Measures CUL-1 and CUL-2** have been incorporated into this MND which require initial ground-disturbing archaeological monitoring, and cultural sensitivity training for construction personnel in the event that inadvertent discoveries of cultural resources be unearthed during project construction. **Mitigation Measures CUL-1 and CUL-2** would thus reduce potential impacts to undiscovered archaeological resources to a less than significant level.

c. Paleontological resource or unique geologic feature**Potentially Significant Unless Mitigation Incorporated.**

Paleontological resources, or fossils, are the remains of ancient plants and animals that can provide scientifically significant information about the history of life on Earth. Paleontological “sensitivity” is defined as the potential for a geologic unit to produce scientifically significant fossils. This sensitivity is determined by rock type, past history of the rock unit in producing significant fossils, and fossil localities that are recorded from that unit. Paleontological sensitivity is assigned based on fossil data collected from the entire geologic unit, not just a specific site.

According to the Paleontological Assessment, a paleontological record search was conducted through the WSC (Western Science Center) in Hemet, California. The WSC does not have fossil localities in the Project Area or within a 1-mile radius of the Project Area. The presence of Holocene alluvial units is insignificant as any fossil material found within these units is unlikely to be fossil material due to the relatively modern associated dates of the deposits. However, if ground disturbance exceeds the Holocene alluvial deposits, the likelihood of reaching Pleistocene (approximately 2 million years ago to 11,700 years ago) alluvial sediments would increase, and there is potential within these sediments to contain fossils. The closest known fossils from surficial alluvial deposits were located from the Chase Ranch neighborhood of Corona, south of the Project Area, consisting of a large collection of over 1,000 fossil leaves from 16 species of plants and trees. Previous records indicate that these mapped fossil deposits derived from middle to early Pleistocene-aged alluvial fan deposits. In addition, between Lincoln Avenue and Main Street, north of Highway 91, the remains of a Pleistocene deer were recovered, and an unknown locality defined as "Corona, Santa Ana River" produced fossil remains of fish, rabbit, vole, mammoth, horse, camel, and bison.

Since the Project has an undetermined potential to yield significant paleontological resources and because the Project site is designated as having "high" paleontological sensitivity according to the City of Corona General Plan EIR paleontological sensitivity map, the Project will be required to prepare a Paleontological Resources Monitoring and Mitigation Plan (PRMMP) for approval by the City of Corona prior to approval of grading plans for the Project as described in **Mitigation Measure CUL-3**. Implementation of the PRMMP, as required by **Mitigation Measure MM CUL-3**, would reduce Project impacts to paleontological resources to less-than-significant levels.

d. Disturb human remains

Potentially Significant Unless Mitigation Incorporated. The Project site does not contain a cemetery and no known cemeteries are located within the immediate site vicinity, and no human remains are known to exist beneath the surface of the site. Nevertheless, the remote potential exists that human remains may be unearthed during grading and excavation activities associated with Project construction. Thus, **Mitigation Measure CUL-4 (MM CUL-4)** has been included which states that if human remains are unearthed during Project construction, the construction contractor would be required by law to comply with California Health and Safety Code, § 7050.5, "Disturbance of Human Remains." According to § 7050.5(b) and (c), if human remains are discovered, the County Coroner must be contacted and if the Coroner recognizes the human remains to be those of a Native American or has reason to believe that they are those of a Native American, the Coroner is required to contact the Native American Heritage Commission (NAHC) by telephone within 24 hours. It should be noted that **Mitigation Measures CUL-1 and CUL 2** recommended in Sections 14.a and 14.b above also address potential impacts relative to disturbing human remains during Project grading.

Additionally, pursuant to California Public Resources Code § 5097.98, whenever the NAHC receives notification of a discovery of Native American human remains from a county coroner, the NAHC is required to immediately notify those persons it believes to be most likely descended from the deceased Native American. The descendants may, with the permission of the owner of the land, or his or her authorized representative, inspect the site of the discovery of the Native American human remains and may recommend to the owner or the person responsible for the excavation work means for treatment or disposition, with appropriate dignity, of the human remains and any associated grave goods. The descendants shall complete their inspection and make recommendations or preferences for treatment within 48 hours of being granted access to the site. According to Public Resources Code § 5097.94(k), the NAHC is authorized to mediate disputes arising between landowners and known descendants relating to the treatment and disposition of Native American human burials, skeletal remains, and items associated with Native American burials.

Therefore, **MM CUL-4**, requiring compliance with California Health and Safety Code § 7050.5 and California Public Resources Code § 5097.98, has been included to reduce the Project's potential impacts to disturbance of human remains to a less than significant level.

Mitigation Measures

MM CUL-1 Archaeological Monitoring. Prior to the issuance of a grading permit, the Project Applicant shall retain and enter a monitoring and mitigation service contract with a qualified Archaeologist ("Archaeological Monitor") for mitigation monitoring services and implement a Cultural Resource Monitoring Program (CRMP). At least 30 days prior to issuance of grading permits, a copy of the executed agreement between the Project Applicant and Archaeologist shall be submitted to the Planning and Development Department:

- A CRMP shall be prepared to guide the procedures and protocols of an archaeological mitigation monitoring program that shall be implemented during initial onsite and offsite ground disturbing activities. The CRMP shall include, but not be limited to, the Project grading and development schedule; approved Project cultural resources mitigation measures and conditions of approval; monitoring procedures; protocols for the identification, assessment, collection, and analysis of any resource(s) observed during grading; curation guidelines; and coordination with project personnel, City staff, and any participating Native American tribe(s). The Rincon Band of Luiseño Indians shall

be notified of any discoveries. The final CRMP shall be submitted to the City Project planner and/or inspector, the appropriate Project supervisor/engineer/etc., and monitoring Native American tribe(s), if any.

- The Archaeological Monitor shall be invited to a preconstruction meeting with construction personnel and City and tribal representatives. The attending archaeologist shall review the provisions of the CRMP and answer any applicable questions.
- Full-time monitoring shall occur throughout the entire Project area, including all off-site improvement areas, during initial ground-disturbing activities. Full-time monitoring shall continue until the Archaeological Monitor determines that the overall sensitivity of the Project area is low as a result of mitigation monitoring and shall have the authority to modify and reduce the monitoring program to either periodic spot-checks or complete suspension of the monitoring program. Should the monitor(s) determine that there are no cultural resources within the Project site or off-site improvement areas, or should the sensitivity be reduced to low during monitoring, all monitoring shall cease.

MM CUL-2 Inadvertent Discovery and Native American Notification. In the event that a significant cultural resource is discovered during ground disturbance activities, the project archaeologist shall notify the City and the Rincon Band of Luiseño Indians for purposes of inviting the Tribe to participate in the CRMP implementation and to observe any continuing ground-disturbing construction activities. Further, all ground disturbance activities within 50 feet of the discovered cultural resource shall be halted and the applicant and a meeting shall be convened between the developer, the consulting archaeologist, the lead agency and a Rincon tribal representative to discuss the significance of the find. Further ground disturbance shall not resume in the area of the discovery until the appropriate treatment has been accomplished.

MM CUL-3 Paleontological Monitor. Prior to the issuance of grading permits, the Project Applicant shall submit to and receive approval from the City of a Paleontological Resources Monitoring and Mitigation Plan (PRMMP). The PRMMP shall include the provision of a trained paleontological monitor during onsite soil disturbance activities. The monitoring for paleontological resources shall be conducted on a full-time basis during the rough grading phases of the Project site within native soils that have the potential to harbor paleontological resources. The paleontological monitor shall be equipped to rapidly remove any large fossil specimens encountered during excavation. During monitoring, samples of soil shall be collected and processed to recover micro-vertebrate fossils. Processing shall include wet screen washing and microscopic examination of the residual materials to identify small vertebrate remains. If paleontological resources are unearthed or discovered during grading activities, the following recovery processes shall apply:

- Upon encountering a large deposit of bone, salvage of all bone in the area shall be conducted with additional field staff and in accordance with modern paleontological techniques.
- All fossils collected during the project shall be prepared to a reasonable point of identification. Excess sediment or matrix shall be removed from the specimens to reduce the bulk and cost of storage. Itemized catalogs of all material collected and identified shall be provided to the museum repository along with the specimens.
- A report documenting the results of the monitoring and salvage activities and the significance of the fossils shall be prepared.
- All fossils collected during this work, along with the itemized inventory of these specimens, shall be deposited in a museum repository (such as the Western Science Center for Archaeology & Paleontology, the Riverside Metropolitan Museum, or the San Bernardino County Museum) for permanent curation and storage.

MM CUL-4 Discovery of Human Remains: In the event that human remains (or remains that may be human) are discovered at the project site during grading or earthmoving activities, the construction contractors, project archaeologist, and/or designated Native American Monitor shall immediately stop all activities within 100 feet of the find. The project proponent shall then inform the Riverside County Coroner and the City of Corona Planning and Development Department, Planning Division, immediately, and the coroner shall be permitted to examine the remains as required by California Health and Safety Code Section 7050.5(b). Section 7050.5 requires that excavation be stopped in the vicinity of discovered human remains until the coroner can determine whether the remains are those of a Native American. If human remains are determined as those of Native American origin, the applicant shall comply with the state relating to the disposition of Native American burials that fall within the jurisdiction of the Native American Heritage Commission (PRC Section

5097). The coroner shall contact the NAHC to determine the most likely descendant(s) (MLD). The MLD shall complete his or her inspection and make recommendations or preferences for treatment within 48 hours of being granted access to the site. The Disposition of the remains shall be overseen by the most likely descendant(s) to determine the most appropriate means of treating the human remains and any associated grave artifacts.

The specific locations of Native American burials and reburials will be proprietary and not disclosed to the general public. The locations will be documented by the consulting archaeologist in conjunction with the various stakeholders and a report of findings will be filed with the Eastern Information Center (EIC). According to California Health and Safety Code, six or more human burials at one location constitute a cemetery (Section 8100), and disturbance of Native American cemeteries is a felony (Section 7052) determined in consultation between the project proponent and the MLD. In the event that the project proponent and the MLD are in disagreement regarding the disposition of the remains, State law will apply and the median and decision process will occur with the NAHC (see Public Resources Code Section 5097.98(e) and 5097.94(k)).

15. AGRICULTURE RESOURCES:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
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- | | | | | |
|--|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a. Williamson Act contract | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| b. Conversion of farmland to nonagricultural use | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |

Discussion:

a. Williamson Act contract

No Impact. The Williamson Act (California Land Conservation Act of 1965) restricts the use of agricultural and open space lands to farming and ranching by enabling local governments to contract with private landowners for indefinite terms in exchange for reduced property tax assessments.

According to the General Plan EIR, Corona does not include any land that is currently under an active Williamson Act contract. Therefore, development of the Project would not result in impacts related to a Williamson Act contract would not occur. Therefore, the Project would result in no impact.

b. Conversion of farmland to non-agricultural use

No Impact. The California Department of Conservation Important Farmland mapping identifies the Project site and surrounding areas as Urban and Built-Up land (CDC 2023). No areas of Prime Farmland, Unique Farmland, or Farmland of Statewide Importance is located on or adjacent to the Project site. Therefore, impacts related to Prime Farmland, Unique Farmland, or Farmland of Statewide Importance would not occur.

16. GREENHOUSE GAS:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
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- | | | | | |
|---|--------------------------|-------------------------------------|-------------------------------------|--------------------------|
| a. Generate greenhouse gases | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> |
| b. Conflict with a plan, policy or regulation | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> | <input type="checkbox"/> |

Discussion:

The following section is based on the Air Quality & Greenhouse Gas Emissions prepared by ECORP Consulting, Inc., dated October 2025 (Revised) (Appendix E). Greenhouse Gas impacts including construction and operational GHGs are discussed in detail under subsection Greenhouse Gas Emission section of the technical memorandum.

a. Generate greenhouse gases

Potentially Significant Unless Mitigation Incorporated. The City of Corona adopted the City of Corona Climate Action Plan Update (CAP) in 2019, which utilizes the Greenhouse Gas Emissions CEQA Thresholds and Screening Tables to determine whether or not a project would have a significant impact on greenhouse gas emissions. The screening tables are to provide guidance in measuring GHG reductions attributable to certain design and construction measures incorporated into development projects. Projects that garner at least 100 points will be consistent with the reduction quantities anticipated in the Corona CAP and would thus be considered less than significant. Utilizing the screening tables would also allow the City to meet its established GHG emissions targets. Small projects that are expected to emit GHG emissions that are less than 3,000 MtCO_{2e} (metric tons of CO_{2e} equivalent) are not required to utilize the screening tables, as they would be expected to have a less than significant individual and cumulative impact for GHG emissions.

As previously described, the CAP Update identifies a two-step approach in evaluating GHG emissions. First, a screening threshold of 3,000 metric tons of CO_{2e} per year is used to determine if additional analysis is required. Projects that exceed the 3,000 metric tons of CO_{2e} per year will be required to quantify and disclose the anticipated GHG then either 1) demonstrate how the project would reduce GHG emissions to levels below 3,000 metric tons annually through project design features and/or mitigation measures, OR 2) garner 100 points through the CAP Screening Tables. As shown on Tables 16-A, the Project would generate 5,866 metric tons of CO_{2e} annually, which is an increase in operational GHG emissions over the existing baseline of 4,661 metric tons of CO_{2e} per year. Thus, the Project must garner at least 100 points in order to be determined to be consistent with the reduction quantities anticipated in the City's CAP.

Table 16-B presents a list of the GHG Reduction Measure options and the associated point values in the GHG Screening Table. Additionally, Table 16-B shows the Reduction Measures selected by the Project applicant, depicted in bold font, and the associated point values.

TABLE 16-A: OPERATIONAL-RELATED GHG EMISSIONS

Emissions Source	CO ₂ e (Metric Tons/ Year)
Proposed Project (Supermarket, Restaurant, Bank)	
Mobile	4,079
Area	1
Energy	313
Water	15
Waste	84
Refrigerants	1,374
Total	5,866
Existing Onsite Land Uses (Retail, Bank, Residence)	
Mobile	1,122
Area	1
Energy	74
Water	3
Waste	5
Refrigerants	0
Total	1,205
Difference (Increase from Baseline)	
Mobile	2,957
Area	0
Energy	239
Water	12
Waste	79
Refrigerants	1,374
Total	4,661

Source: CalEEMod version 2022.1. Refer to Attachment A for Model Data Outputs.

Notes: Emission projections predominately based on CalEEMod model defaults for Riverside County. Mobile source emissions are based on trip rates identified in the Project's Traffic Report (Linscott, Law, and Greenspan Engineers 2024).

TABLE 16-B: CITY OF CORONA SCREENING TABLES FOR GHG REDUCTION MEASURES FOR COMMERCIAL DEVELOPMENT

Reduction Measure	Description	Assigned point Values	Project Points
Reduction Measure 4.1: Exceed Energy Efficiency Standards in New Commercial Units			
4.1.A.1 Building Envelope			
4.1.A.1 Insulation	o 2017 Title 24 Requirements (walls R-13; roof/attic R-30)	o 0 points	9
	o Modestly Enhanced Insulation (walls R-13, roof/attic: R-38)	o 9 points	
	o Enhanced Insulation (rigid wall insulation R-13, roof/attic: R-38)	o 11 points	
	o Greatly Enhanced Insulation (spray foam wall insulated walls R-15 or higher, roof/attic R-38 or higher)	o 12 points	

Reduction Measure	Description	Assigned point Values	Project Points
4.1.A.2 Windows	<ul style="list-style-type: none"> 2016 Title 24 Windows (0.57 U-factor, 0.4 solar heat gain coefficient (SHGC)) Modestly Enhanced Window Insulation (0.4 U-Factor, 0.32 SHGC) Enhanced Window Insulation (0.32 U-Factor, 0.25 SHGC) Greatly Enhanced Window Insulation (0.28 or less U-Factor, 0.22 or less SHGC) 	<ul style="list-style-type: none"> 0 point 4 points 5 points 7 points 	4
4.1.A.3 Cool Roof	<ul style="list-style-type: none"> Modest Cool Roof (CRRC Rated 0.15 aged solar reflectance, 0.75 thermal emittance) Enhanced Cool Roof (CRRC Rated 0.2 aged solar reflectance, 0.75 thermal emittance) Greatly Enhanced Cool Roof (CRRC Rated 0.35 aged solar reflectance, 0.75 thermal emittance) 	<ul style="list-style-type: none"> 7 points 8 points 10 points 	7
4.1.B.4 Water Heaters	<ul style="list-style-type: none"> 2016 Title 24 Minimum Efficiency (0.57 Energy Factor) Improved Efficiency Water Heater (0.675 Energy Factor) High Efficiency Water Heater (0.72 Energy Factor) Very High Efficiency Water Heater (0.92 Energy Factor) Solar Pre-heat System (0.2 Net Solar Fraction) Enhanced Solar Pre-heat System (0.35 Net Solar Fraction) 	<ul style="list-style-type: none"> 0 points 8 points 10 points 11 points 2 points 5 points 	10
4.1.B.7 Appliances	<ul style="list-style-type: none"> Energy Star Refrigerator (new) Energy Star Dish Washer (new) Energy Star Washing Machine (new) 	<ul style="list-style-type: none"> 2 points 2 points 2 points 	4
4.1.C Miscellaneous Commercial Building Efficiencies			
4.1.C.1 Building Placement	<ul style="list-style-type: none"> North/South alignment of building or other building placement such that the orientation of the buildings optimizes natural heating, cooling, and lighting. 	<ul style="list-style-type: none"> 4 points 	4
4.1.C.2 Shading	<ul style="list-style-type: none"> At least 90% of south-facing glazing will be shaded by vegetation or overhangs at noon on June 21st. 	<ul style="list-style-type: none"> 6 points 	6
Reduction Measure 5.2: Exceed Water Efficiency Standards			
5.2.D Commercial Irrigation and Landscaping			
5.2.D.1 Water Efficient Landscaping	<ul style="list-style-type: none"> Eliminate conventional turf from landscaping 	<ul style="list-style-type: none"> 0 points 	5
	<ul style="list-style-type: none"> Only moderate water using plants 	<ul style="list-style-type: none"> 2 points 	
	<ul style="list-style-type: none"> Only low water using plants 	<ul style="list-style-type: none"> 3 points 	
	<ul style="list-style-type: none"> Only California Native landscape that requires no or only supplemental irrigation 	<ul style="list-style-type: none"> 5 points 	
	<ul style="list-style-type: none"> Low precipitation spray heads < .75"/hr or drip irrigation Weather based irrigation control stems combined with drip irrigation (demonstrate 20% reduced water use) 	<ul style="list-style-type: none"> 1 point 3 points 	
5.2.E Commercial Potable Water			
5.2.E.1 Showers	<ul style="list-style-type: none"> Water Efficient Showerheads (2.0 gpm) 	<ul style="list-style-type: none"> 2 points 	7
5.2.E.2 Toilets	<ul style="list-style-type: none"> Water Efficient Toilets/Urinals (1.5 gpm) Waterless Urinals (note that commercial buildings having both waterless urinals and high efficiency toilets will have a combined points value of 6 points) 	<ul style="list-style-type: none"> 3 points 3 points 	
5.2.E.2 Faucets	<ul style="list-style-type: none"> Water Efficient faucets (1.28 gpm) 	<ul style="list-style-type: none"> 2 points 	
5.2.E.4 Commercial Dishwashers	<ul style="list-style-type: none"> Water Efficient Dishwasher (20% water savings) 	<ul style="list-style-type: none"> 2 points 	

Table 3-4. City of Corona Screening Table for GHG Reduction Measures for Commercial Development			
Reduction Measure	Description	Assigned point Values	Project Points
Reduction Measure 7.1: Alternative Transportation Options			
7.1.E Mixed-Use Development			
7.1.E.1 Mixed Use	Mixes of land uses that complement one another in a way that reduces the need for vehicle trips can greatly reduce GHG emissions. The point value of mixed-use projects will be determined based upon traffic studies that demonstrate trip reductions and/or reductions in vehicle miles traveled.	TBD	
7.1.E.2 Local Retail Near Residential (Commercial only Projects)	Having residential developments within walking and biking distance of local retail helps to reduce vehicle trips and/or vehicle miles traveled. The point value of residential projects in close proximity to local retail will be determined based upon traffic studies that demonstrate trip reductions and/or reductions in vehicle miles traveled.	TBD	35+
7.1.F Preferential Parking			
7.1.F.1 Parking	<ul style="list-style-type: none"> o Provide reserved preferential parking spaces for car-share, carpool, and ultra-low or zero-emission vehicles. o Provide larger parking spaces that can accommodate vans used for ride-sharing programs and reserve them for vanpools and include adequate passenger waiting/loading areas. 	<ul style="list-style-type: none"> o 1 point o 1 point 	1
7.1.G.1 Signal Improvements	<p>Techniques for improving traffic flow include: traffic signal coordination to reduce delay, incident management to increase response time to breakdowns and collisions, Intelligent Transportation Systems (ITS) to provide real-time information regarding road conditions and directions, and speed management to reduce high free-flow speeds.</p> <ul style="list-style-type: none"> o Synchronize signals along arterials used by project. o Connect signals along arterials to existing ITS. 	<ul style="list-style-type: none"> o 1point /signal o 3 points /signal 	3
Reduction Measure 7.2: Adopt and Implement a Bicycle Master Plan to Expand Bike Routes around the City			
7.2.B.1 Sidewalks	<ul style="list-style-type: none"> o Provide sidewalks on one side of the street (required) o Provide sidewalks on both sides of the street o Provide pedestrian linkage between commercial and residential land uses within 1 mile 	<ul style="list-style-type: none"> o 0 Point o 1 point o 3 points 	3
Reduction Measure 8.1: Reduce Waste to Landfills			
8.1.B.1 Recycling	<p>City initiated recycling program diverting 80% of waste requires coordination with commercial development to realize this goal. The following recycling features will help the City fulfill this goal:</p> <ul style="list-style-type: none"> o Provide separated recycling bins within each commercial building/floor and provide large external recycling collection bins at central location for collection truck pick-up o Provide commercial/industrial recycling programs that fulfills an on-site goal of 80% diversion of solid waste 	<ul style="list-style-type: none"> o 2 points o 5 points 	2
Total Points Earned by Project			100+

As shown in Table 16-B, the Project, with incorporated **Mitigation Measure MM GHG-1**, would achieve more than the required 100 points necessary to be considered consistent with the city CAP. As a result, with **MM GHG-1**, the Project would result in a less than significant impact with respect to GHG emissions.

b. Conflict with a plan, policy or regulation

Less than Significant Impact.

In November 2022, CARB released the Final 2022 Scoping Plan Update, which identifies the State’s progress towards the statutory 2030 target, while providing a path towards carbon neutrality and reduce greenhouse gases emissions by 85%

below 1990 levels by 2045. Recent studies show that the State’s existing and proposed regulatory framework will allow the State to reduce its GHG emissions level to 40% below 1990 levels by 2030. The Project would not conflict with any of the 2022 Scoping Plan elements as any regulations adopted would apply directly or indirectly to the Project.

Additionally, the Project, with incorporated **Mitigation Measure MM GHG-1**, would achieve more than the required 100 points necessary to be considered consistent with the city CAP. Thus, Project-related emissions would not have a significant direct or indirect impact on GHG and climate change and would therefore comply with the City’s GHG policies under the CAP with implementation of **MM GHG-1**. With **MM GHG-1**, the proposed Project would not conflict with any applicable plan, policy or regulation of an agency adopted for the purpose of reducing the emissions of greenhouse gases.

Mitigation Measure

GHG-1 Greenhouse Gas Reduction. The Proposed Project shall demonstrate consistency with the City of Corona Climate Action Plan Commercial Development GHG Emission Screening Table, providing for a minimum 100 points per the City Screening Tables. The Project must be consistent with the CAP’s requirement to achieve at least 100 points. The City of Corona Planning Division shall verify incorporation of the identified Screening Table Measures within the Project building plans and Site designs prior to the issuance of building permit(s) and shall verify implementation of the identified Screening Table Measures prior to the issuance of Certificate(s) of Occupancy.

Timing/Implementation: Prior to the issuance of a building permit and occupancy permits

Monitoring/Enforcement: The City of Corona Building and Planning Divisions

17. TRIBAL CULTURAL RESOURCES

	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k), or	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Discussion:

AB 52 and SB 18 Requirements

The Project is subject to tribal consultation under AB 52 and SB 18. Chapter 532, Statutes of 2014 (i.e., AB 52), requires that Lead Agencies evaluate a Project’s potential to impact “tribal cultural resources.” Such resources include sites, features, places, cultural landscapes, sacred places, and objects with cultural value to a California Native American tribe that are eligible for inclusion in the California Register or included in a local register of historical resources (PRC Section 21074). AB 52 also gives Lead Agencies the discretion to determine, supported by substantial evidence, whether a resource falling outside the definition stated above nonetheless qualifies as a “tribal cultural resource.”

SB 18 requires cities and counties acting as Lead Agency to contact and consult with California Native American tribes before adopting or amending a General Plan. The intent of SB 18 is to establish meaningful consultation between tribal governments and local governments at the earliest possible point in the planning process and to enable tribes to manage “cultural places.” Cultural places are defined as a Native American sanctified cemetery, place of worship, religious or ceremonial site, or sacred shrine (PRC Section 5097.9), or a Native American historic, cultural, or sacred site, that is listed or may be eligible for listing in the California Register, including any historic or prehistoric ruins, any burial ground, or any archaeological or historic site (PRC Section 5097.993).

In addition, as part of the Cultural Resources Records Search (ECORP, 2023) a Sacred Lands File search was requested from the NAHC. ECORP received the results of the search, which were positive, indicating the presence of Sacred Lands within the Project Area. In compliance with NAHC’s response, on August 15, 2023, the city, acting as Lead Agency, sent 37 letters to all tribes included in NAHC’s list of Native American tribes that may have knowledge regarding tribal cultural

resources in the Project area. On September 8, 2023, the city received correspondence in the form of a comment letter, from the Rincon Band of Luiseno Indians requesting more information and applicable documents related to the Project. Thereafter, on November 7, 2024, after review of the city provided documents and internal review of these documents, the Rincon Band of Luiseno Indians sent a second letter in which they were in agreement with the city’s proposed cultural mitigation measures which include archaeological monitoring, a monitoring report, and protocols for discovery of cultural material and human remains within **Mitigation Measures MM CUL-1, MM CUL-2, and MM CUL-4**. No other requests for consultation or recommendations under AB 52 or SB 18 regarding the proposed Project were received by the city.

a. Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k), or

No Impact. The Project site was previously developed and is located within an urbanized developed area. No resources that are listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code section 5020.1(k), are present on the site. Therefore, no impacts are anticipated as it relates to this area of concern.

b. A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resources Code section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe

Potentially Significant Unless Mitigation Incorporated. In response to the SB18 and AB52 notices, three tribes responded which are summarized below:

- On September 8, 2023, the city received correspondence in the form of a comment letter, from the Rincon Band of Luiseño Indians, requesting copies of the Project’s cultural survey including the archaeological site records, shape files, archaeological record search results, geotechnical report, and the grading plans. After review of the provided documents and their internal information, the Tribe consulted with the city on October 24, 2024. On November 7, 2024, the Rincon Band of Luiseno Indians sent a second letter in which they were in agreement with the city’s proposed cultural mitigation measures which include archaeological monitoring, a monitoring report, and protocols for discovery of cultural material and human remains, **Mitigation Measures MM CUL-1, MM CUL-2, and MM CUL-4**.
- On August 15, 2023 the Gabrielino/Tongva Nation Tribe requested consultation with the city. On February 22, 2024, the city followed up with the Gabrielino/Tongva Nation Tribe on their consultation request. On March 1, 2024, the tribe signed off on the project and withdrew their consultation request.
- On August 15, 2023, the Agua Caliente Band of Cahuilla Indians responded that this Project was not located within the Tribe’s Traditional Use Area and deferred to other tribes in the area. The letter then concluded consultation efforts.

In consideration of the consultation conducted with the Rincon Band of Indians, compliance with the mitigation measures in the Cultural Resources section (**MM CUL-1, MM CUL-2 and MM CUL-4**) would reduce impacts to Tribal Cultural Resources to less than significant should any resources be discovered during the Project’s ground-disturbing construction activities.

18. MANDATORY FINDING OF SIGNIFICANCE:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
a. Fish/ wildlife population or habitat or important historical sites	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
b. Cumulatively considerable impacts	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
c. Substantial adverse effects on humans	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
d. Short-term vs. long-term goals	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Discussion:**a. Fish/wildlife population or habitat or important historical sites**

Potentially Significant Unless Mitigation Incorporated. As indicated throughout the analysis in this IS/MND (refer specifically to the analysis in IS/MND Sections 7, 14, and 17), assuming incorporation of the mitigation measures identified herein, implementation of the proposed Project would not substantially degrade the quality of the environment, substantially reduce the habit of fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, or reduce the number or restrict the range of a rare or endangered plant or animal, or eliminate important examples of the major periods of California history or prehistory. Therefore, with mitigation, impacts would be less than significant.

b. Cumulatively considerable impacts

Potentially Significant Unless Mitigation Incorporated. Cumulative effects that would result from implementation of the Project have been evaluated throughout this IS/MND, which concludes that such impacts would not occur, would be less than significant, or would be reduced to below a level of significance with the incorporation of mitigation measures identified herein and included in the Project's conditions of approval. For example, for the issue of Air Quality (IS/MND Section 5), the SCAQMD's CEQA Air Quality Significance Thresholds indicate that any projects in the SCAB with daily emissions that exceed any of the indicated thresholds should be considered as having an individually and cumulatively-considerable air quality impact. Thus, the analysis of the Project's air quality impacts inherently addresses potential cumulatively-considerable air quality impacts, and shows that Project-related cumulatively considerable impacts to air quality would be less than significant. As indicated in the analysis of Greenhouse Gas Emissions (IS/MND Section 16), projects that are consistent with the city's CAP are considered to have a less-than-significant individual and cumulative impact on GHG emissions.

As described in Section 16, the CAP Update identifies a two-step approach in evaluating GHG emissions. First, a screening threshold of 3,000 metric tons of CO₂e per year is used to determine if additional analysis is required. Projects that exceed the 3,000 metric tons of CO₂e per year will be required to quantify and disclose the anticipated GHG then either 1) demonstrate how the project would reduce GHG emissions to levels below 3,000 metric tons annually through project design features and/or mitigation measures, OR 2) garner 100 points through the CAP Screening Tables. As shown on Tables 3-3, the Project would generate 5,866 metric tons of CO₂e annually, which is an increase in operational GHG emissions over the existing baseline of 4,661 metric tons of CO₂e per year. Thus, the Project must garner at least 100 points in order to be determined to be consistent with the reduction quantities anticipated in the City's CAP. In order to garner the over 100 points, the Project will implement design criteria to reduce GHG emissions. With incorporated **Mitigation Measure MM GHG-1**, the Project would achieve more than the required 100 points necessary to be considered consistent with the city CAP. As a result, with **MM GHG-1**, the Project would result in a less than significant impact with respect to GHG emissions.

Furthermore, the analysis of Project impacts due to noise (IS/MND Section 10) demonstrates that the Project's construction, operational, and transportation-related noise impacts would be less than significant with the incorporation of mitigation measures. Accordingly, with the incorporation of mitigation measures identified herein and included in the Project's conditions of approval, the Project would not have impacts which are individually limited, but cumulatively considerable.

c. Substantial adverse effects on humans

Less Than Significant Impact. The Project's potential to result in substantial adverse effects on human beings has been evaluated throughout this IS/MND (e.g., Air Quality, Geology/Soils, Noise, etc.). Where potentially significant impacts are identified, mitigation measures have been identified to reduce these adverse effects to the maximum feasible extent. There are no components of the proposed Project that could result in substantial adverse effects on human beings that are not already evaluated and disclosed throughout this IS/MND. Accordingly, impacts would be less than significant and no mitigation is warranted.

d. Short term vs. long term goals

Less Than Significant Impact. The Project would develop a 40,000 square foot market and remodel an existing bank building into a restaurant/bank use with associated improvements which is consistent with the General Plan land use designation of Mixed Use Downtown (MUD) as described in Section 1, Land Use and Planning. Further, the proposed development would be consistent with the policies and intent of the General Plan. As such, the Project would not conflict with the General Plan's short- or long-term goals.

19. WILDFIRE:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
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a. Substantially impair an adopted emergency response plan or emergency evacuation plan

b. Due to slope, prevailing wind, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from wildfire or the uncontrolled spread of a wildfire

c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water resources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment

d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability or drainage changes

Discussion

a. Substantially impair an adopted emergency response plan or emergency evacuation plan

No Impact. According to the CAL FIRE Hazard Severity Zone map, the Project site is not within an area identified as a Very Fire Hazard Severity Zone (VFHSZ) or a State Responsibility Area (SRA) (CALFIRE 2025). The proposed Project would be located within a Local Responsibility Area (LRA). Additionally, the proposed Project would not physically interfere with an adopted emergency response plan or emergency evacuation plan. The proposed Project does not include any characteristics (e.g., permanent road closures or long-term blocking of road access) that would substantially impair or otherwise conflict with an emergency response plan or emergency evacuation plan. Further, the proposed Project would not obstruct or alter any transportation routes that could be used as evacuation routes during emergency events.

The proposed Project would provide adequate emergency access to the site via 28-foot-wide driveways along S. Main Street, W. Sixth Street and S. Belle Avenue that would ensure access for emergency vehicles within the interior of the site. Additionally, access to and from the Project site for emergency vehicles would be reviewed and approved by the Corona Fire Department and the City as part of the Project approval process to ensure the proposed Project is compliant with all applicable codes and ordinances for emergency vehicle access. As a result, the proposed Project would not impair an adopted emergency response plan or emergency evacuation plan and impacts would not occur.

b. Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from wildfire or the uncontrolled spread of a wildfire

No Impact. As described in the previous response, the Project site is not located within a Very High Fire Hazard Severity Zone. The Project site is in an urbanized area and surrounding land uses are fully developed, lacking vegetation necessary for the uncontrolled spread of a wildfire. Further, the areas within the Project’s vicinity do not contain hillsides or other factors that could exacerbate wildfire risks. Therefore, no impact would occur.

c. Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment

No Impact. As described in the previous responses, the Project site is not within a Very High Fire Hazard Severity Zone, and the Project does not include infrastructure that could exacerbate fire risks. Although the Project includes new driveways within the Project site and other utility offsite improvements, the Project does not include any changes to public or private roadways that would exacerbate fire risk or that would result in impacts to the environment. Project design and implementation of utility improvements would also be reviewed and approved by the City as part of the Project approval process to ensure the proposed Project is compliant with all applicable design standards and regulations. Therefore, the proposed Project would not include infrastructure (such as roads, fuel breaks, emergency water sources, power lines, or

other utilities), that would exacerbate fire risk or that would result in impacts to the environment. Therefore, no impacts would occur.

d. Expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability or drainage changes

No Impact. According to Figure 5-14 of the Technical Background Report prepared for the City’s General Plan, the Project site and surrounding areas are fully developed and are not subject to wildland fire hazards (Corona, 2020a, Technical Background Report, Figure 5-14). Due to the developed nature of the Project vicinity, the Project would not expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire slope instability or drainage changes. No impact would occur.

20. ENERGY:	Potentially Significant Impact	Potentially Significant Unless Mitigation Incorporated	Less than Significant Impact	No Impact
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a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation

b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency

Discussion

In order to evaluate the Project’s potential impacts due to energy demand, a site-specific technical report was prepared titled “Energy Consumption Assessment” (herein, “EA”), prepared by ECORP Consulting, Inc., dated April 2024 (Appendix L). Please refer to the EA for a discussion of existing conditions, a discussion of applicable regulatory requirements, and a description of the methodology used to estimate the Project’s energy demand.

a. Result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation.

Less than Significant Impact.

The Project proposes the construction and operation of a 40,000 square foot supermarket, the remodel of an existing bank into a 3,297 square foot restaurant, 3,633 square foot bank, and 261 parking spaces. It is noted that the existing Project Site currently accommodates an existing retail building currently serving as a strip mall and a drive-in bank that would be demolished as a part of the Project.

For the purpose of this analysis, the amount of operational electricity and natural gas to be consumed by the Project is quantified and compared to the relative amount consumed by all nonresidential land uses in Riverside County. The amount of fuel necessary for Project construction is calculated and compared to that consumed in Riverside County. Similarly, the amount of fuel necessary for Project operations is calculated and compared to that consumed in Riverside County. Energy consumption associated with the Proposed Project is summarized in Table 20-A.

TABLE 20-A: PROPOSED PROJECT ENERGY AND FUEL CONSUMPTION

Energy Type	Annual Energy Consumption	Percentage Increase Countywide
Building Energy Consumption		
Electricity Consumption ¹	1,578,730 kilowatt-hours	0.018 percent
Natural Gas Consumption ¹	12,035 therms	0.008 percent
Automotive Fuel Consumption		
Project Construction Calendar Year One ²	37,833 gallons	0.006 percent
Project Construction Calendar Year Two ²	18,818 gallons	0.003 percent
Project Operations ³	476,722 gallons	0.070 percent

Source: ¹CalEEMod; ²Climate Registry 2016; ³EMFAC2021 (CARB 2022)

Notes: The Project increases in electricity consumption and natural gas consumption are compared with all nonresidential uses in Riverside County in 2022, the latest data available. The Project increases in construction and operational fuel consumption are compared with the anticipated countywide fuel consumption in 2023, the most recent full year of data.

Fuel necessary for Project construction would be required for the operation and maintenance of construction equipment and the transportation of materials to the Project Site. The fuel expenditure necessary for construction activities would be temporary, lasting only as long as Project construction. As indicated in Table 2-4, the Project’s gasoline fuel consumption during the one-time construction period is estimated to be 37,833 gallons during the first calendar year of construction and 18,818 during the second year of construction. This would increase the annual countywide gasoline fuel use in the county by 0.006 percent and 0.003 percent, respectively. As such, Project construction would have a nominal effect on local and regional energy supplies. No unusual Project characteristics would necessitate the use of construction equipment that would be less energy efficient than at comparable construction sites in the region or the state. Construction contractors would purchase their own gasoline and diesel fuel from local suppliers and would judiciously use fuel supplies to minimize costs due to waste and subsequently maximize profits. Additionally, construction equipment fleet turnover and increasingly stringent state and federal regulations on engine efficiency combined with state regulations limiting engine idling times and requiring recycling of construction debris, would further reduce the amount of transportation fuel demand during Project construction.

Operations of the proposed Project would include electricity and natural gas consumption. As shown in Table 2-4, the annual electricity consumption due to operations would be 1,578,730 kilowatt-hours resulting in a negligible increase of approximately 0.018 percent in the typical annual electricity consumption attributable to all nonresidential uses in Riverside County. However, this is potentially a conservative estimate. In September 2018 Governor Jerry Brown Signed EO B-55-18, which established a new statewide goal “to achieve carbon neutrality as soon as possible, and no later than 2045, and achieve and maintain net negative emissions thereafter.” Carbon neutrality refers to achieving net zero carbon dioxide (CO₂) emissions. This can be achieved by reducing or eliminating carbon emissions, balancing carbon emissions with carbon removal, or a combination of the two. This goal is in addition to existing statewide targets for greenhouse gas emission reduction. Governor’s Executive Order B-55-18 requires CARB to “work with relevant state agencies to ensure future Scoping Plans identify and recommend measures to achieve the carbon neutrality goal. Natural gas consumption due to operations would be 12,035 therms resulting in an imperceptible increase (0.008 percent) in the typical annual natural gas consumption attributable to all nonresidential uses in Riverside County.

Project is estimated to generate approximately 3,377 daily vehicle trips (Linscott, Law, and Greenspan Engineers 2024). As indicated in Table 2-4, this would equate to a consumption of approximately 476,722 gallons of automotive fuel per year, which would lead to a Countywide percentage increase in fuels consumption of approximately 0.07 percent. As previously mentioned, this represents a conservative estimate due to the fact that the existing Project Site currently accommodates retail, single family, and bank land uses, which contribute to currently countywide automotive fuel consumption. Therefore, the actual increase of automotive fuel consumption over existing conditions would actually be less than what is stated above. Nevertheless, the proposed Project’s fuel consumption associated with the vehicle trips generated by the Project during operations would not be considered inefficient, wasteful, or unnecessary in comparison to other similar developments in the region.

b. Conflict with or obstruct a state or local plan for renewable energy or energy efficiency

Less than Significant Impact.

The State’s IEPR (Integrated Energy Policy Report) provides policy recommendations to be implemented by energy providers in California. Electricity would be provided to the Project by SCE. SCE has various programs to support cleaner and more

sustainable power. For instance, SCE has expanded in developing their portfolio of solar, wind, and hydropower technology. Furthermore, SCE had developed its own Climate Adaptation Community Engagement Plan, along with several other plans to address climate change vulnerabilities, clean power initiatives, long-term upgrades to the grid, and reducing greenhouse gases from electricity generation. Therefore, SCE is consistent with, and would not otherwise interfere with, nor obstruct implementation of the goals presented in the 2023 IEPR. Thus, because the SCE is consistent with the 2023 IEPR, the proposed Project is consistent with, and would not otherwise interfere with, nor obstruct implementation of the goals presented in the 2023 IEPR.

The Project would be designed in a manner that is consistent with relevant energy conservation plans designed to encourage development that results in the efficient use of energy resources. The City's General Plan has several goals and policies that ensure that new developments are energy efficient and generally uphold all local and state energy efficiency standards. Additionally, the General Plan contains several energy consumption-reducing policy provisions specific to new land use development projects such as Policy LU-6.2, which requires that new residential, commercial, office, and industrial development be designed to minimize consumption of and sustain scarce environmental resources by the requirement to install energy- and water efficient fixtures, recycled building materials, insulation and wall thickness, and permeable paving surfaces. City General Plan Policy IU-7.1. requires that new development is approved contingent upon its ability to be served with adequate natural gas, energy facilities, and other critical infrastructure and Policy ER-12.14 mandates the reduction of energy consumed by commercial and residential uses by requiring the use and installation of energy conservation features in all new construction projects and wherever feasible, retrofitting existing and redevelopment projects. The proposed Project would be subject to implementation of these General Plan policy provisions. All development in the City, including the Project, is required to adhere to all applicable City-adopted policy provisions, including those contained in the City General Plan. The City ensures all applicable provisions of the General Plan are incorporated into projects and their permits through development review and applications of conditions of approval as applicable.

The Project will be built to the Energy Efficiency Standards for Residential and Nonresidential Buildings, as specified in Title 24, Part 6, of the California Code of Regulations (Title 24). Title 24 was established in 1978 in response to a legislative mandate to reduce California's energy consumption. Title 24 is updated approximately every three years; the 2019 Title 24 updates went into effect on January 1, 2020. The 2022 standards went into effect became effective January 1, 2023. The 2022 Energy Standards improve upon the 2019 Energy Standards for new construction of, and additions and alterations to, residential and nonresidential buildings. The 2022 update to the Energy Standards focuses on several key areas to improve the energy efficiency of newly constructed buildings and additions and alterations to existing buildings, encouraging better energy efficiency, strengthening ventilation standards, and more. The 2022 Energy Standards are a major step toward meeting Zero Net Energy. Buildings permitted on or after January 1, 2023, must comply with the 2022 Standards. Compliance with Title 24 is mandatory at the time new building permits are issued by city and county governments. Furthermore, the Air Quality and Greenhouse Gas Assessment prepared for the Proposed Project requires the implementation of Mitigation Measure GHG-1, which ensures that measures within the City's Climate Action Plan are integrated into the Project's operation (ECORP Consulting Inc. 2024). MM GHG-1 (Greenhouse Gas Reduction) would require that the proposed Project demonstrate consistency with the City of Corona Climate Action Plan (CAP) Commercial Development GHG Emission Screening Table, providing for a minimum 100 points per the City Screening Tables. The Project must be consistent with the CAP's requirement to achieve at least 100 points. The City of Corona Planning Division shall verify incorporation of the identified Screening Table Measures within the Project building plans and Site designs prior to the issuance of building permit(s) and shall verify implementation of the identified Screening Table Measures prior to the issuance of Certificate(s) of Occupancy. As such, the Project would adhere to the policies within the General Plan's planning documents that promote energy efficiency measures.

As discussed above, the proposed Project would not conflict with any of the State or local for renewable energy or energy efficiency. As such, impacts would be less than significant.

21. PREVIOUS ENVIRONMENTAL ANALYSIS:

Earlier analysis may be used when one or more of the environmental effects have been adequately analyzed in an earlier EIR or Negative Declaration (Section 15063).

DOCUMENTS INCORPORATED BY REFERENCE:

1. City of Corona General Plan 2020-2040. Available online:
[General Plan 2020-2040](#)
2. City of Corona Technical Background Update EIR, 2019. Available online:
[Volume 1](#) ; [Volume 2a](#) ; [Volume 2b](#)
3. City of Corona Municipal Code. Available online:
https://codelibrary.amlegal.com/codes/corona/latest/corona_ca/0-0-0-33689
4. Geotechnical Engineering Investigation Report prepared by SALEM Engineering Group on December 20, 2022, and revised on April 3, 2024 (Appendix A).
5. Project Specific Water Quality Management Plan (WQMP), prepared by Webb Associates, October 9, 2025 (Revised) (Appendix B).
6. Preliminary Drainage Study, prepared by Webb Associates, October 2025 (Revised) (Appendix C).
7. Water and Sewer Study Report, prepared by Webb Associates, October 2025 (Revised) (Appendix D).
8. Air Quality & Greenhouse Gas Emissions Assessment prepared by ECORP Consulting, Inc., dated October 2025 (Appendix E).
9. Traffic Impact Analysis (TIA) prepared by Linscott, Law, and Greenspan Engineers, dated July 10, 2025 (Appendix F).
10. Biological Technical Report (BTR) and Western Riverside County MSHCP (Multiple Species Habitat Conservation Plan) Consistency Analysis prepared by ECORP Consulting, Inc. dated February 2024 (Appendix G).
11. Phase I Environmental Site Assessments (ESAs) prepared by EEC Environmental, dated October 18, 2021 and November 29, 2021, Limited Subsurface Assessments prepared by EEC Environmental dated December 9, 2021 and January 24, 2022, respectively; and Phase 1 ESA prepared by Ramboll, dated May 20252024 (Appendix H).
12. Noise Impact Assessment prepared by ECORP Consulting, Inc., dated November 2024 (Appendix I).
13. Cultural Resources Records Search Results prepared by ECORP Consulting, Inc., dated December 13, 2023 (Appendix J).
14. Paleontological Assessment prepared by ECORP Consulting, Inc., dated December 5, 2023 (Appendix K).
15. Energy Consumption Assessment, prepared by ECORP Consulting, Inc., dated April 2024 (Appendix L).



**MITIGATION MONITORING AND REPORTING PROGRAM
CITY OF CORONA**

No.	Mitigation Measures	Implementation Action	Method of Verification	Timing of Verification	Responsible Person	Verification Date
Biological Resources						
MM AQ-1	<p>Construction Related Emissions. Prior to the issuance of a grading and building permit for the Corona Northgate Market Project, the Project Applicant shall add the following construction note on the grading and building plans, and demonstrate to the satisfaction of the City of Corona Planning and Development Department that the following measure is implemented during Project construction.</p> <ul style="list-style-type: none"> All offroad equipment of greater than 50 horsepower used in the site preparation phase of Project construction shall be California Air Resources Board (CARB) Tier 4 Certified, as set forth in Section 2423 of Title 13 of the California Code of Regulations, and Part 89 of Title 40 of the Code of Federal Regulations. 	Condition of Approval	Submittal of grading and construction plans	Prior to the issuance of a grading and building permit, and during construction	Planning & Development Department – Planning & Building Divisions	
MM BIO-1	<p>Pre-Construction Nesting Bird Survey. If grading activities occur within the active breeding season for birds (January 15-September 15), the applicant shall retain a qualified biologist that is familiar with local birds and their nesting behaviors to conduct a nesting bird survey no more than 3 days prior to commencement of construction activities. The nesting bird survey shall be submitted to the City of Corona Planning</p>	Condition of Approval	Submittal of nesting bird survey	Prior to issuance of grading permit	Project Applicant, Project Biologist/Planning and Development Department – Planning Division	

NORTHGATE GONZALEZ MARKET & COMMERCIAL PROJECT - IS/MND

No.	Mitigation Measures	Implementation Action	Method of Verification	Timing of Verification	Responsible Person	Verification Date
	<p>and Development Department, Planning Division prior to issuance of a grading permit. The nesting bird survey shall include the Project Site and areas immediately adjacent to the site that could potentially be affected by Project-related construction activities, such as noise, human activity, and dust, etc. If active nesting of birds is observed within 100 feet of the designated construction area prior to construction, the qualified biologist shall establish an appropriate buffer around the active nests (e.g. 200 feet and/or subject to the recommendations of the qualified biologist), and a biological monitor shall visit the site once a week during ground disturbing activities to ensure all fencing is in place and no nesting birds are being impacted.</p>					
MM BIO-2	<p>Burrowing Owl Survey. Prior to the issuance of a grading permit, the applicant shall submit a 30-day preconstruction survey to the City of Corona Planning & Development Department, Planning Division to ensure that no burrowing owls have colonized the site in the days or weeks preceding Project activities. If burrowing owls are found to have colonized the Project Site prior to the initiation of construction, the Project applicant shall immediately inform the City of Corona Planning & Development Department, Western Riverside County Regional Conservation Authority (RCA) and the Wildlife Agencies prior to initiating ground disturbance. If ground-disturbing activities occur but the site is left undisturbed for more than 30 days, a pre-construction survey shall again be necessary to ensure burrowing owl has not colonized the site since it was last disturbed. If burrowing owl is found, the same coordination with the City of Corona, RCA and/or Wildlife Agencies shall be necessary.</p>	Condition of Approval	Submittal of burrowing owl survey	Prior to issuance of grading permit	Project Applicant, Project Biologist/Planning and Development Department – Planning Division	
MM BIO-3	<p>Tree Avoidance and Removal Process. If trees are scheduled to be removed (e.g., relocating) and/or modified (i.e., trimming), the</p>	Condition of Approval	Submittal of a bat survey, tree removal schedule,	Prior to issuance of grading permit	Project Applicant, Project Biologist/Planning and	

No.	Mitigation Measures	Implementation Action	Method of Verification	Timing of Verification	Responsible Person	Verification Date
	<p>applicant shall retain a qualified bat biologist to determine if the trees are suitable for bat roosting. If the trees scheduled for removal are determined to be suitable for bat roosting, these activities shall be scheduled during seasonal periods of bat activity - September 1 to October 15 - or when evening temperatures are above 45 degrees Fahrenheit, and rain is less than ½ inch in 24 hours; or between March 1 to April 1 with the same parameters.</p> <p>1. If tree removal/modification occurs during the maternity season (generally April 15 to August 31), a qualified bat biologist shall conduct a focused emergence survey(s) of the tree(s) within 48 hours of scheduled work. If a maternity roost is located, whether solitary or colonial, that roost shall remain undisturbed until after the maternity season or until a qualified biological monitor has determined the roost is no longer active.</p> <p>2. If work is expected to occur outside of the bat maternity season, work adjacent to trees suitable as bat habitat can continue without additional surveying efforts. If trees with suitable bat roosting habitat are scheduled for removal or relocation, tree removal during the weather parameters described above using the two-step method shall be conducted:</p> <ul style="list-style-type: none"> • As much as feasible, vegetation and trees within the area that are not suitable for roosting bats shall be removed first to provide a disturbance that might reduce the likelihood of bats using the habitat. • Two-step tree removal shall occur over two consecutive days under the supervision of a qualified bat biologist. On Day 1, small branches and small limbs containing no cavity, crevice, or 		<p>and other documentation as necessary evidencing that mitigation is being met.</p>		<p>Development Department – Planning Division</p>	

No.	Mitigation Measures	Implementation Action	Method of Verification	Timing of Verification	Responsible Person	Verification Date
	<p>exfoliating bark habitat on habitat trees (or outer fronds in the case of palm trees), as identified by a qualified bat biologist are removed first, using chainsaws only (i.e., no dozers, backhoes). The following day (Day 2), the remainder of the tree is to be felled/removed. The intention of this method is to disturb the tree with noise and vibration on Day 1 during branch removal. This should cause any potentially present day-roosting bats to abandon the roost tree after they emerge for nighttime foraging. Removing the tree quickly the next consecutive day should avoid reoccupation of the tree by bats.</p>					
Cultural Resources and Tribal Cultural Resources						
MM CUL-1	<p>Archaeological Monitoring. Prior to the issuance of a grading permit, the Project Applicant shall retain and enter a monitoring and mitigation service contract with a qualified Archaeologist (“Archaeological Monitor”) for mitigation monitoring services and implement a Cultural Resource Monitoring Program (CRMP). At least 30 days prior to issuance of grading permits, a copy of the executed agreement between the Project Applicant and Archaeologist shall be submitted to the Planning and Development Department:</p> <ul style="list-style-type: none"> • A CRMP shall be prepared to guide the procedures and protocols of an archaeological mitigation monitoring program that shall be implemented during initial onsite and offsite ground disturbing activities. The CRMP shall include, but not be limited to, the Project grading and development schedule; approved Project cultural resources mitigation measures and conditions of approval; monitoring procedures; protocols for the identification, assessment, collection, and analysis of any resource(s) 	Condition of Approval	Submittal of an executed agreement showing that an archaeologist has been retained for the Project.	Prior to issuance of grading permits and during grading activities	Project Applicant, Project Archaeologist/Planning and Development Department – Planning Division	

NORTHGATE GONZALEZ MARKET & COMMERCIAL PROJECT - IS/MND

No.	Mitigation Measures	Implementation Action	Method of Verification	Timing of Verification	Responsible Person	Verification Date
	<p>observed during grading; curation guidelines; and coordination with project personnel, City staff, and any participating Native American tribe(s). The Rincon Band of Luiseño Indians shall be notified of any discoveries. The final CRMP shall be submitted to the City Project planner and/or inspector, the appropriate Project supervisor/engineer/etc., and monitoring Native American tribe(s), if any.</p> <ul style="list-style-type: none"> • The Archaeological Monitor shall be invited to a preconstruction meeting with construction personnel and City and tribal representatives. The attending archaeologist shall review the provisions of the CRMP and answer any applicable questions. • Full-time monitoring shall occur throughout the entire Project area, including all off-site improvement areas, during initial ground-disturbing activities. Full-time monitoring shall continue until the Archaeological Monitor determines that the overall sensitivity of the Project area is low as a result of mitigation monitoring and shall have the authority to modify and reduce the monitoring program to either periodic spot-checks or complete suspension of the monitoring program. Should the monitor(s) determine that there are no cultural resources within the Project site or off-site improvement areas, or should the sensitivity be reduced to low during monitoring, all monitoring shall cease. 					
MM CUL-2	<p>Inadvertent Discovery and Native American Notification. In the event that a significant cultural resource is discovered during ground disturbance activities, the project archaeologist shall notify the City and the Rincon Band of Luiseño Indians for purposes of inviting the Tribe to participate in the CRMP implementation and to observe any continuing ground-disturbing construction activities. Further, all ground disturbance activities within 50 feet of the</p>	Condition of Approval	Submittal of an executed agreement showing that a Native American Monitor has been retained for the Project.	Prior to issuance of grading permits and during grading activities	Project Applicant, Project Archaeologist, Planning and Development Department – Planning Division, Native American Monitor	

No.	Mitigation Measures	Implementation Action	Method of Verification	Timing of Verification	Responsible Person	Verification Date
	<p>discovered cultural resource shall be halted and the applicant and a meeting shall be convened between the developer, the consulting archaeologist, the lead agency and a Rincon tribal representative to discuss the significance of the find. Further ground disturbance shall not resume in the area of the discovery until the appropriate treatment has been accomplished.</p>					
<p>MM CUL-3</p>	<p>Paleontological Monitor. Prior to the issuance of grading permits, the Project Applicant shall submit to and receive approval from the City of a Paleontological Resources Monitoring and Mitigation Plan (PRMMP). The PRMMP shall include the provision of a trained paleontological monitor during onsite soil disturbance activities. The monitoring for paleontological resources shall be conducted on a full-time basis during the rough grading phases of the Project site within native soils that have the potential to harbor paleontological resources. The paleontological monitor shall be equipped to rapidly remove any large fossil specimens encountered during excavation. During monitoring, samples of soil shall be collected and processed to recover micro-vertebrate fossils. Processing shall include wet screen washing and microscopic examination of the residual materials to identify small vertebrate remains. If paleontological resources are unearthed or discovered during grading activities, the following recovery processes shall apply:</p> <ul style="list-style-type: none"> • Upon encountering a large deposit of bone, salvage of all bone in the area shall be conducted with additional field staff and in accordance with modern paleontological techniques. • All fossils collected during the project shall be prepared to a reasonable point of identification. Excess sediment or matrix shall be removed from the specimens to reduce the bulk and cost 	<p>Condition of Approval</p>	<p>Submittal of a Paleontological Resources Monitoring and Mitigation Plan</p>	<p>Prior to issuance of grading permits and during grading activities</p>	<p>Project Applicant, Planning and Development Department – Planning Division, Paleontological Monitor</p>	

No.	Mitigation Measures	Implementation Action	Method of Verification	Timing of Verification	Responsible Person	Verification Date
	<p>of storage. Itemized catalogs of all material collected and identified shall be provided to the museum repository along with the specimens.</p> <ul style="list-style-type: none"> • A report documenting the results of the monitoring and salvage activities and the significance of the fossils shall be prepared. • All fossils collected during this work, along with the itemized inventory of these specimens, shall be deposited in a museum repository (such as the Western Science Center for Archaeology & Paleontology, the Riverside Metropolitan Museum, or the San Bernardino County Museum) for permanent curation and storage. 					
<p>MM CUL-4</p>	<p>Discovery of Human Remains: In the event that human remains (or remains that may be human) are discovered at the project site during grading or earthmoving activities, the construction contractors, project archaeologist, and/or designated Native American Monitor shall immediately stop all activities within 100 feet of the find. The project proponent shall then inform the Riverside County Coroner and the City of Corona Planning and Development Department, Planning Division, immediately, and the coroner shall be permitted to examine the remains as required by California Health and Safety Code Section 7050.5(b). Section 7050.5 requires that excavation be stopped in the vicinity of discovered human remains until the coroner can determine whether the remains are those of a Native American. If human remains are determined as those of Native American origin, the applicant shall comply with the state relating to the disposition of Native American burials that fall within the jurisdiction of the Native American Heritage Commission (PRC Section 5097). The coroner shall contact the NAHC to determine the</p>	<p>Condition of Approval</p>	<p>Submittal of documentation</p>	<p>If human remains are discovered during ground-disturbing construction activities</p>	<p>Construction Contractor(s), Project Applicant, County Coroner, NAHC</p>	

No.	Mitigation Measures	Implementation Action	Method of Verification	Timing of Verification	Responsible Person	Verification Date
	<p>most likely descendant(s) (MLD). The MLD shall complete his or her inspection and make recommendations or preferences for treatment within 48 hours of being granted access to the site. The Disposition of the remains shall be overseen by the most likely descendant(s) to determine the most appropriate means of treating the human remains and any associated grave artifacts.</p> <p>The specific locations of Native American burials and reburials will be proprietary and not disclosed to the general public. The locations will be documented by the consulting archaeologist in conjunction with the various stakeholders and a report of findings will be filed with the Eastern Information Center (EIC).According to California Health and Safety Code, six or more human burials at one location constitute a cemetery (Section 8100), and disturbance of Native American cemeteries is a felony (Section 7052) determined in consultation between the project proponent and the MLD. In the event that the project proponent and the MLD are in disagreement regarding the disposition of the remains, State law will apply and the median and decision process will occur with the NAHC (see Public Resources Code Section 5097.98(e) and 5097.94(k)).</p>					
MM GHG-1	<p>City of Corona Screening Tables for GHG Reduction.</p> <p>Project shall adhere to the Commercial Development screening tables for compliance in reducing greenhouse gas emissions. The screening tables are to provide guidance in measuring GHG reductions attributable to certain design and construction measures incorporated into development projects.</p>	Condition of Approval	Submittal of building plans	Prior to issuance of building permit and occupancy permits	Planning & Development Department – Building and Planning Divisions	

Fw: Gm

From J Blue <jpblue1978@hotmail.com>
Date Mon 11/17/2025 10:18 AM
To Rocio Lopez <Rocio.Lopez@coronaca.gov>
Cc J Blue <jpblue1978@hotmail.com>

You don't often get email from jpblue1978@hotmail.com. [Learn why this is important](#)

[CAUTION] DO NOT CLICK links or attachments unless you recognize the sender and know the content is safe.

From: Blue Family <jpb03021978@icloud.com>
Sent: Monday, November 17, 2025 8:49 AM
To: Rocio.Lopez@coronaca.gove <Rocio.Lopez@CoronaCA.gove>
Cc: Jeffrey Blue <jpblue1978@hotmail.com>
Subject: Gm

I believe that the Northgate Grocery store is not right for that location period. It does not serve the purpose for a Downtown. I believe this is a horrible idea for a diverse Corona that is looking for Retail, Restaurants, and Entertainment period. A grocery store will do nothing for Fun and Excitement. We have too many grocery stores already that are very close to downtown. Doesn't make any sense at all!!!!

For that location what should be considered: **Round 1, Main Event, Dave and Busters, New Gym with various eateries on same location (Jamba Juice, Daves Chicken, Korean Barbecue, GNC, Boba, Yogurtland, Sees Candy, Pho, 5 Guys, Chick Fill A, Panera, Starbucks drive thru.....and so on.**

Let's do this project right!!!! Please!!!!

Our downtown that will serve the purpose of a citizen / outsider to visit with his or her family. Parking all day and walking around into the night. While every tangible and intangible serves all the senses for everyone on site. Everyone's senses are to be fulfilled!!! By smell, taste, touch, sight and or ear!!!! Can you imagine the sounds of Christmas music being played and lights on display. While eating sugary treats from all the different sugary locations. The smell of all different kinds of things as well such as; Hot Chocolate, Coffee, Sweets, Burgers, Fries, Barbecue, Mexican, Thai, Chinese, Mediterranean, Steak, Sea Food. Wow!!!! The sounds of Christmas music being played by a band and or live DJ!!! As we walk around shopping for gifts!!! Downtown should be a awesome experience for ALL!!!

Planning and Housing Commission Secretary

Questions on Public Hearing

On December 8th

RECEIVED

DEC 03 2025

Planning and Development Dept

- ① How will delivery of product effect the residents on Belle Ave with the closing of 4th & 5th street?
- ② Are the roads equipped to carry excess weight of semi delivery trucks?
- ③ Have you looked into traffic data on 3rd & Belle Ave?
There have been many traffic accidents at this intersection.
- ④ Have you reached out to local community asking them what they want in the area?
- ⑤ Is this just the beginning of the Downtown area.
Are there plans or talk of getting rid of the single family homes in the surrounding area?



Yana Garcia
Secretary for
Environmental Protection



Department of Toxic Substances Control

Katherine M. Butler, MPH, Director
8800 Cal Center Drive
Sacramento, California 95826-3200
dtsc.ca.gov



Gavin Newsom
Governor

SENT VIA ELECTRONIC MAIL

November 25, 2025

Rocio Lopez
Senior Planner
City of Corona
400 S. Vicentia Avenue
Corona, CA 92882
rocio.lopez@coronaca.gov

RE: MITIGATED NEGATIVE DECLARATION FOR THE NORTHGATE GONZALEZ
MARKET DATED NOVEMBER 17, 2025, STATE CLEARINGHOUSE NUMBER
[2025110646](#)

Dear Rocio Lopez,

The Department of Toxic Substances Control (DTSC) reviewed the Mitigated Negative Declaration (MND) for the Northgate Gonzalez Market (Project). The proposed Project consists of a General Plan Amendment (GPA), Specific Plan Amendment (SPA), Parcel Map 38981 and Precise Plan applications. Additionally, a review of the site plan, architecture, perimeter walls/fencing and landscaping for a 40,000 square foot Northgate Gonzalez grocery market and remodel of an existing 6,930 square foot bank building into a bank and restaurant use on a combined acreage of 4.88 net acres consisting of 13 combined parcels. The GPA will change land use of two parcels from Low Density Residential and General Commercial to Mixed Use Downtown; the SPA will change zoning of two parcels from Single Family and Gateway Business to Downtown. The Parcel Map will merge 13 parcels into one parcel to accommodate the proposed Project. DTSC recommends and requests consideration of the following comments:

1. If buildings or other structures are to be demolished on any Project sites included in the proposed Project, surveys should be conducted for the presence of lead-based

EXHIBIT 18

paints or products, mercury, asbestos containing materials, and polychlorinated biphenyl caulk. Removal, demolition, and disposal of any of the above-mentioned chemicals should be conducted in compliance with California environmental regulations and policies. In addition, sampling near current and/or former buildings should be conducted in accordance with [DTSC's Preliminary Endangerment Assessment \(PEA\) Guidance Manual](#).

2. DTSC recommends all imported soil/fill material be tested to ensure all COCs meet screening levels as outlined in [DTSC's PEA Guidance Manual](#). Furthermore, DTSC advises referencing the [DTSC Information Advisory Clean Imported Fill Material Fact Sheet](#) if importing fill is necessary. To minimize the possibility of introducing contaminated soil/fill material there should be documentation of the origins of the soil/fill material and, if applicable, sampling be conducted to ensure that the imported soil/fill material are suitable for the intended land use. The soil sampling should include analysis based on the source of the soil/fill and knowledge of prior land use.
3. The City of Corona should consider soil testing as mentioned in comment #2. If, in the event any COC results are above DTSC residential screening levels, DTSC recommends the City of Corona address the contaminations within the Project area through an Environmental Site Assessment and/or receive oversight from a [self-certified local agency](#), DTSC or Regional Water Quality Control Board. If entering into one of DTSC's voluntary agreements, please note that DTSC uses a single standard Request for Lead Agency Oversight Application for all agreement types. Please apply for DTSC oversight using this link: [Request for Agency Oversight Application](#). Submittal of the online application includes an agreement to pay costs incurred during agreement preparation. If you have any questions about the application portal, please contact the relevant [Regional Brownfield Coordinator](#) for your Project.

DTSC would like to thank you for the opportunity to comment on the MND for the Northgate Gonzalez Market. Thank you for your assistance in protecting California's people and environment from the harmful effects of toxic substances. If you have any

Rocio Lopez
November 25, 2025
Page 3

questions or would like clarification on DTSC's comments, please respond to this letter or via our [CEQA Review email](#) for additional guidance.

Sincerely,



Dave Kereazis
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HWMP-Permitting Division – CEQA Unit
Department of Toxic Substances Control
Dave.Kereazis@dtsc.ca.gov

cc: (via email)

Governor's Office of Land Use and Climate Innovation
State Clearinghouse
state.clearinghouse@lci.ca.gov

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California Department of Transportation

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December 11, 2025

Route & Postmile #: SR-91/ PM 6.252
Cross Street: W. Sixth Street & S. Main Street
GTS ID: 38530
SCH #: 2025110646

City of Corona
Planning Division
Attn: Rocio Lopez
400 S Vicentia Ave,
Corona, CA 92882

Subject: Caltrans LDR Branch Review of the Corona Northgate Gonzalez Market Project

The California Department of Transportation (Caltrans) Local Development Review (LDR) Branch has completed its review of the Northgate González Market Mitigated Negative Declaration (MND) and related Traffic Impact Analysis (TIA). The project site is located in the City of Corona at the northwest corner of W. Sixth Street and S. Main Street, encompassing 13 parcels as well as portions of W. Fifth Street and W. Fourth Street. The site is approximately 1,200 feet from the State Route 91 (SR-91) and S. Main Street interchange.

The project site is partially vacant and currently includes a strip retail building slated for demolition, a vacant bank, and two vacant parcels along W. Fourth Street. Existing zoning includes Downtown (D), Single Family (SF), and Gateway Business (GB), with General Plan land use designations of Mixed Use Downtown (MUD), Low Density Residential (LDR), and General Commercial (GC).

The proposed Project includes a General Plan Amendment (GPA2024-0003), Specific Plan Amendment (SPA2024-0003), Parcel Map 38981 (PM 38981), and Precise Plan (PP2024-0001) to allow development of a 40,000-square-foot Northgate González grocery store and the remodel of an existing 6,930-square-foot bank building into a bank and restaurant use. The 4.88-acre site consists of 13 parcels that will be merged into one through PM 38981.

The GPA would change the land use of two parcels from Low Density Residential (LDR) and General Commercial (GC) to Mixed Use Downtown (MUD). The SPA would rezone these parcels from Single Family (SF) and Gateway Business (GB) to Downtown (D). The Project also includes new landscaping, parking, and public right-of-way improvements, including sidewalks, curb and gutter, utilities, and stormwater infrastructure upgrades.

**Public correspondence received after
Planning & Housing agenda was posted.**

"Improving lives and communities through transportation"

Based on the information available, we are submitting the following comments and recommendations for your consideration:

Local Development Review

Vehicle Miles Traveled (VMT):

Although the project's VMT impacts are considered less than significant, we strongly recommend implementing VMT reduction strategies wherever feasible. Such measures can help advance long-term sustainability goals, enhance multimodal accessibility, and support environmentally responsible development. Below are a few recommendations that may be appropriate for this project:

1. Install bicycle racks near the front entrance of the market to provide convenient and secure parking for cyclists.
2. Continue developing a cohesive and functional bicycle network within the City of Corona to better connect residents to the proposed project.

Traffic Operations

General:

1. Update the PE stamp on TIA as it has expired.
2. Include truck turning templates for nearby intersections on the State Highway System (SHS).

Traffic Impact Analysis:

3. Include horizon year scenario with and without the project in the traffic analysis.
4. Include queuing analysis for all scenarios at locations on the State Highway System (SHS).
5. Use a speed of 40 mph for all on and off ramps for the SR-91 and Main St interchange VISTRO analysis.

Safety Review:

6. Consider preparing a Traffic Safety Review as a stand-alone report for proposed land use projects and plans affecting the State Highway System. [Local Development Review \(LDR\) Safety Review Practitioner's Guidance](#).
 - a) Please analyze the existing crash data and discuss project's impact on safety as needed.
 - b) To request crash data on the State Highway System, please submit your request via our California Public Records Act (CPRA) portal at mycusthelp.com.
 - c) Please provide appropriate countermeasures (if any) to mitigate/reduce project's impact.
7. Please analyze safety concerns related to the increase in traffic volumes and potential queuing, propose appropriate mitigation measures as necessary.

Equitable Access

If any Caltrans facilities are impacted by the project, they must comply with American Disabilities Act (ADA) Standards upon project completion. Additionally, the project must ensure the maintenance of bicycle and pedestrian access throughout the construction phase. These access considerations align with Caltrans' equity mission to provide a safe, sustainable, and equitable transportation network for all users.

Caltrans Encroachment Permit

Be advised that any permanent work or temporary traffic control that encroaches onto Caltrans' right-of-way requires a Caltrans-issued encroachment permit.



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MEMORANDUM

To: Ms. Rocio Lopez, MPA
 City of Corona
 Date: December 23, 2025

From: Keil D. Maberry, P.E., Principal
 Angela Besa, P.E., Transportation Engineer III
 Linscott, Law & Greenspan, Engineers
 LLG Ref: 2.24.4620.1

Subject: ***Response to Comments Memorandum for Northgate Market – Corona***

Linscott, Law & Greenspan, Engineers (LLG) is pleased to provide the following responses to address comments provided in the Caltrans District 8 LDR Branch Review Letter, dated December 11, 2025, of the *Traffic Impact Analysis Report for Northgate Market*, dated July 10, 2025. The Caltrans Letter is attached and referenced in our responses provided below.

Response to Caltrans Traffic Analysis Comments:

- **Response to Local Development Review:** Comment acknowledged.
 1. Bicycle racks will be provided consistent with City of Corona requirements.
 2. The bicycle network will be developed within the vicinity of the Project site consistent with City of Corona requirements.

▪ **Response to Traffic Operations:**

General:

1. Comment acknowledged. The PE stamp will be updated accordingly.
2. Given that the Main Street/SR-91 Interchange was recently reconstructed in conjunction with the SR-91 Freeway Project, the ramps were reconstructed to adequately accommodate large truck movements

Traffic Impact Analysis:

3. Horizon analysis is not required per City of Corona TIA Guidelines.
4. Comment acknowledged. Review of the level of service analyses within the TIA revealed no freeway ramp queuing deficiencies at SR-91/Main Street Interchange.
5. Comment acknowledged. Using 40 mph would not change the acceptable queuing results.

Safety Review:

6. Comment acknowledged. The Project has no direct access to the State Highway System (SHS).
 - a) Not applicable (No direct SHS access)
 - b) Not applicable (No direct SHS access)

David S. Shender, PE
 John A. Boarman, PE
 Richard E. Barretto, PE
 Keil D. Maberry, PE
 KC Yellapu, PE
 Dave Roseman, PE
 Shankar Ramakrishnan, PE

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Public correspondence received after Planning & Housing agenda was posted.



c) Not applicable (No direct SHS access)

7. Comment acknowledged. The Project has no direct access to the State Highway System (SHS).

- **Response to Equitable Access:** Comment acknowledged. No Caltrans facilities are impacted by the proposed Project. However, ADA, bicycle, and pedestrians access will be maintained per City of Corona requirements.
- **Response to Caltrans Encroachment Permit:** Comment acknowledged. It is not anticipated that any permanent work or temporary traffic control will encroach into Caltrans right-of-way.

* * * * *

Please let us know if you have any comments or questions regarding this response memorandum.

Attachment

